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To: Councillor Crockett, Convener; Councillor Milne, Vice-Convener; and all Members of the Enterprise, Planning and Infrastructure Committee.

Town House,
ABERDEEN 23 May, 2012

ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE

The Members of the **ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE** are requested to meet in Council Chamber - Town House on **THURSDAY, 31 MAY 2012 at 2.00 pm.**

JANE G. MACEACHRAN
HEAD OF LEGAL AND DEMOCRATIC SERVICES

B U S I N E S S

1 Determination of Exempt items of Business

Members are requested to resolve that any exempt business on this agenda be considered with the press and public excluded.

2 Requests for Deputation

(1) Mrs Reid – Braeside and Mannofield Community Council – in relation to item 5.1 on the agenda.

3 Presentation

3.1 Aberdeen Airport 2012 Draft Master Plan Presentation by Mr Dan Peck, Head of Development Aberdeen Airport

4 Committee Business Statement and Motions List

4.1 Committee Business Statement (Pages 1 - 20)

4.2 Motions List (Pages 21 - 28)

5 Motions

5.1 Motion by Councillor Yuill (Pages 29 - 34)

'Introduction of possible restrictions to deter Heavy Goods Vehicles from using Morningside Road, Cranford Road and Duthie Terrace as through roads.'

5.2 Motion by Councillor Yuill (Pages 35 - 42)

'Introduction of restrictions that would prevent heavy or large goods vehicles from using Broomhill Road as a through road.'

5.3 Motion by Councillor Yuill (Pages 43 - 54)

'That given the significant inconvenience and difficulties caused to householders and others by bollards opposite the entrance to Deeside Drive to the rear lane lying between the rear of properties on Deeside Drive and Deeside Crescent, Council instructs officers to report to the appropriate Committee on the removal of these bollards.'

5.4 Motion by Councillor Graham (Pages 55 - 62)

'That the officers consult with the residents of Bonnyview Drive, Aberdeen to ascertain the requirement to install traffic calming measures and report back to Committee.'

5.5 Motion by former Councillor Leslie - Marischal College - Broad Street Access (Pages 63 - 70)

6 Referrals

6.1 Minute of Meetings of the Controlled Parking Areas - Working Party of 3 February and 5 April 2012 (Pages 71 - 80)

6.2 Appointment of Members to Specific Sub-Committees, Groups, Council Supported Trusts and Boards - Referral from Council of 16 May 2012 (Pages 81 - 82)

7 Performance Management and Service Issues

- 7.1 Enterprise, Planning and Infrastructure Service Asset Management Plans (Pages 83 - 118)

8 Finance

There are no reports under this heading.

9 Enterprise

- 9.1 Community Council Business Development Grant Competition (Pages 119 - 124)
- 9.2 Positive Procurement Programme - Supplier Development Programme (Pages 125 - 130)
- 9.3 North Sea Commission and External Funding Update (Pages 131 - 148)
- 9.4 International Partnerships and Twinning Applications (Pages 149 - 166)
- 9.5 Sustainable Urban Fringes (SURF) Project Update (Pages 167 - 178)
- 9.6 Olympic Torch Relay (Pages 179 - 182)

10 Planning and Sustainable Development

- 10.1 Draft Supplementary Guidance on 'Infrastructure and Developer Contributions Manual', 'Children's Nurseries and Sports Facilities' and 'Bats and Development' (Pages 183 - 258)
- 10.2 River Don Corridor Framework Supplementary Guidance, in support of the Aberdeen Local Development Plan (Pages 259 - 276)
- 10.3 Supplementary Guidance: Householder Development Guide (Pages 277 - 330)
- 10.4 Technical Advice Note: Retail Impact Assessments (Pages 331 - 334)
- 10.5 Consultation Response to the Aberdeen Airport Master Plan (Pages 335 - 340)
- 10.6 Planning Reform and Consultations (Pages 341 - 362)

11 Infrastructure

PROPOSED TRAFFIC ORDERS COMING TO COMMITTEE FOR THE FIRST TIME

- 11.1 Various Small Scale Traffic Management and Development Associated Proposals (New Works) (Pages 363 - 390)

TRAFFIC ORDERS AT THE MIDDLE STAGE OF THE PROCEDURE

There are no reports under this heading.

TRAFFIC ORDERS AT THE LAST STAGE (WHERE THE MAIN STATUTORY OBJECTION PERIOD IS OVER)

- 11.2 Disabled Persons' Parking Places Traffic Regulation Orders - Outcome of Main Statutory Advertisement Stage (Pages 391 - 416)
- 11.3 Various Traffic Orders - Outcome of Main Statutory Advertisement Stage (Pages 417 - 432)
- 11.4 Four Traffic Orders - Outcome of Main Statutory Advertisement Stage (Pages 433 - 478)

OTHER INFRASTRUCTURE, TRANSPORTATION, ROADS AND PARKING ISSUES

- 11.5 Strategic and Local Transportation Projects Update Report (Pages 479 - 590)
- 11.6 Review of Charges for Street Occupations (Pages 591 - 600)
- 11.7 Justice Mill Lane - Revised Traffic Management Proposals (Pages 601 - 612)
- 11.8 Road and Transport Related Capital Budget Programme 2012/2013 (Pages 613 - 636)

BUSINESS THE COMMITTEE MAY WISH TO CONSIDER IN PRIVATE

- 12.1 Road and Transport Related Capital Budget Programme 2012/2013 Appendix - Exempt Information in Relation to Item 11.8 on the agenda (Pages 637 - 652)

- 12.2 Aberdeen Integrated Catchment Study (Pages 653 - 658)

Please note that reports marked with an * have implications for agreed Priority Based Budget (PBB) options.

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Should you require any further information about this agenda, please contact Allison Swanson, tel. 522822 or email aswanson@aberdeencity.gov.uk

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Please note that this statement contains a note of every report which has been instructed for submission to this Committee. All other actions which have been instructed by the Committee are not included, as they are deemed to be operational matters after the point of committee decision.

ENTERPRISE, PLANNING AND INFRASTRUCTURE

COMMITTEE BUSINESS

31 May 2012

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
1.	Enterprise, Planning and Infrastructure Committee 26.11.09 article 18	<p><u>Berryden Corridor – Transport Infrastructure Improvements</u></p> <p>The Committee resolved to approve the preferred option for progression to detailed design (i.e. the so-called “Do Something” option for the corridor, in association with a bus gate at Bedford Road/Powis Terrace), and that the officials report back on detailed design, costs and programming.</p>	<p>Definitive dates for the major transportation projects are difficult to provide as much of the work required to get to the next key stage for reporting will either be several months away or at least more than 12 months - in which case we don't have committee dates, and as always subject to budget and resource availability. Strategic Transport Projects reports which contain updates on all these projects are provided to the Committee by way of a report or bulletin as and when necessary.</p> <p>As per the update of 24 May funding has been secured for this financial year to enable development boundaries to be established.</p>	Head of Planning and Sustainable Development	Indicative Date 2011/2012	No report required this financial year

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2.	Enterprise, Planning and Infrastructure Committee 26.11.09 article 19	<p><u>(1) Access from the North – An Integrated Transport Solution - (2) Access from the North Proposals “Third Don Crossing”</u></p> <p>The Committee resolved, amongst other things to request a regular report back on progress in these matters, including the development of a Delivery Programme.</p>	<p>At its meeting on 18 January, 2011, the Committee resolved:-</p> <p>(a) agree, in principle, the provisional programme for delivery of the ‘Access from the North’ integrated transport proposals;</p> <p>(b) instruct officers to continue discussions with BEAR and Transport Scotland regarding options on the trunk road network;</p> <p>(c) instruct officers to progress schemes from the full delivery list as priority and funding would permit, subject to consultation and referral to future Committees as required; and</p> <p>(d) to instruct officers to keep the Committee up to date with progress of the delivery plan as timescales might be amended subject to agreement of future years spend profiles of the Non-Housing Capital budget and other relevant sources of funding.</p> <p>Future updates will be provided as</p>	Head of Planning and Sustainable Development	31.05.12	31.05.12

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			<p>funding is identified for delivery of this programme. It is proposed to update Members on an annual basis on progress of the whole programme, with separate reporting should individual elements require further Committee decisions. Where key progress is made during the year, ad-hoc updates can be provided as part of the bulletin or included within the Strategic Transport projects update report. Progress will be reported in May 2012.</p> <p>A report is on the agenda.</p>			
3.	Housing and Environment Committee 16.02.10 article 20	<p><u>Furnishings Contract</u></p> <p>The Committee resolved to request officers to submit a report to the Enterprise, Planning and Infrastructure Committee exploring the possibility of the overall amendment of the Council's procurement procedures to take account of social enterprises etc.</p>	<p>At its meeting on 31 May 2010, the Committee resolved, amongst other things, to instruct officers within Corporate Governance to continue to work on the development of a robust corporate policy on the use of community benefit clauses within the Council's procurements, and that the policy be submitted to a future meeting of the Committee for approval.</p> <p>At its meeting on 13 September, 2011, the Committee noted that a report seeking approval of a guidance policy on delivering community benefits through</p>	Head of Procurement	15.11.11	August 2012

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			<p>procurement would be considered by the Corporate Policy and Performance Committee on 22 September, 2011, and that if approved, officers would report to the next meeting of the Enterprise, Planning and Infrastructure Committee with a user friendly guide to the policy specifically for this service.</p> <p>The Corporate Procurement Unit (“CPU”) are taking a lead on community benefits and will shortly be providing officer training on this topic. Category Managers are now aligned to Services and this will allow them to provide a more tailored Service on issues such as community benefits. It is therefore thought that the CPU will be able to cater for the previously identified needs in respect of community benefits. An update report on the Council’s use of the community benefit clauses will be submitted in the next 6-9 months to ensure that community benefit needs are being met.</p>			
4.	Enterprise, Planning and Infrastructure 23.02.10 article 27	<p><u>Aberdeen Western Peripheral Route – Progress Report</u></p> <p>The Committee resolved to instruct officers to provide a further report to</p>	<p>An appeal by Mr Walton against the judgement of Lord Tyre ([2011] COSH 131) sitting in the Outer House of the Court of Session was heard before Lords Clarke,</p>	Head of Planning and Sustainable Development	Date can't be determined until the completion	

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	<p>Council Budget 10.02.11</p>	<p>the appropriate Committee during the procurement process providing an updated cost estimate and programme.</p> <p><u>General Fund Revenue Budget 2011/2012 and Indicative 2012/2013 to 2015/2016 Budget</u></p> <p>To instruct the Director for Enterprise, Planning and Infrastructure to enter into negotiations with the Scottish Government with a view to having the 3rd Don crossing delivered as part of the Aberdeen Western Peripheral Road Scheme and report back to the relevant Committee on the progress and outcome.</p>	<p>Bonny and Philip sitting in the Inner House of the Court of Session between 13th & 16th December 2011.</p> <p>Their Lordships delivered their judgement on 29th February 2012 dismissing the appeal.</p> <p>Following the issuing of the judgement, the unsuccessful party has a statutory right of appeal within 42 days to the UK Supreme Court in London, which acts as the final court of appeal for all United Kingdom civil cases. Mr Walton on the 12 April 2012 submitted an appeal to the UK Supreme Court. A date for a hearing has yet to be set.</p> <p>At this stage it is difficult to be precise about the impact of the appeal will have on the project timetable, however it is likely that the appeal will delay the project by at least a further 9 to 12 months.</p> <p>Officers from Aberdeen City and Aberdeenshire Councils and Transport Scotland are currently examining the impact of this further delay to the AWPR programme.</p>		<p>of the statutory procedure and the commencement of the formal procurement process</p>	

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5.	Enterprise, Planning and Infrastructure 20.04.10 article 24	<p><u>Justice Mill Lane</u></p> <p>The Committee resolved to request officials to carry out preliminary statutory consultation where a traffic order to provide for the proposals outlined in the report, to move street to substantive statutory advertisement if there were no significant preliminary responses, and thereafter to report back on detailed design, cost estimates and substantive statutory objections.</p>	<p>An update is on the agenda.</p> <p>At its meeting on 13 September, 2011, the Committee resolved not to proceed with the current proposed traffic management proposal for Justice Mill Lane, and to request officers to further review the area and report back to the Enterprise, Planning and Infrastructure Committee with new traffic management proposals that would benefit pedestrians, cyclists and motorists, and preferably that this scheme would allow the area to remain two-way.</p> <p>The decision by the Committee will require substantial redesign work to be carried out and to commence the legal process again for the Traffic Regulation Order. Officers are currently reviewing the design of Justice Mill Lane and once complete will consult with the local councillors prior to being presented to the next Enterprise, Planning and Infrastructure Committee in May 2012.</p> <p>A report is on the agenda.</p>	Head of Asset Management and Operations	31.01.12	31.05.12

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6.	Enterprise, Planning and Infrastructure 18.01.11 article 17	<p><u>Community Digital Media Channel</u></p> <p>The Committee resolved, amongst other things, to instruct officers to seek external funding to progress with this project; and to receive regular updates on progress of this project.</p>	<p>Aberdeen City Council have been invited to participate in an advisory group being established by RGU to look at Smart Media and the management of digital assets. The potential of "cultural" assets being included within the remit of such work has been identified with a view to utilising this technology to manage available content on an Aberdeen Digital Channel. If agreed a bid will be tabled for lottery (or alternative) funding to further develop this area of work.</p> <p>Such an approach will widen the opportunity for participation in the project and help to overcome issues around content provision within the channel deliverable.</p> <p>The UK Government has announced its intentions with regards the delivery of Local (broadcast) TV and has included Aberdeen as one of the potential areas to be included in any roll out. The Government will make available broadcast space in the spectrum and a potential for some support funding for Local TV channels through the license fee.</p> <p>A review of the findings of the original Community Digital Media</p>	Economic/ Business Development Project Director	24.05.11	After summer recess

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			<p>Channel research will be undertaken in light of this intervention with a view to assessing whether the case for a broadcast channel is sufficiently improved as a result of this announcement as to make it a preferred option.</p> <p>A report will be submitted to a future committee on the findings of this review and proposals on delivery methods.</p> <p>A report will be submitted following the summer recess.</p>			
7.	Enterprise, Planning and Infrastructure 15.03.11 article 4	<p><u>Minute Of The Meeting Of The Controlled Areas Parking Working Party</u></p> <p>The Committee resolved, amongst other things,</p> <p>(i) in relation to recommendation 5, to agree that a review of parking charges and entitlement set by other Council services be undertaken and reported to the Enterprise, Planning and Infrastructure Committee for consideration and possible revision;</p>	<p>Officers were requested to report back on a number of issues which requires a considerable degree of investigation, survey work and staff input to complete the work.</p> <p>An update on these are as follows:-</p> <p>(i) All other relevant Services within ACC have been contacted to request details of the locations within their service where charging occurs and to request details of the charges applied. Officers are still awaiting a response and the review may return to the Committee at its meeting on 31 January,</p>	Head of Asset Management and Operations	13.09.11	31.01.12

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		<p>(ii) in relation to recommendation 6, to request officers to submit a detailed report on the possibility of establishing a city wide emissions-based permit system, on the proviso that such a proposal would be revenue neutral to the Enterprise, planning and Infrastructure Committee at its meeting on 13 September, 2011;</p> <p>(iii) in relation to recommendation 8, to request officers to submit a detailed report on the price of city centre residential permits per year and also on the creation of intermediate parking zones, with differential permit prices reflecting the premium for parking spaces to a future meeting of the Enterprise, Planning and Infrastructure Committee; and</p> <p>(iv) in relation to recommendation 11, to request officers to submit a detailed report on the price of parking vouchers in the Foresterhill and Garthdee on-</p>	<p>however it is more likely to be reported on 31 May 2012;</p> <p>(ii) In relation to resolution iii, work regarding the possibility of establishing a city wide emissions-based permit system is ongoing; this will be considered at the Controlled Parking Areas - Working Party on 3 February 2012. Thereafter, the proposal will be developed in conjunction with the review of 2012/2014 parking charges to be undertaken in Autumn 2012 and reported to Committee in Spring 2013 ;</p> <p>(iii) This review may be reported to Committee at its meeting on 31 May;</p> <p>(iv) The decision at the September 2011 committee was to request officers for further details and survey work to be carried out. It is</p>			<p>31.05.12</p> <p>Spring 2013</p> <p>31.05.12</p> <p>31.05.12</p>

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		<p>street zones, and that this include information on the financial implications, comparable charges, new payment technologies and the existing prices to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>	<p>intended that this report will be submitted to the Committee at its meeting on 31 May.</p> <p>The minutes of the last two meetings of the Group are on the agenda and deal with the above actions.</p>			
8.	Housing and Environment Committee 13/04/10 article 30	<p><u>Community Gardens Policy</u></p> <p>The Committee resolved: to instruct the Director of Housing and Environment to report back in 2011 on progress with the implementation of the policy.</p>	<p>The aims of the Community Gardens Policy have been subsumed into and superceded by Aberdeen's Open Space Strategy. The Strategy was approved by this Committee on 11th September 2011 (Report: EPI/11/193). The Strategy includes a detailed action plan which, in part, focuses on encouraging greater community involvement and partnership working in developing and maintaining open spaces. Specifically: 3.3: Support communities and organisations who express an interest in community-led maintenance of open spaces, including 3.3.1: Develop and promote web based resource for community groups and local businesses to form park user groups such as 'Friends of groups'; 6.1: Develop private, public and</p>	Head of Planning and Sustainable Development	10.05.11	31.05.12

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			<p>voluntary partnerships;</p> <p>6.2: Build capacity of communities to take responsibility for open spaces, including 6.2.1: Develop a Guide to Improving Your Open Spaces to facilitate community ownership and management of open spaces;</p> <p>6.5: Establish an Open Space Forum to promote, encourage and oversee community based projects and share skills and experience.</p> <p>These actions aim to provide a more welcoming and practical approach to involving communities in the planning & management of public open spaces.</p> <p>Recommended for removal.</p>			
9.	Housing and Environment 12/01/11 article 29	<p><u>Environmental Strategy</u> –</p> <p><u>Progress Report</u></p> <p>The Committee resolved: to instruct officers to provide this Committee with an annual update on environmental progress through the Scottish Climate Change declaration report format.</p>	<p>Referred by the Housing and Environment Committee.</p> <p>A report is on the agenda.</p>	Head of Planning and Sustainable Development	31.01.12	31.01.12
10	Enterprise, Planning and Infrastructure 13.09.11 article 24	<p><u>VisitScotland Funding 2011/2012</u></p> <p><u>And Proposed Aberdeen Destination Marketing Organisation (DMO)</u></p> <p>The Committee resolved, amongst</p>		Economic/ Business Development Project Director	September 2012	

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		<p>other things, to authorise the submission of a further report to Enterprise, Planning and Infrastructure Committee in September 2012, requesting ongoing council support for the Aberdeen DMO for the next 3-5 years, based on the transfer of existing resources (i.e. no new resources), and subject to the DMO Chief Executive and Board:-</p> <p>(1) producing an appropriate, detailed and financially sustainable 3-5 year Business Plan; and</p> <p>(2) securing significant private sector cash contributions or other non-council income streams, equivalent to at least 20% of the DMO's total annual operating costs over the 3-5 year business plan period.</p>				
11	Enterprise, Planning and Infrastructure 13.09.11 article 24	<p><u>South Aberdeen Coastal Regeneration Project (SACRP) – Programme Developments</u></p> <p>The Committee resolved, amongst other things, agree to receive a report back to the Enterprise, Planning and Infrastructure Committee in the April/May 2012 cycle, linking coastal regeneration initiatives into the mainstream development of climate change</p>	A report is on the agenda.	Economic/ Business Development Project Director	31.05.12	31.05.12

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		adaptation strategies and flood risk management.				
12	Enterprise, Planning and Infrastructure 13.09.11 article 33	<u>North Dee – Development Update</u> The Committee resolved, amongst other things, to agree that relevant officers commence the initial design and related studies for the North Dee Controlled Parking Zone (NDCPZ), and to request officers to report back to the Enterprise, Planning and Infrastructure Committee on its feasibility and the results of the informal consultations, prior to the commencement of the legal process for the required Traffic Regulation Order.		Head of Asset Management and Operations	End of 2012	
13	Enterprise, Planning and Infrastructure 13.09.11 article 35	<u>Abbotswell Crescent/ Redmoss Road/ Berryden Road/ Bingham Road, Milltimber/ Brunswick Place/ Claymore Avenue/ Fonthill Road/ Greenfern School/ Hardgate/ Hermitage Avenue/ High Street/ Howe Moss Drive/ Mcdonald Court/ Morningfield Road/ Osborne Place/ Queens Road/ Riverside Drive/ Belgrave Mansion/ Scott Cassie Circle/ Cairn Road, Peterculter/ East North Street Car Park/ Loading Bays In The Green</u>	These schemes are about to go out to Initial Statutory, Should any objections be received then they will be reported back to the September 2011 committee, otherwise they will go straight out to public advert and be reported to the May 2012 committee. A report is on the agenda.	Head of Asset Management and Operations	31.01.12	31.05.12

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		<p>The Committee resolved, amongst other things,</p> <p>(i) in respect of the proposal at Bingham Road, Milltimber, to request officers to extend the proposed “at any time” waiting restrictions to beyond the junction into Bingham Crescent and that it be advertised on this basis;</p> <p>(ii) to approve the proposals, in principle, and instruct officers to commence the necessary legal procedures of the preliminary statutory consultation for the traffic regulation orders as required, and if no significant objections were received, then to progress with the public advertisement and report the results to a future meeting of the Enterprise, Planning and Infrastructure Committee;</p> <p>(iii) instruct the appropriate officers to commence the combined statutory consultation for the traffic regulation order for the list of Blue Badge parking spaces and report back to a future meeting of the Enterprise, Planning and Infrastructure Committee; and</p> <p>(iv) to instruct officers to</p>				

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		undertake the necessary process to revert two of the four existing loading bays in The Green to one on-street parking space and one disabled parking space.				
14	Enterprise, and Infrastructure 13.09.11 article 39	<u>Greenbrae Cycle Project</u> The Committee resolved, amongst other things, to instruct officers to report back to this Committee on an annual basis on progress/ impact and intermittently as legislative decisions required.	Progress will be included in the Strategic and Local Transport Projects Update Report.	Head of Planning and Sustainable Development	September 2012	
15	Enterprise, Planning and Infrastructure 13.09.11 article 41	<u>Blue Badge Reform</u> The Committee resolved, amongst other things, (i) to instruct officers to review the possible impact on the City Council's Green Badge scheme and to report back to a future meeting of the Committee following consultation with interested parties; and (ii) to instruct officers to review the staffing and resources necessary to support the Blue Badge Scheme following the implementation of reforms in January 2012.	A report will be submitted following the summer recess.	Head of Asset Management and Operations	31.01.12	Post summer recess

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16	Enterprise, Planning and Infrastructure 15.11.11 article 16	<p><u>Supplementary Guidance: Householder Development Guide</u></p> <p>The Committee resolved, amongst other things, to agree that following completion of the relevant consultation, any comments received and subsequent amendments to the draft Supplementary Guidance be presented to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>	A report is on the agenda.	Head of Planning and Sustainable Development	31.05.12	31.05.12
17	Enterprise, Planning and Infrastructure 15.11.11 article 17	<p><u>Technical Advice Note : The Repair And Replacement Of Windows And Doors</u></p> <p>The Committee resolved, amongst other things, to agree that following completion of the relevant consultation, any comments received and subsequent amendments to the draft advice note be presented to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>		Head of Planning and Sustainable Development	31.05.12	Post summer recess
18	Enterprise, Planning and Infrastructure 15.11.11 article 18	<p><u>Supplementary Guidance : Wind Turbine Development In Aberdeen City</u></p> <p>The Committee resolved, amongst other things, to agree that following completion of the relevant consultation, any comments</p>	A report will be submitted following the summer recess.	Head of Planning and Sustainable Development	31.05.12	Post summer recess

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19	Enterprise, Planning and Infrastructure 15.11.11 article 19	<p>received and subsequent amendments to the draft Supplementary Guidance be presented to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p> <p><u>Craigton Road, Huntly Street, Ivanhoe Place, Midstocket Lane, Queens Lane South, Roslin Place, Auchmill Road, Bedford Road And University Access Road, Foresterhill Road/Aberdeen Royal Infirmary Bus Interchange, North Balnagask Road</u></p>	<p>These schemes are about to go out to Initial Statutory, Should any objections be received then they will be reported back to the September 2011 committee, otherwise they will go straight out to public advert and be reported following the summer recess.</p> <p>A report will be submitted following the summer recess.</p>	Head of Asset Management and Operations	31.05.12	Post summer recess
	<p>The Committee resolved:-</p> <p>(i) to approve the proposals, in principle, and instruct officers to commence the necessary legal procedures of the preliminary statutory consultation for the traffic orders as required, and if no significant objections were received, then to progress with the public advertisement and report the results to a future meeting of the Enterprise, Planning and Infrastructure Committee; and</p> <p>(ii) to instruct officers to commence the combined statutory consultation for the traffic order for the list of Blue Badge parking spaces and report back to a future meeting of the</p>					

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		Enterprise, Planning and Infrastructure Committee.				
20	Enterprise, Planning and Infrastructure 31.01.12 article 33	<p><u>Draft River Don Corridor Framework – Supplementary Guidance In Support Of The Proposed Aberdeen Local Development Plan</u></p> <p>The Committee resolved, amongst other things, to agree that following completion of a public consultation, any comments received and subsequent amendments to the draft Supplementary Guidance be presented to a future meeting of the Enterprise, Planning and Infrastructure Committee for approval.</p>	A report is on the agenda.	Head of Planning and Sustainable Development	After summer recess 2012	31.05.12
21	Enterprise, Planning and Infrastructure 31.01.12 article 36	<p><u>Technical Advice Note – Retail Impact Assessments</u></p> <p>The Committee resolved, amongst other things, to agree that following completion of the relevant consultation, any comments received and subsequent amendments to the draft advice be presented to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>	A report is on the agenda.	Head of Planning and Sustainable Development	After summer recess 2012	31.05.12

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
22	Enterprise, Planning and Infrastructure 31.01.12 article 39	<p><u>Ashgrove Road/Braeside Terrace, Bridge Street, Broomhill Road, Cornhill Road, Forest Avenue, Girdleness Road, Hilton Place At Hilton Street Junction, Kettlehills Lane, Northfield, Kirkhill Road, Lower Deeside Hgv Restriction Modification, Morningside Lane, Newhills Avenue, North Deeside Road, Peterculter, Palmerston Road, Ross Crescent, South Esplanade East, The Bush, Peterculter, Union Grove Lane, Urquhart Place, Advocates Road, Burnside Road, Grampian Road, Greenbank Crescent, Littlejohn Street And Queen Street, And Raik Road</u></p> <p>The Committee resolved:-</p> <p>(i) approve the proposals, in principle, and instruct officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation orders required, and if no significant objections were received, then to progress with the public advertisement and report the results to a future meeting of the Enterprise, Planning and Infrastructure Committee;</p> <p>(ii) instruct officers to commence the combined statutory</p>	A report will be submitted following the summer recess.	Head of Asset Management and Operations	After summer recess 2012	Post summer recess

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
		<p>consultation for the traffic order for the list of Blue Badge parking spaces and to report back to a future meeting of the Enterprise, Planning and Infrastructure Committee; and</p> <p>(iii) that officers promote the proposed 'at any time' waiting restrictions on both sides of Kettlehills Lane, Northfield.</p>				
23	Enterprise, Planning and Infrastructure 31.01.12 article 42	<p><u>Audit Scotland Report On Transport For Health And Social Care – Self Assessment And Action Plan</u></p> <p>The Committee resolved, amongst other things, to request officers to report back to the Committee at its meeting on 31 May, 2012, with a detailed action plan setting out how the Council and partner organisations would tackle the areas for development following self assessment and how the Council would meet the recommendations as set out by the Audit Scotland report.</p>	<p>At this stage there is still one Health Transport Action Plan (HTAP) partner organisation to go through their self assessment. This is expected imminently following which all self assessments will be collated and an action plan will be drawing up as a collective with all HTAP partners. As we are not at this stage yet, officers are not able to provide the requested action plan report to Committee at this cycle.</p>	Head of Planning and Sustainable Development	31.05.12	Post summer recess

Please note that this statement tracks all Notices of Motion submitted by members, until the point of disposal. The motion will remain on the statement until the Committee has agreed to remove it.

ENTERPRISE, PLANNING AND INFRASTRUCTURE

MOTIONS LIST

31 May 2012

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
1.	<p><u>Motion by Councillor Yuill</u></p> <p>“That Aberdeen City Council agrees to instruct officers to prepare a report on both the feasibility of developing, in partnership with Aberdeenshire Council and Perth and Kinross Council, a long distance footpath – The Royal Deeside and Perthshire Way – from Fittie to Perth via Deeside and ways in which this project might be funded.”</p>	27.04.11	<p><u>Enterprise, Planning and Infrastructure 13.09.11</u></p> <p>Amongst, other things, to request officers to participate in the Royal Deeside, Angus and Perthshire Way Steering Group which was already discussing the establishment of the “Pictish Way”, and that officers report back to the next meeting of the Enterprise, Planning and Infrastructure Committee regarding the terms of the motion, and in particular detailing the outstanding aspects required to establish and signpost the proposed “Pictish Way”, as well as the financial cost of achieving this.</p>	<p>At its meeting on 31 January 2012, the Committee resolved to:-</p> <p>(i) instruct officers to monitor developments on the Pictish Way by partners and attend Steering Group meetings (so far held in Forfar) where resources allowed; and</p> <p>(ii) instruct officers to report back to the Enterprise, Planning and Infrastructure Committee after the summer recess on the findings of a report that Angus Council intended to commission into the development of the route.</p>	Head of Planning and Sustainable Development	31.05.12	No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
2.	<p><u>Motion by Councillor Yuill</u></p> <p>“That, given the ongoing difficulties caused by HGVs using Broomhill Road as a through route, Council instructs officers to report to the appropriate committee on the introduction of an ‘except for access’ weight or width restriction on some or all of Broomhill Road between Holburn Street and South Anderson Drive.”</p>	17.08.11	<p><u>Enterprise, Planning and Infrastructure 13.09.11</u></p> <p>To request officers to report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>	<p>Additional surveys are required to be carried out and it is intended to submit a report to the Committee at its meeting on 31 January.</p> <p>At its meeting on 31 January 2012, the Committee resolved to request officers to explore other options available, not excluding the introduction of an “except for access” width and/or weight restriction, to address the concerns of residents on Broomhill Road, and to report back to a future meeting of the Enterprise, Planning and Infrastructure Committee on these options, as well as the implications of the introduction of each.</p> <p>A report is on the agenda.</p>	Head of Asset Management and Operations	31.5.12	Yes – to be decided on consideration of the report.

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
3.	<p><u>Motion by Councillor Yuill</u></p> <p>“That, given the ongoing difficulties caused by lorries using Morningside Road, Cranford Road and Duthie Terrace as through routes so as to avoid the junction of Great Western Road and South Anderson Drive, Council instructs officers to report to the appropriate committee on the introduction of an ‘except for access’ weight restriction on these roads plus that part of Hammerfield Avenue not already covered by such a restriction.”</p>	17.08.11	<p><u>Enterprise, Planning and Infrastructure 13.09.11</u></p> <p>To request officers to report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>	<p>Additional surveys are required to be carried out and it is intended to submit a report to the Committee at its meeting on 31 January.</p> <p>At its meeting on 31 January 2012, the Committee resolved to defer a decision on the report and request officers to contact BEAR Scotland regarding the possible reconfiguration of the junction from Great Western Road to Anderson Drive, and to report back to a future meeting of the Committee in this regard.</p> <p>A report is on the agenda.</p>	Head of Asset Management and Operations	31.1.12	Yes – to be decided on consideration of the report.

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
4.	<p><u>Motion by Councillor Yuill</u></p> <p>“That, given the significant inconvenience and difficulties caused to householders and others by bollards opposite the entrance from Deeside Drive to the rear lane lying between the rear of properties on Deeside Drive and Deeside Crescent, Council instructs officers to report to the appropriate committee on the removal of these bollards.”</p>	17.08.11	<p><u>Enterprise, Planning and Infrastructure 13.09.11</u></p> <p>To request officers to report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>	<p>Officers are required to liaise with the Public Utility companies and carry out some design work in relation to the Notice of Motion. It is intended that this work will be submitted to the Committee at its meeting on 31 January.</p> <p>At its meeting on 31 January 2012, the Committee resolved to defer consideration of the report and to request officers to explore alternative solutions to the difficulties experienced by the bollards opposite the entrance to Deeside Lane and report back to a future meeting of the Committee in this regard.</p> <p>A report is on the agenda.</p>	Head of Asset Management and Operations	31.5.12	Yes – to be decided on consideration of the report.
5.	<p><u>Motion by former Councillor Farquharson</u></p> <p>“Council is asked to set up a working party to explore the possibility of changing the whole structure of traffic speed</p>	6.10.11	<p><u>Enterprise, Planning and Infrastructure 15.11.11</u></p> <p>To request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and</p>	<p>At its meeting on 31 January 2012, the Committee resolved to request officers to report back a future meeting of the Enterprise, Planning and Infrastructure Committee with further information on:-</p>	Head of Asset Management and Operations	31.5.12	No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
	<p>controls within the city of Aberdeen. Such a study should start from the principle of having all housing sub divisions designated as 20mph traffic zones within arterial or main roads being designated as 30 or 40mph maximum speed zones.</p> <p>The objective is to create standardisation in the interests of road safety and to alleviate confusion in the minds of drivers. This has become necessary due to the vast array of speed bumps, mandatory 20mph roads and advisory 20mph roads. As a result of these changes it is intended to educate the public in terms of safe driving. The working group should comprise Council officers and token Councillor representation, reporting to Council or Committee as necessary."</p>		<p>Infrastructure Committee, and to agree that a Working Group should not be established for this purpose.</p>	<p>(a) the blanket city wide 20mph for residential uses implemented by Portsmouth Council; (b) traffic controls implemented by other comparable cities; and (c) the prosecution rates for motorists exceeding the existing 20mph speed limit in the city.</p> <p>A report will be submitted following the summer recess.</p>			

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
6.	<u>Motion by former Councillor Leslie</u> “That officers are instructed to prepare a report on proper access for disabled people using mobility scooters at Marischal College, and such a report be remitted to the appropriate Committee.”	14.12.11	<u>Enterprise, Planning and Infrastructure 31.01.12</u> The Committee resolved to request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee and to request that the Disability Advisory Group be consulted on this matter.		Programme Director for Marischal	31.05.12	
7.	<u>Motion by Councillor Graham</u> “That the relevant department consults with the residents of Bonnyview Drive to ascertain the requirement to install traffic calming measures and report back to Committee.”	14.12.11	<u>Enterprise, Planning and Infrastructure 31.01.12</u> The Committee resolved to request officers to undertake traffic surveys on Bonnyview Drive and to report back to the Enterprise, Planning and Infrastructure Committee with their findings, and whether there was a requirement for traffic calming measures at this location.	A report is on the agenda.	Head of Asset Management and Operations	31.5.12	Yes – to be decided on consideration of the report.

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
8.	<p><u>Motion by Councillor Graham</u></p> <p>“That officers provide a report to advise Aberdeen City Council on what current measures are in place and what further measures need to be undertaken to promote/ensure competition in the bus routes of Aberdeen, thus ensuring a more competitive price for buses for the citizens of Aberdeen that are comparable with other cities in Scotland.”</p>		<p><u>Enterprise, Planning and Infrastructure 31.01.12</u></p> <p>The Committee resolved to request the Convener to write to both the Westminster and Scottish Governments promoting a change in legislation regarding bus routes to encourage competition and deliver fair prices.</p>		<p>Planning and Sustainable Development</p>		<p>Yes</p>
9.	<p><u>Motion by Councillor Jennifer Stewart</u></p> <p>“That this Council agrees to run a robust recruitment campaign to increase the number of school crossing patrollers, the costs to be met through budget underspend.”</p>		<p><u>Enterprise, Planning and Infrastructure 31.01.12</u></p> <p>The Committee resolved to approve the terms of the motion and to request the Director to implement this accordingly.</p>		<p>Head of Asset Management and Operations</p>		<p>Yes</p>

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Introduction of possible restrictions to deter Heavy Goods Vehicles from using Morningside Road, Cranford Road and Duthie Terrace as through routes.
REPORT NUMBER:	EPI/12/119

1. PURPOSE OF REPORT

This report addresses the instruction from the Committee meeting on 30 January that officers contact BEAR Scotland regarding the possible reconfiguration of the traffic light junction from Great Western Road to Anderson Drive, and to report back to a future meeting of the Committee in this regard.

2. RECOMMENDATION(S)

That the Committee agrees

- a) to take no action at this time given the low volume of Large / Heavy Goods Vehicles using these roads and the level of resources that would be required to ensure compliance with a traffic order restricting such vehicles.
- b) officers continue to monitor the level of usage of these roads by HGVs on an annual basis and if there should be a significant change reconsider the possibility of introducing a restriction.

3. FINANCIAL IMPLICATIONS

Given the above recommendation there would be no significant financial implications. However, if a restriction on HGVs was to be introduced, the illuminated signs and advance warning signs associated with a restriction would cost in the region of £7500.

4. OTHER IMPLICATIONS

None

5. BACKGROUND/MAIN ISSUES

- 5.1 At the meeting of this committee on 13 September 2011 a motion by Councillor Yuill on the following terms was considered: -

“That, given the ongoing difficulties caused by lorries using Morningside Road, Cranford Road and Duthie Terrace as through routes so as to avoid the junction of Great Western Road and South Anderson Drive, Council instructs officers to report to the appropriate committee on the introduction of an ‘except for access’ weight restriction on these roads plus that part of Hammerfield Avenue not already covered by such a restriction.”

The Committee instructed officers to report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee.

- 5.2 A report on the terms of Councillor Yuill’s motion was considered at the committee on 31 January 2012. The recommendations in this report being that given the low volume of Heavy Goods Vehicles (HGVs) using Morningside Road, Cranford Road and Duthie Terrace, the introduction of a restriction on such vehicles, and the level of resources required to ensure compliance with any order, the Committee agrees to take no action at this time. It also recommended that officers continue to monitor the level of usage of these roads by HGVs on an annual basis to identify when any restrictions would be required.
- 5.3 After considering the report the January committee resolved to defer a decision and request officers to contact BEAR (Scotland) Ltd with regard to the possible reconfiguration of the junction from Great Western Road to Anderson Drive, and to report back to a future meeting of the Committee.

5.4 The issues of congestion and delay at the junction are consistently raised with the Trunks Road Authority at liaison meetings. Officers of Aberdeen City Council did carry out a study of the junction two years ago and considered options to improve the junction performance. The study considered a number of modifications to the junction. Changes where traffic modeling showed there would be no improvement over the current situation, were: -

- a) a ban on westbound vehicles from turning right on to Anderson Drive from Great Western Road.
- b) separate phases for each arm of the junction.
- c) widening of Great Western Road on the west side of the junction.

Whilst for other options, modest improvements were shown to be possible by the following: -

- d) a ban on northbound vehicles from South Anderson Drive turning right on to Great Western Road and changes to the running phases of the signals.
- e) the aforementioned option (d) in combination with the widening of Great Western Road on the west side of the junction.
- f) (i) a ban on northbound vehicles from South Anderson Drive turning right on to Great Western Road.
(ii) provide a full right turn lane on Anderson Drive for southbound vehicles turning on to Great Western Road.
(iii) changes to the running phases of the signals.
(iv) widening of Great Western Road on the east side of the junction to create a two lane entry to the junction.

With regard to the above, the traffic modeling indicated option 'e' would offer most improvement. However, the Trunk Road Representative did not feel the alterations could be justified.

5.5 Consultation with BEAR (Scotland) Ltd was carried out in line with the committee instruction and the trunk road representative has indicated there are no current or future plans to alter the configuration of this junction. The representative from BEAR (Scotland) Ltd highlighted the planned Aberdeen Western Peripheral Route (AWPR) as the long-term solution that would reduce congestion.

- 5.6 Officers also consider the introduction of the AWPR as being the long-term solution to easing congestion at this junction. As highlighted in the previous report traffic modeling has suggested in the first year of opening the volume of traffic on Anderson Drive will reduce by 22%, with vehicles of the goods class falling by 28%. These falls are highly significant and in turn traffic flow at the Great Western Road / Anderson Drive junction would be improved. The effect on surrounding streets would be twofold; drivers would be less inclined to use surrounding streets to avoid the junction and given the substantial reduction of HGVs on Anderson Drive, it is likely any continuing through traffic would be of the light vehicle type e.g. cars, motorcycles etc.
- 5.7 If a restriction on HGVs was introduced, the time associated with surveillance and prosecution would be considerable, with the Police required to wait and thereafter track a vehicle entering and exiting the restricted route. Consequently, unless such a restriction is self-enforcing the level of compliance through Police enforcement is unsustainable and community expectations in this regard often unrealistic.
- 5.8 As indicated in the previous report, the number of Large/Heavy Goods Vehicles is low when considering the overall volume of motor vehicles entering these streets. The surveys indicate an element of HGVs using Cranford Road and occasional HGVs using Duthie Terrace and Morningside Road. The volume of these vehicles must also be considered in the wider context of the overall road network and similar street types, and the current situation would not be considered exceptional. It is also anticipated there would be no significant change in the volume of such vehicles on these streets in the coming years, albeit the situation will continue to be monitored.
- 5.9 In conclusion, the recommendations within the original report remain; to take no further action at this time, but nevertheless continue to monitor traffic volumes / vehicle classification on an annual basis. Then, should there be further significant increases in the volume of HGVs utilising these roads, reconsider the possibility of introducing a restriction on HGVs.

6. IMPACT

6.1 This report will be of interest to the residents/proprietors on the roads concerned.

6.2 There are links to the Single Outcome Agreement in terms of living life free of crime, disorder and danger and to live in well designed, sustainable places where we are able to access the amenities and services we need.

7. BACKGROUND PAPERS

“Report following Councillor Yuill’s motion at the September 2011 E, P & I Committee to introduce restrictions that would deter Heavy Goods Vehicles from using Morningside Road, Cranford Road and Duthie Terrace as through routes.” Enterprise, Planning & Infrastructure Committee, 31 January 2012.

<http://councilcommittees/ieListDocuments.aspx?CId=140&MId=2283&Ver=4>

8. REPORT AUTHOR DETAILS

Graeme McKenzie
Technical Officer
Road Safety & Traffic Management Team
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Consultee Comments

Councillors

Ian Yuill - ***has been consulted.***

Callum McCaig – ***has been consulted.***

Council Officers

Barry Jenkins, Head of Finance – ***no comment.***

Jane MacEachran, Head of Legal & Democratic Services – ***has been consulted.***

Ciaran Monaghan, Head of Service, Office of Chief Executive – ***has been consulted.***

Gordon McIntosh, Director of Enterprise, Planning and Infrastructure – ***has been consulted.***

Hugh Murdoch, Head of Asset Management & Operations – ***has been consulted.***

Margaret Bochel, Head of Planning & Sustainable Development - ***we agree with the recommendations within this report.***

Mike Cheyne, Roads Manager – ***has been consulted.***

Dave Young, ICT, Enterprise Planning and Infrastructure – ***has been consulted.***

ABERDEEN CITY COUNCIL

COMMITTEE :	Environment, Planning and Infrastructure
DATE:	31st May 2012
DIRECTOR:	Gordon McIntosh
TITLE OF REPORT:	Report following Councillor Yuill's motion at the September 2011 E, P and I Committee to introduce restrictions that would prevent heavy or large goods vehicles from using Broomhill Road as a through route.
REPORT NUMBER:	EPI/11/285

1. PURPOSE OF REPORT

This report responds to Councillor Yuill's notice of motion "That given the ongoing difficulties caused by HGVs using Broomhill Road as a through route, Council instructs officers to report to the appropriate committee on the introduction of an "except for access" weight or width restriction on some or all of Broomhill Road between Holburn Street and South Anderson Drive" and to explore other options available, not excluding the introduction of an "except for access" width and/or weight restriction, to address the concerns of residents on Broomhill Road.

2. RECOMMENDATION(S)

It is recommended that Committee:

1. note the contents of this report
2. agree that, given the strategic importance of Broomhill Road within the road network and the proportionately small percentage of HGVs that are using Broomhill Road as a "through route", restricting LGVs/HGVs along Broomhill Road would not be appropriate.
3. agree that no action be taken at this time

3. FINANCIAL IMPLICATIONS

There are no financial implications for the recommendation. However, should the Committee agree to any measure to address the residents' concerns there will be financial implication which can be explained by officers at the committee meeting

4. OTHER IMPLICATIONS

Implementing a restriction of this magnitude over a road of this importance will have significant implications for Bear (Scotland) and their ability to manage trunk road traffic under emergency conditions. Although in practical terms Grampian Police could over-rule any traffic restriction that is in place, consideration would have to be given to reforming the Major Incident Diversion Plan

It is likely that they will put forward an objection to this proposal.

5. BACKGROUND/MAIN ISSUES

- 5.1 At the meeting of this Committee on 13th September 2011 a motion from Councillor Yuill was considered:

“That given the ongoing difficulties caused by HGVs using Broomhill Road as a through route, Council instructs officers to report to the appropriate committee on the introduction of an ‘except for access’ weight or width restriction on some or all of Broomhill Road between Holburn Street and South Anderson Drive”

This committee subsequently instructed officers to report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee which was presented to the EP & I on 31st January 2012 with the findings. The Committee instructed a further report detailing other options and their respective merits to be reported back at a future meeting.

- 5.2 Over the years there have been a number of residential complaints and inquiries regarding the volume of Heavy Goods Vehicles (HGVs) using Broomhill Road and the possibility of limiting their access. For the purpose of this report the term HGV will also cover Large Goods Vehicles (LGVs) that are not necessarily over the 7.5T weight limit but have a significant physical presence.
- 5.3 Surveys carried out in the past have shown significant usage of Broomhill Road by HGVs but that this is restricted mainly to the peak flow hours. Broomhill Road is a district distributor road linking to the city centre from the south-west serving a large residential area, but also providing access to a number of commercial and retail properties along its length. These retail premises require regular deliveries, some of which require HGV access ie vehicles over 11.5m in length. Although these vehicles would still be eligible to access these premises under the ‘except for access’ exemption, they will also form a significant proportion of the HGVs recorded in the surveys detailed in the table below.

- 5.6 Broomhill Road also forms part of the No 1 and 2 bus routes which travel from the City Centre and extends beyond the Robert Gordon University in Garthdee to Auchinyell Road. It caters for a large number of students attending the University as well as residents within the area. This service runs every 12 minutes throughout the peak travel times, on average 12 vehicles an hour. These buses, although exempt from the proposed restriction, form a significant percentage (approx 50%) of the recorded HGVs.
- 5.7 This road also has an important part to play in the recently formed strategic emergency traffic management plan, **“Major Incident - Anderson Drive - Diversion Plan”**. This plan results from work carried out with the emergency services, and Bear (Scotland) to identify diversion routes during major incidents on the A90(T), (North of the Bridge of Dee). These routes are required to ensure the free flow of traffic should the A90(T) be closed at any point along its length.
- 5.8 Broomhill Road forms a crucial part of a number of these diversion routes and as such is currently required to take HGVs from the A90, in both directions, should a major incident occur.
- 5.9 There are a number of vehicles currently using the Broomhill Road, Great Southern Road route south during the evening peak hours as a legitimate alternative route to avoid the congestion at the South Anderson Drive / Holburn Street / Garthdee Road roundabout. Of these vehicles, a percentage are HGVs, however, the numbers are relatively low in comparison to the total number of HGVs taking access to Broomhill Road to serve local premises.
- 5.10 To quantify the above information an additional, more detailed, sample survey was carried out on the 26th January this year. Wireless CCTV cameras were installed at both ends of Broomhill Road, one at Holburn Street looking towards Anderson Drive and one at the Anderson Drive roundabout end looking towards the city centre.
- 5.11 These cameras recorded all vehicles entering Broomhill Road, in both directions, over a 5 hour period, 8:00am to 1:00pm, on a Thursday morning. The weather conditions were good for the time of year and the temperatures moderate. There was no snow or ice on the road surface. The results of this survey are detailed in the table below:

BROOMHILL ROAD

Survey period: Thursday morning 8:00am to 1:00pm

	Total vehicles (averaged)	Total no of HGVs (including buses)	No of buses	No of HGVs	% buses	% HGVs	* HGVs taking access to the area as a % of the total vehicles	HGV travelling the full length of Broomhill Road as a % of the total vehicles
Eastbound	1347	73	35	38	2.6	2.8	1.2	1.6
Westbound	1009	52	24	28	2.4	2.8	0.6	2.2

* These HGVs were recorded as entering Broomhill Road at either end and not emerging at the other end. It was assumed that these vehicles were visiting premises on this street or using Broomhill Road as a means to accessing other streets in the vicinity.

5.12 The result detailed in the above table clearly shows a small percentage of HGVs using Broomhill Road as a through route, 1.6% eastbound and 2.2% westbound. In considering the low level of HGV traffic utilising Broomhill Road, officers do not feel that a restriction of HGV vehicles could be justified.

5.13 Further options to reduce the HGV movements along this road have been considered in line with the Committee's instruction.

5.14 There are four options that could potentially be introduced, two of which have already been discussed in the previous report and a further two that were not previously highlighted. In all cases there are potential problems with enforcement of the restriction.

5.14.1 "Weight restriction – except for access" – The vehicle weight must initially be confirmed to establish whether the restriction applies to that vehicle. With the exemption of "Except for access" being applied Police must then also establish whether legitimate access was being taken if the weight restriction applies. As there are no public weighbridges in the City Grampian Police have to use private weighbridges to enforce any weight restrictions that currently exist. These weighbridges are several miles away from the location in question. This would involve a considerable amount of travel and investigation work.

5.14.2 "Width restriction – except for access" – this restriction is slightly easier to enforce as, unlike the vehicle weight, the vehicle width is easily established. However identical problems will occur with establishing the right of the vehicle to be on this road taking legitimate access if the width restriction is exceeded.

5.14.3 Timed "Weight/Width restriction – except for access" – as for the above restrictions but limiting the restriction to times that are deemed to cause the greatest inconvenience

- 5.15 The above restrictions can only be enforced by Grampian Police through a police presence. There are no reasonable means of self-enforcement. For such restrictions to be adhered to there would be required a considerable investment of police resources. Failure to adequately enforce would render the restriction ineffective and contribute to bringing these types of restrictions into disrepute. Grampian Police have submitted a statement of their views on the possibility of introducing an HGV restriction to Broomhill Road. The statement is attached as **APPENDIX A**.
- 5.16 Should a restriction be introduced there would be an expectation from the residents that they would see a notable reduction in the numbers of HGVs on the street. Referring to the most recent survey this would be unlikely as the numbers that would not legitimately be able to use this road are very small and could well be within normal daily variations. In such circumstances it is likely that residents will not perceive a change in the level of HGV movement on Broomhill Road.
- 5.17 As with similar restrictions elsewhere in the city, restricting HGV access along one street results in drivers choosing alternative routes. It would be comforting to think that these drivers would then choose to use the trunk road and join the queue from the Holburn Street/South Anderson Drive/Garthdee Road roundabout and accept the extended journey time. The reality, however, is likely to be different and it would be counter-productive if these drivers then choose to use roads less suitable than Broomhill Road in a further attempt to reduce journey times.
- 5.18 This issue of an HGV restriction is reported to stem mainly from the desire of drivers, (in particular HGV drivers), to avoid traffic congestion at the Holburn Street/South Anderson Drive/Garthdee Road roundabout. It is worth noting that the traffic modeling, carried out as part of the assessment of the Western Peripheral Route, suggests a substantial reduction in the volume of traffic using the A90 and in turn the congestion at this roundabout. With reduced congestion it is reasonable to expect that there will be less need to find alternative routes whilst travelling south.
- 5.19 In summary, Broomhill Road plays a major role in the hierarchy of the roads network in Aberdeen. It serves as a distributor road providing access to the retail and commercial section in the centre of the city. It therefore services the access needs of HGV traffic at an appropriate level. A restriction on HGV access is likely to have far-reaching effects on the surrounding roads network and create problems elsewhere that will require further interventions.
- 5.20 Taking the above into consideration and also the more detailed survey results now available, it is recommended that no further action is taken with regard to any of the options put forward for Broomhill Road.

6. IMPACT

- 6.1 The content of the report meets with the local Community Plan objectives to continually improve road safety and maximize accessibility for all modes of transport

7. BACKGROUND PAPERS

N/A

8. REPORT AUTHOR DETAILS

Ruth Milne
Technical Officer,
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(01224) 538052

Consultees comments

Councillors

Callum McCaig - has been consulted

Councillor Ian Yuill - *Has been consulted and does not agree with the recommendations. He remains of the view Broomhill Road is not suitable as a through route for HGVs.*

Council Officers

Ciaran Monaghan – Head of Service, Office of Chief Executive – has been consulted

Barry Jenkins, Head of Finance, Resources – *has been consulted and has no comments*

Jane MacEachran, City Solicitor, Continuous Improvement - has been consulted

Gordon McIntosh, Director of Enterprise, Planning and Infrastructure – has been consulted

Hugh Murdoch, Head of Service, Shelter and Environment – has been consulted

Margaret Bochel, Head of Planning & Infrastructure, Strategic Leadership – *has been consulted and is in agreement with the recommendations within this report*

Mike Cheyne, Roads Manager - has been consulted

Dave Young, Account Manager, Service, Design and Development - has been consulted

Anne Ross, Service Co-ordinator

Allison Swanson, Committee Services

From: "McDonald, Mike" <Mike.McDonald@grampian.pnn.police.uk>
To: <Ruth.Milne@aberdeencity.gov.uk>
Date: 19/04/2012 15:00
Subject: Traffic Management Proposals - Broomhill Road, Aberdeen

NOT PROTECTIVELY MARKED

Response from;

Mike McDonald

Traffic Management Manager

Grampian Police

Nelson Street,

Aberdeen

AB24 5EQ

I refer to the above subject and understand that the Local Policing Team (LPT) Inspectors are aware of historic dialogue and an informal proposal by the local Community Council on behalf of a small number of members, to restrict the number of Large Goods Vehicles (LGV's) using the Broomhill Road corridor. However it is my understanding there are no records to be found which would support wider community concerns in this regard. I am also aware of the Aberdeen City Council Road safety and Traffic Management Unit's current position on the subject

Grampian Police is an evidence-based organisation and invariably seeks to gather evidence to support the justification for taking, or in some cases for not taking action.

Through research I have established the collision history for Broomhill Road, which shows 16 road traffic collisions have occurred there over the past 5 years. These were all "damage only" collisions and of the 30 or thereby, vehicles involved, all were motor cars with the exception of 3 buses and 1 LGV, the circumstances of which indicate that the driver was not personally at fault. From a road safety and also an intelligence-led perspective, it is my view that 16 minor, non injury collisions recorded over a 5 year period is almost insignificant.

Despite the presence of signage or other traffic management measures, any restriction proposed to limit the weight or width of vehicles using Broomhill Road will require to be enforced by Grampian Police. If this restriction was to be approved, there would be an expectation from local residents that it would be regularly enforced.

In relation to LGV's currently using Broomhill Road, I am advised there are naturally daily variations in their numbers. Many of these vehicles would have a valid reason for being there and it is unlikely that, given the number of LGV's that would currently access Broomhill Road legitimately, any reduction through enforcement is likely to be minimal.

Given the apparent lack of evidence to justify such a restriction, competing demands and limited Police resources would invariably mean that enforcement of any width restriction, is likely to become a low priority.

In practical terms, the enforcement of legislation with an "except for access" exemption, is notoriously difficult to enforce, and

would be so in respect of a location like Broomhill Road. Experience from similar situations in other areas indicates this would be challenging to police effectively. For example most LGV drivers, if challenged, would be able to provide a perhaps dubious but otherwise valid reason or an acceptable excuse for being there. Therefore any restriction would become almost unenforceable.

Again, if this route was restricted, would it not simply displace the LGV "problem" to another route in the same or possibly the neighbouring area? If so, it is likely we would then see the local Councillor for that Ward petition to have the LGV issue, which would be created there, displaced again and cite Broomhill Road as an example and reason for doing so.

Such action has the potential to create an unwelcome ripple effect in respect of traffic management across the West End and in the strongest terms, this is a precedent we do not want to set.

It is also our belief that if an LGV restriction was implemented on Broomhill Road, displaced traffic would probably use Great Western Road, Cromwell Road, Union Grove or even Queens Road. Great Western Road is an area around which Local Councillors already have concerns, particularly in respect of the junction with Anderson Drive (turning right against the red light and pedestrian phases from Great Western Road). Therefore additional displaced traffic volume at this junction would not be helpful.

Grampian Police appreciate constituents and members of the Community Council may not welcome large goods vehicles or buses using the thoroughfare outside their homes but whilst understanding their position, if a resident makes the choice to live along a busy urban street, such traffic using the carriageway outside their property, should not be unexpected.

Since 2007 there has been no record of correspondence through the Road Policing Department relating to LGVs or buses creating traffic management issues or traffic noise on Broomhill Road. Further research via the other recording systems within Grampian Police also failed to identify any related complaints.

There is little doubt that when the AWPR is finally built, and it is appreciated that may be some time away, traffic pressure on a great many roads in the city, including Broomhill Road, will be alleviated to some degree.

In conclusion, and having consulted the LPT Inspectors, past and present, the Road Policing Inspector for Aberdeen City and the Force Head of Road Policing, it is the position of Grampian Police that we would not be in favour of such a proposal and fully support the Aberdeen City Council Road Safety and Traffic Management Unit view.

Mike McDonald

Traffic Management Manager

19 April 2012

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	31st May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Notice of Motion from Councillor Yuill – ‘That given the significant inconvenience and difficulties caused to householders and others by bollards opposite the entrance to Deeside Drive to the rear lane lying between the rear of properties on Deeside Drive and Deeside Crescent, Council instructs officers to report to the appropriate committee on the removal of these bollards.’
REPORT NUMBER:	EPI/12/108

1. **PURPOSE OF REPORT**

This report provides further information to members of the Committee following their request, at the meeting on 31st January 2012, for officers to explore alternative solutions to the difficulties experienced by the bollards opposite the entrance to Deeside Lane with regard to Councillor Yuill’s notice of motion.

2. **RECOMMENDATION(S)**

It is recommended that the Committee:

1. Note the content of this report and the proposals that officers have considered.
2. Agree that no further action should be taken with regard to the removal of the bollards as this cannot be justified due to the costs of the associated works.

3. **FINANCIAL IMPLICATIONS**

3.1 There are no financial implications as a result of the recommendation from this report.

3.2 Should the bollards be removed, funding would require to be made available and a source identified. An estimate for the total costs for each proposal provided by officers lies in the range from £19,000 - £22,000, details of which can be found in section 5.4.

4. OTHER IMPLICATIONS

None.

5. BACKGROUND/MAIN ISSUES

5.1 Background

At its meeting on 13th September 2011 the Enterprise, Planning and Infrastructure committee considered a notice of motion raised by Councillor Yuill "That, given the significant inconvenience and difficulties caused to householders and others by bollards opposite the entrance to Deeside Drive to the rear lane lying between the rear of properties on Deeside Drive and Deeside Crescent, council instructs officers to report to the appropriate committee on the removal of these bollards". The committee instructed officers to report on the terms of this motion to a future meeting of the Enterprise, Planning and Infrastructure Committee.

A report was presented to the E P & I Committee on the 31st January 2012 at which time the Committee instructed officers to consider other options and their respective merits and to report back.

- 5.2 There are a number of properties on Deeside Gardens and Deeside Crescent that back onto this lane where the majority of garages exit onto the lane. During the winter period these residents encounter difficulty exiting onto Deeside Drive via a parallel inset road as it receives limited treatment during periods of severe inclement weather and is on a steep gradient of approximately 1 in 12 uphill towards North Deeside Road, similar to other roads to the west of this location.

There is currently a footway with two bollards at the end of this lane between Deeside Drive and the inset road (see plan Appendix 1). The footway is of standard for pedestrians and not intended for vehicles. Should it be used by vehicular traffic it would require to be strengthened and upgraded to carriageway standards with additional works carried out to protect to the existing British Telecom services..

Following an approach by residents, Councillor Yuill requested that the bollards situated on this footway be removed to allow residents to exit directly onto Deeside Drive at the end of this lane. The removal of the bollards on this footway raises concerns that vehicles would proceed from the lane over this footway causing possible damage to the underground telecom cables and presenting a road safety hazard by bringing vehicles into conflict with pedestrians, who will not be expecting to encounter any vehicles crossing a footway at this point.

- 5.3 Officers have reviewed the range of options available and have listed them in detail below.

Proposal 1 – Formalised Junction

To allow vehicles to enter / exit at this point would require the junction to be realigned and depth of construction upgraded to the standards of a carriageway. Details are shown in Appendix 2. This work would require the footway to be widened into the grass verges and would have a cost implication with an estimated cost for the works of £10,000.

Proposal 2 – Single Lane Exit

To allow vehicles to exit only out of Deeside Lane directly onto Deeside Drive as above would require a single lane junction to be created and depth of construction upgraded to withstand vehicular traffic. Details are shown in Appendix 3. This work would require construction into the existing grass verges with a realigned kerb line reconstruction. The estimated cost for the works is £7,000.

This option would incorporate a short section of one-way which would require a Traffic Regulation Order that would take up to 9 months to promote and implement. It should be noted that given experience with short sections of one-way in remote locations it is likely to be abused.

Officers considered a third proposal which was to create a new section of carriageway either north or south of the existing bollards. However this was rejected due the number and location of the trees within each verge and that there are further public service boxes located at each of these locations. This proposal would still encounter the similar construction costs and an additional sum for removing the mature trees. It would also fail to address the current problems faced by residents during the winter period with vehicles having to endure the steep gradient on the inset road.

5.4 Services

Within the existing footway and surrounding grass verges there is British Telecom services and a concrete access chamber. British Telecom have identified that the reconstruction of the footpath to a carriageway standard would require the replacement of the service chamber to a carriageway box standard, and due to the shallow depth of the BT duct and cabling it would be necessary to lower and protect the services with a reinforced concrete mat. This work would have an additional cost implication with an estimated cost for this works of £9,000.

- 5.5 The above options do not include the associated costs of signing and lining work required with the construction of a new junction, The estimated cost for the associated signing and lining is £3,000.

Therefore the total costs of each of the proposals are as follows:

- Proposal 1 Formalised Junction = £22,000
- Proposal 2 Single Lane Exit = £19,000

5.6 Conclusion

The problems of access and egress for residents of Deeside Gardens and Deeside Crescent are only experienced during relatively short periods during periods of inclement winter weather.

Whilst the difficulties of the residents is understood they do have the option to park either on Deeside Gardens / Drive and Deeside Crescent during the short periods of severe inclement weather.

With the current budget constraint it is felt that the solutions identified are to address short term problem that can be managed by the residents through the use of alternative parking practices. Officers are therefore of the view that the works are difficult to justify at this time.

6. **IMPACT**

- 6.1 The content of the report meets with the local Community Plan objectives to continually improve road safety and maximize for pedestrians and all modes of transport.

7. **BACKGROUND PAPERS**

Minute of Enterprise, Planning & Infrastructure Committee meeting, 13th September 2011.

<http://councilcommittees/mgConvert2PDF.aspx?ID=1908&T=1>

Minute of Enterprise, Planning & Infrastructure Committee meeting 31st January 2012.

<http://councilcommittees/mgConvert2PDF.aspx?ID=2283&T=1>

8. **REPORT AUTHOR DETAILS**

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Consultees Comments

Local Members

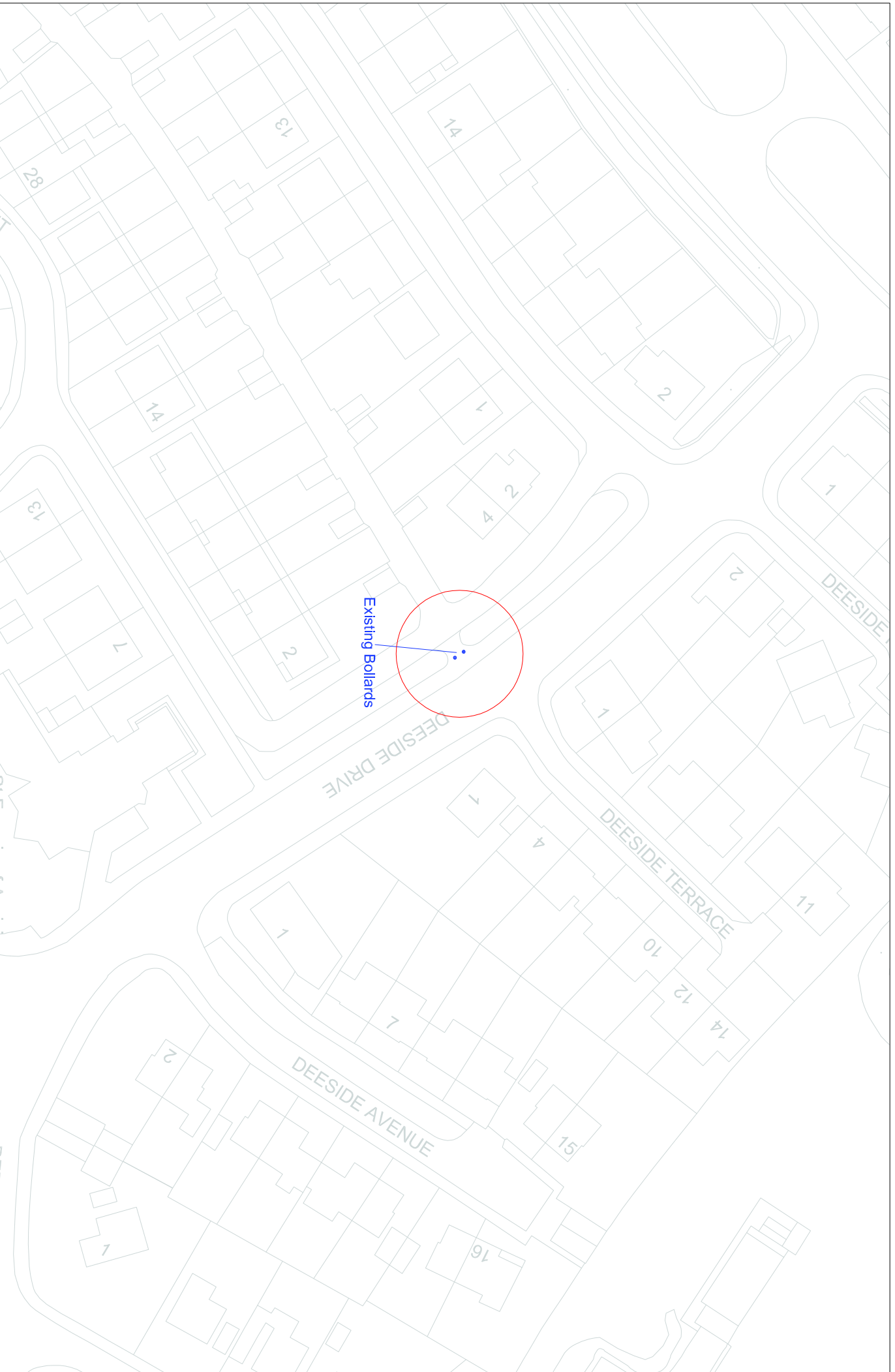
Councillor Ian Yuill	Email sent 26/04/2012 and does not agree with the recommendations made by Officers. Councillor Yuill would also like to highlight that this report only addresses the issue of access in bad weather, with there being another problem of since the bollards were installed residents have been unable to receive deliveries via the rear of their properties as the delivery vehicles can't turn from the service road into the lane.
Councillor Callum McCaig	Email sent 26/04/2012

Council Officers

Barry Jenkins, Head of Finance, Resources – <i>has been consulted and has no comments</i>
Jane MacEachran, City Solicitor, Continuous Improvement – <i>has been consulted</i>
Ciaran Monaghan, Head of Service, Office of Chief Executive – <i>has been consulted</i>
Gordon McIntosh, Director of Enterprise, Planning and Infrastructure – <i>has been consulted</i>
Hugh Murdoch, Head of Asset Management and Ops – <i>has been consulted</i>
Margaret Bochel, Head of Planning & Sustainable Development – <i>has been consulted and has not further comments</i>
Mike Cheyne, Roads Manager – <i>has been consulted</i>
Neil Carnegie, Community Safety Manager – <i>has been consulted</i>
Dave Young, Account Manager, Service Design & Delivery – <i>has been consulted</i>
Anne Ross, Service Co-ordinator
Allison Swanson, Committee Services

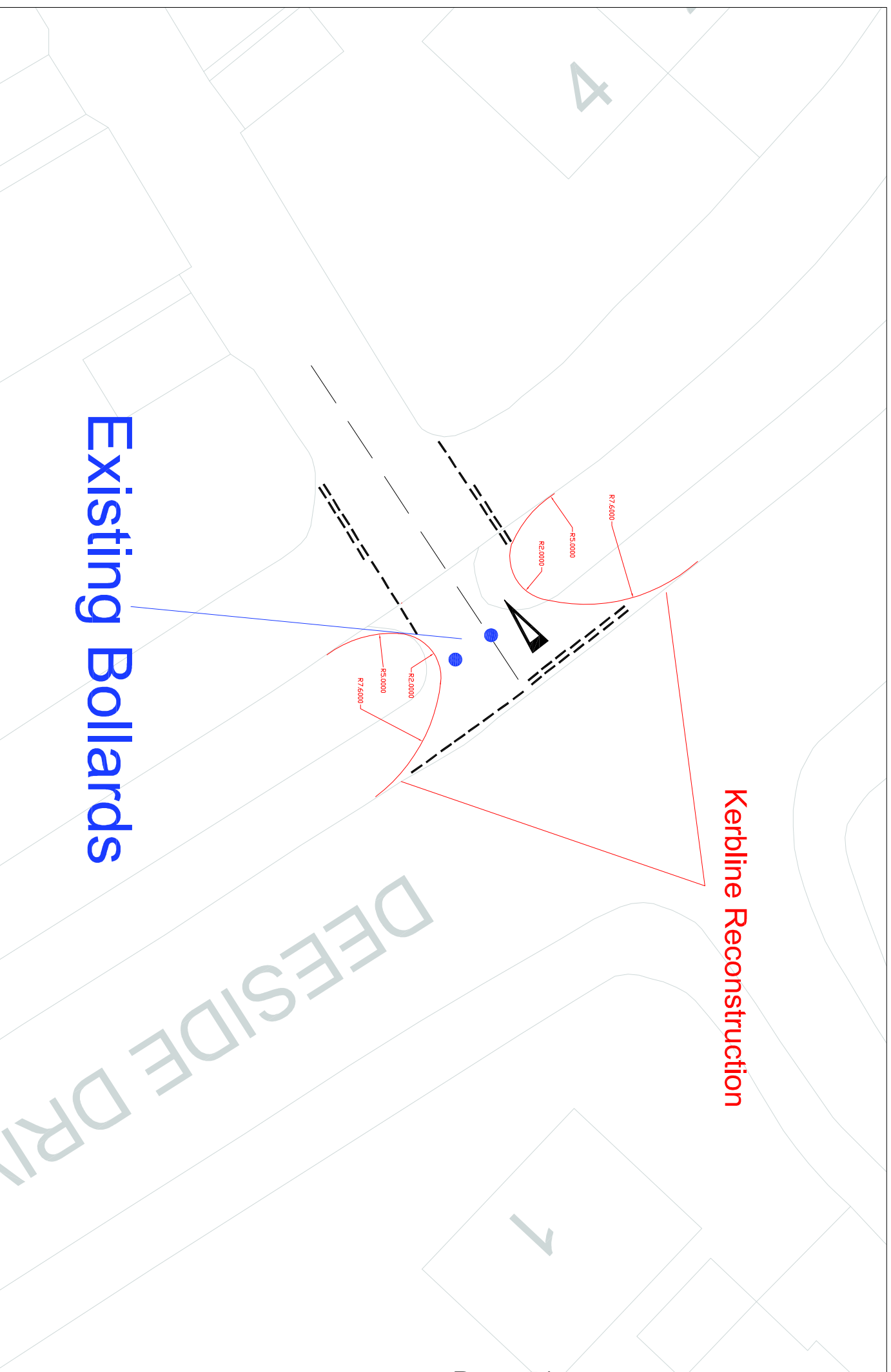
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Deeside Drive - Existing bollards



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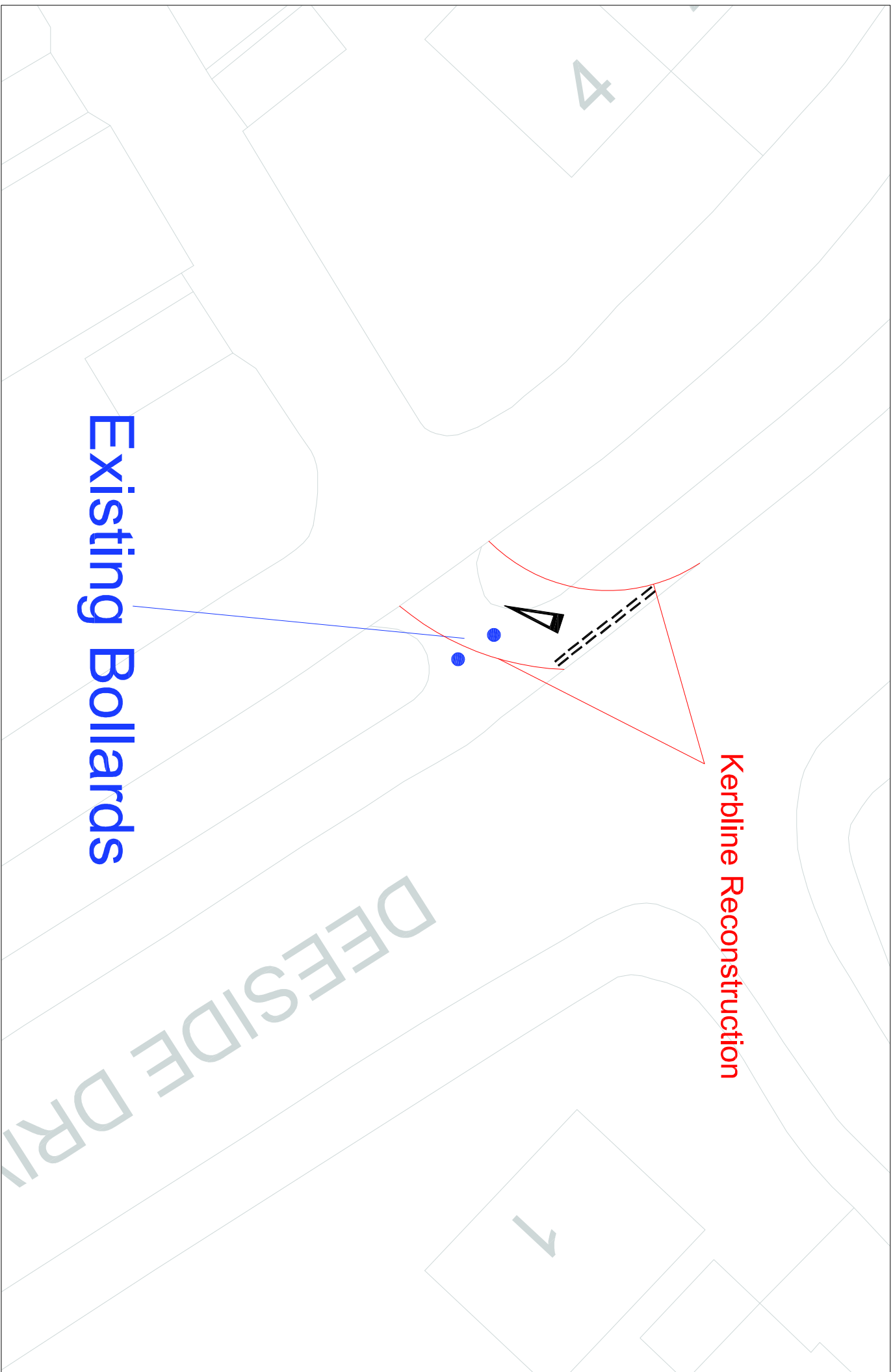
Deeside Drive - Existing bollards



Existing Bollards

Kerblines Reconstruction

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Notice of Motion from Councillor Gordon Graham – “That the officers consult with the residents of Bonnyview Drive, Aberdeen to ascertain the requirement to install traffic calming measures and reports back to Committee.”
REPORT NUMBER	EPI/12/107

1. PURPOSE OF REPORT

- 1.1 This report provides information to members of the Committee in relation to Councillor Graham notice of motion regarding the implementation of speed cushions and mandatory 20mph speed limit at Bonnyview Drive.

2. RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:-
1. Notes the content of this report and the speed surveys carried out by officers; and
 2. Instructs officers, given the findings of the speed survey, not to consult with residents and that no further action is to be taken.

3. FINANCIAL IMPLICATIONS

- 3.1 None

4. OTHER IMPLICATIONS

- 4.1 None at this stage of the process

5. BACKGROUND/MAIN ISSUES

- 5.1 At its meeting on 31 January 2012 the Enterprise, Planning and Infrastructure committee a notice of motion by Councillor Graham on the following terms was considered: -

“That the relevant department consults with the residents of Bonnyview Drive to ascertain the requirement to install traffic calming measures and reports back to committee”.

In turn, this committee instructed officers to conduct traffic surveys on Bonnyview Drive and report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee.

5.2 The road is residential in nature which links Longview Terrace, Bonnyview Place and also connects Howes Drive and Howes Crescent. Vehicles using this road are subject to an advisory 20mph speed limit.

5.3 The volume and speed of vehicles was assessed at two straight sections of Bonnyview Road, section 1 and section 2 (Please see the enclosed plan for exact location).

The following table gives an indication of the volume of traffic using Bonnyview Road on an average working day. The data within the table was collected in March 2012.

7am to 7pm (12 hour period)		8am to 9am (am peak)		4pm to 6pm (pm peak)	
Southbound	Northbound	Southbound	Northbound	Southbound	Northbound
392	297	44	09	70	81

5.4 The following table gives further indication of speeds of vehicles using both sections of the road for a week period.

Location	85 % of vehicular speeds		Average speeds	
	Southbound / Westbound	Northbound / Eastbound	Southbound / Westbound	Northbound / Eastbound
Section 1	27mph	30mph	22mph	25mph
Section 2	24mph	22mph	17mph	17mph

**The 85 percentile speed is the speed at which 85% of the total number of recorded vehicles was travelling at or below.*

5.5 Circular 6/2001 was published by the Scottish Executive in 2001 and provided local authorities with guidance on 20 mph zones and both mandatory and advisory 20 mph speed limits (an explanation for mandatory 20 mph limits and advisory 20 mph limits are located in Appendix A). The guidance on advisory 20 mph speed limits offered recommendations on suitable areas where they may be introduced

which included such as no significant accident history; self enclosed areas used mainly by local residents with little or no through traffic,

- 5.6 An inspection of the accident history for Bonnyview Drive shows a total of 3 damage only accidents occurring, during the three year period from January 2009 to December 2011. It should be noted that the Bonnyview Road layout and its environment does not encourage vehicular speeding. Moreover, the vehicular parking and narrow width of section 2 of this road act as natural traffic calming and reduce vehicular speeds. As can be appreciated from the data, it is evident on section 1, where there is lay-by parking, vehicular speeds are slightly higher than in section 2, but still within acceptable limits for an advisory 20mph limit.
- 5.7 In conclusion, as there is a low damage only accident record and the recorded speeds are within an acceptable range, it is recommended that consultation with residents is not necessary, as no further traffic calming measures are proposed.

6. IMPACT

- 6.1 No impact.

7. BACKGROUND PAPERS

- 7.1 Minute of Enterprise, Planning & Infrastructure Committee meeting, 31 January 2012.
- 7.2 SEDD Circular No.6/2001 – Development Department Transport Division 3 – Scottish Executive

8. REPORT AUTHOR DETAILS

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Appendix A

Explanatory Note for 20mph zones, Mandatory 20mph etc

Mandatory 20 mph speed limits –

The following quote from the guidance is of particular relevance: - “Authorities may regard mandatory 20 mph speed limits without relatively expensive speed reduction features as an attractive option, but such limits should not be introduced where there is no realistic expectation that they will achieve the required decrease in traffic speeds, or where the police are unable to give an undertaking to provide an effective level of enforcement”. In situations where speeds are low there is little benefit in introducing a mandatory limit simply to provide a “feel good factor”. It is not considered best value to spend limited resources on providing such limits where speeds are already low (for example in town centres).

Advisory 20 mph limits –

Local authorities should exercise caution where 85th percentile speeds are higher than 30mph – some traffic calming to support the advisory speed limit is recommended in such areas. An advisory 20mph speed limit is not considered appropriate where the legally enforceable speed limit is higher than 30mph.

Consultees comments

Lead Councillor Callum McCaig - Email sent 26/04/2012

Local Members

Councillor Gordon Graham Email sent 26/04/2012

Council Officers

Barry Jenkins, Head of Finance, Resources – ***has been consulted and had no comments relating to finance.***

Jane MacEachran, City Solicitor, Continuous Improvement - ***has been consulted***

Ciaran Monaghan, Head of Service, Office of Chief Executive - ***has been consulted***

Gordon McIntosh, Director of Enterprise, Planning and Infrastructure - ***has been consulted***

Hugh Murdoch, Head of Service, Shelter and Environment – ***has been consulted***

Margaret Bochel, Head of Planning & Infrastructure, Strategic Leadership – ***has been consulted, had no comments and agreed with the recommendations***

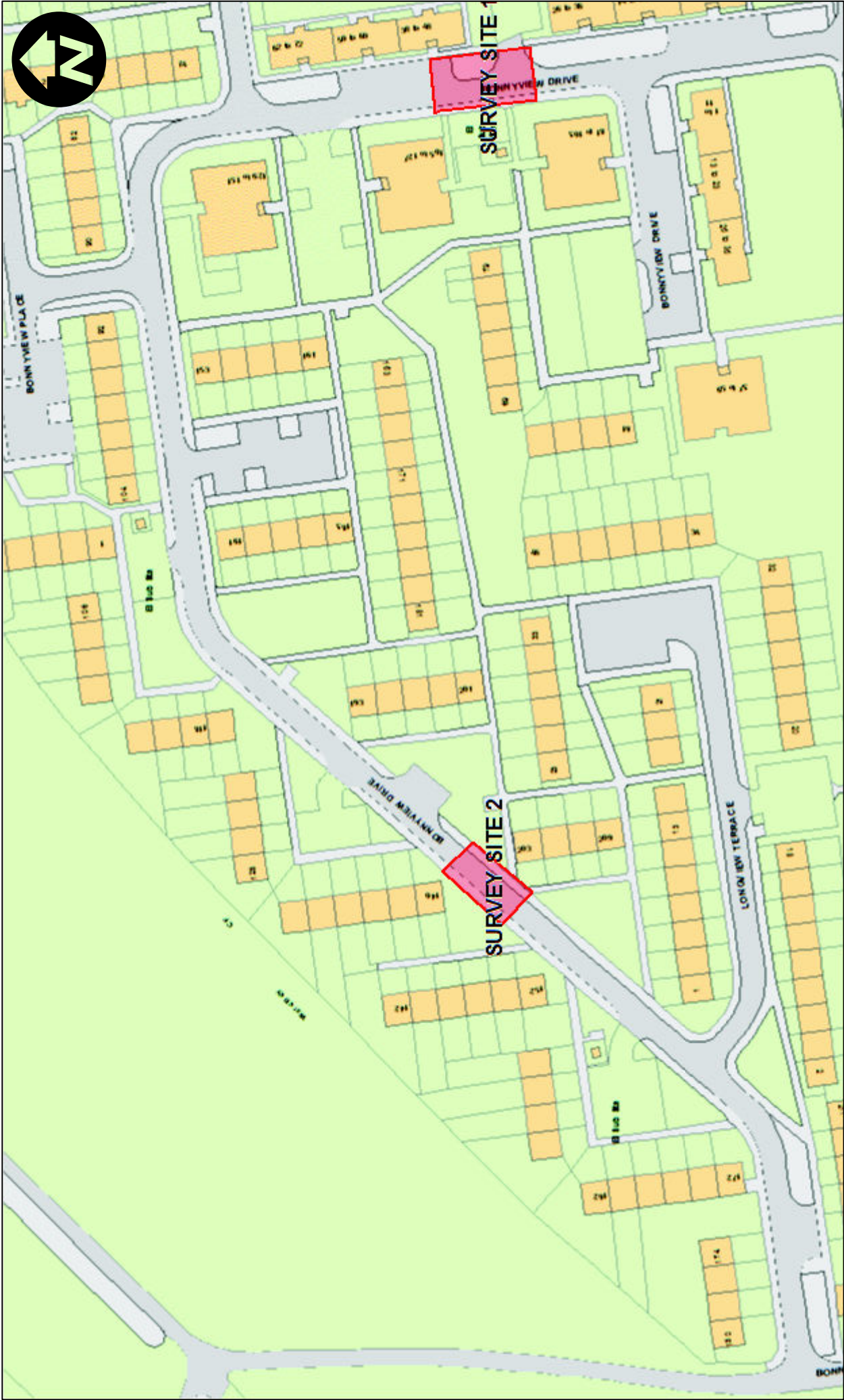
Mike Cheyne, Roads Manager - ***has been consulted***

Dave Young, Account Manager, Service, Design and Development - ***has been consulted***

Kathryn McFarlane, Service Co-ordinator

Allison Swanson, Committee Services

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100 Meters
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BONNYVIEW DRIVE/SPEED AND VOLUME SURVEY
APPENDIX A

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ABERDEEN CITY COUNCIL

COMMITTEE : Enterprise, Planning and Infrastructure

DATE : 31 May 2012

DIRECTOR: Gordon McIntosh

TITLE OF REPORT: Marischal College – Broad Street Access

REPORT NUMBER: EPI/12/089

1. PURPOSE OF REPORT

This report has been produced in response to a motion presented at Enterprise, Planning and Infrastructure committee (31 Jan 2012) on Broad Street access arrangements for mobility scooters.

2. RECOMMENDATION(S)

It is recommended that committee:-

Agree that as an operational matter the gate between the main structure and the pend column be opened during business hours as an 'alternative' straight-on means of access and that, subject to obtaining relevant consent an additional 'left hand operation' push pad be mounted on the main structure wall at an appropriate distance from the access door. In addition, consideration could be made to rehand the existing gate to minimize any perception of an obstruction. This constitutes minimal intervention and minimal cost for minimal need. See photograph hereunder:-



3. FINANCIAL IMPLICATIONS

Minimal and contained within existing capital budget.

4. OTHER IMPLICATIONS

None

5. BACKGROUND/MAIN ISSUES

5.1 The motion has put been in the following terms, "That officers are instructed to prepare a report on proper access for disabled people using mobility scooters at Marischal College, and such a report be remitted to the appropriate Committee." The subject of the motion relates specifically to access/egress via the Broad Street entrance to Marischal College.

5.2 Marischal College is a grade A listed building and as such was subjected to, not only attaining Planning Consent but also Listed Building Consent via Historic Scotland. Planning consent, et al, was applied for and is referenced as application A8/0898-9. Any intervention requires to be viewed in the context of mitigation of impact upon internal environmental control and the listed building status of Marischal College and consents for the existing configuration.

5.3 Aberdeen Access Panel/Aberdeen Disability Advisory Group

5.3.1 The Aberdeen Access Panel, Aberdeen Disability Advisory Group commented on the detail of the application via letter dated 12 June 2008. The comments received are noted below (with original reference numbers):-

(6) Powered doors should be controlled by either an automatic sensor, such as motion detector, or by manual activation device, such as a push pad. Any manual control should be located at a height between 750mm and 1.0m above ground level and at least 1.4m from the plane of the door, or where the door opens towards the direction of approach, 1.4m from the leading edge of the door leaf. A manual control should contrast visually with the surface on which it is mounted.

(8) All principal entrance doors must contain a leaf which provides a clear opening width of at least 800mm and have an unobstructed space on the side next to the leading edge of at least 300mm except where the doors open via automatic controls.

(11) Where the principal accessible entrance comprises a revolving door, there must be provided an adjacent side hinged or automatic door.

5.3.2 Holmes Partnership, the appointed architect and lead design consultant, issued a written response to the observations of the Aberdeen Access Panel, Aberdeen Disability Advisory Group dated 03 November 2008. This document contained the following:-

(6.0) There will be a powered door at the main entrance to Broad Street. The Broad Street door will be activated by a manual activation device. Controls will be located between 750mm and 1000mm

(8.0) All principal entrance doors will have a clear opening of at least 800mm with an unobstructed 300mm clear space to the leading edge.

(11.0) The revolving door to Broad Street is supplemented by 2 powered doors, immediately to either side.

- 5.3.3 The Disability Advisory Group met on 04 February 2009. At item 5 of the minute the Marischal College Development was discussed and recorded as follows:-

'The Group had before it a note dated 03 November 2008, prepared by the Holmes Partnership, Architects for the Marischal College redevelopment, setting out responses to observations and recommendations raised by the Disability Access Group'

It goes on to record: 'The external glass doors on Broad Street will be operated by a push pad. Automatic doors cannot be installed because of the wind levels. Concern was expressed by members of the Group with visual impairment that it can be difficult to locate the push pads and the point was also made that guide dogs are not trained to negotiate revolving doors. In response, the Group was advised that Council staff will always be on hand at the main entrance to assist visitors'

Finally the Group resolved: '(ii) that dialogue with the Group and the Access Panel be maintained; and that in particular any proposed changes to the design of the development be highlighted at an early stage'.

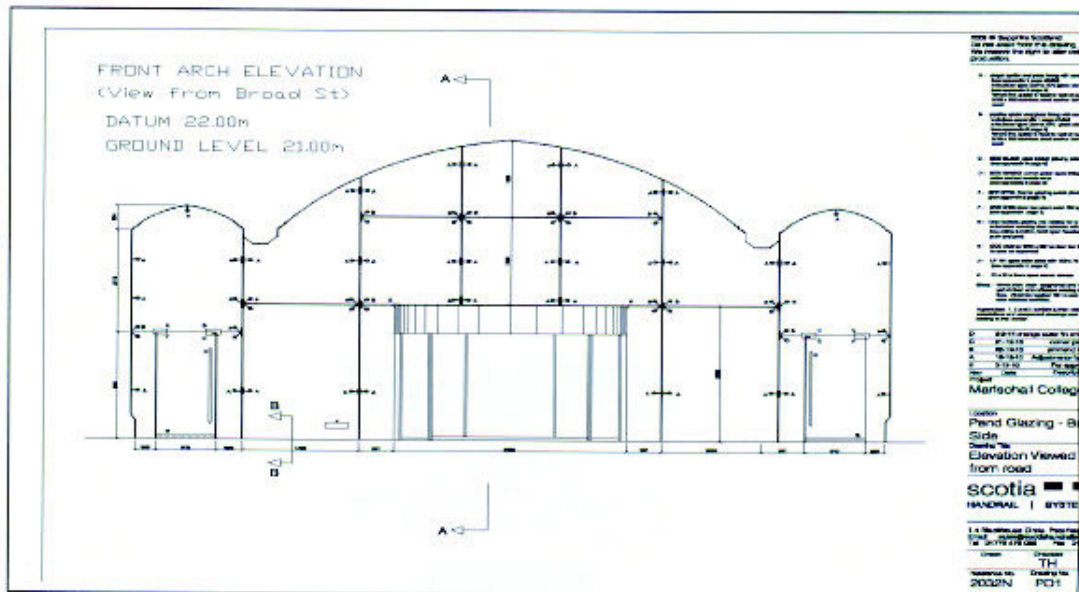
Based on the above it is contended that the design proposals had been accepted by the Group.

5.4 Planning/Listed Building Consent

- 5.4.1 Planning and Listed Building Consent was duly obtained and at item reference LBC.2 of said consent the following condition was stipulated:

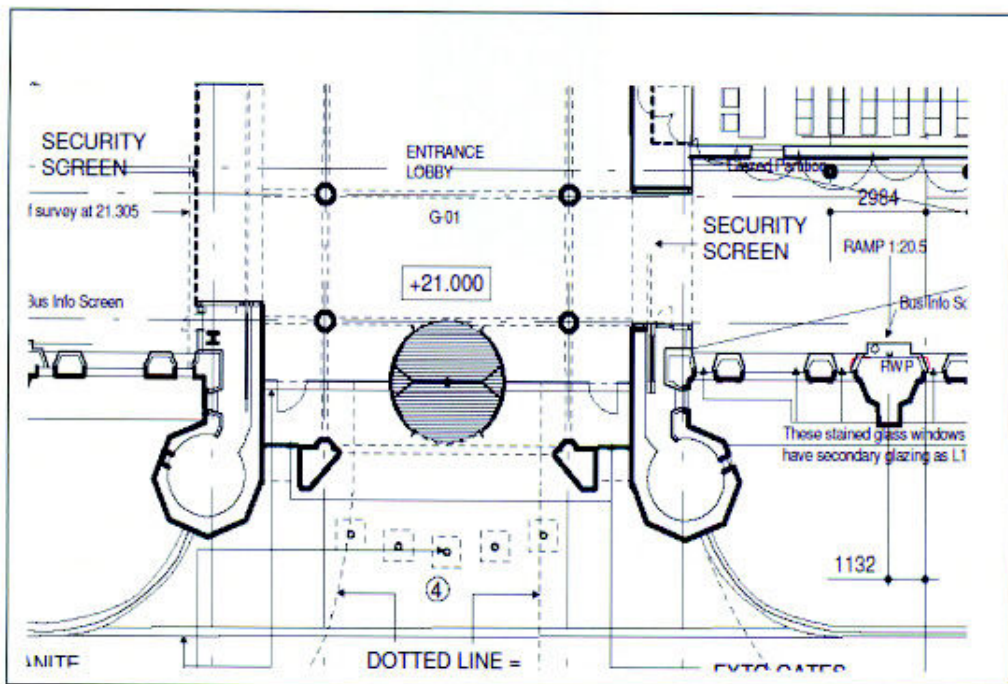
'No construction in respect of the glazing of the courtyard entrance pend shall take place unless fabrication details of the glazing system and its fixings have been submitted to Historic Scotland, and approved in writing by the Planning Authority. Such details include specifications and/or details of any letterboxes, intercoms etc, that will be fixed to the glazing, and that are required for the operation of the building, and the work shall be carried out in accordance with the details so approved – in the interests of the character and appearance of the listed building.'

This condition was duly purified and the approved glazing detail is as under:-



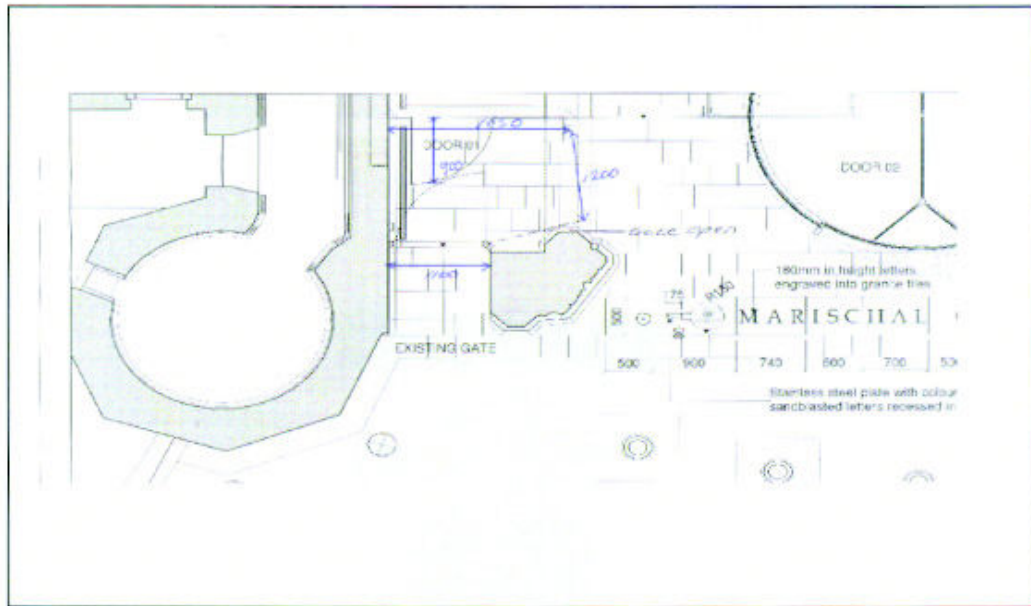
The disabled access door is the single leaf entrance to the left, the single leaf access to the right is a 'fob' controlled staff entrance.

What is not illustrated in the above is the position of the granite columns and side entrance 'gates'. This relationship can be seen in the drawing below:-



The above configuration has laterally given rise to the expressed concern i.e. that approach to the door is parallel to the plane of the door which after activation of the push pad requires the disabled user to make a right hand turn through the door entrance.

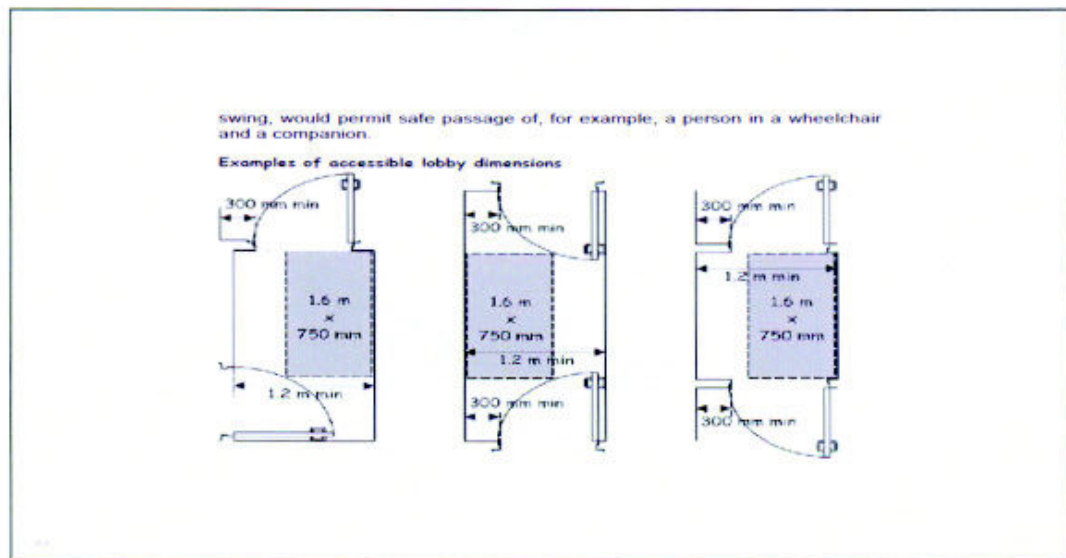
A more detailed representation of this issue is as shown below. Also indicated is the 'gate' in an open position.



Dimensions indicated (open door):-

- (1) Structural opening by existing gate = 1100mm
- (2) Access opening towards door (with gate in open position) = 1200mm
- (3) Door size = 900mm
- (4) Structure to push pad = 1650mm

5.4.2 Building Standards recognises that for non-domestic buildings, at section 4.2, that lobby arrangements can accommodate a right hand turn subject to the configuration below:



The left hand diagram indicates compliant construction in terms of making a right angle turn.

5.5 Trial

5.5.1 Notwithstanding all of the above background information Aberdeen City Council's Strategist (Disability, Faith & Race Equalities) conducted an informal trial which was witnessed by a representative of the Planning service of Enterprise, Planning & Infrastructure. His report on this exercise is stated hereunder verbatim:

'Together with a senior planner, Enterprise Planning and Infrastructure, and a motorised wheelchair user we conducted a trial of entering and exiting through the side opening door at the front of Marischal College.

The trial was successful in highlighting to the senior planner and myself the difficulties experienced by wheelchair users trying to negotiate the entrance. The first problem was that the push pad was located so close to the door opening that the feet supports of the wheelchair were within the arc of the door as it opened and obstructed the door opening so that when the pad was pressed the user then had to reverse smartly out of the way of the door to allow it to open.

The next problem was the manoeuvring space was barely sufficient to permit the chair to pass through the door opening. This was a distinct problem as the wheelchair user had to move very close to the glass panels in order to press the push pad and then had to move back and forth to get the proper swing room to get through the door. In fact, for someone not having attempted this operation a number of times to "practice" it by the time the chair is lined up properly, having hit the side of the glass panels a few times, the door began to close and hit the chair. This can cause the chair to become trapped by the door and would need an assistant to press the push pad to release the door from the chair. This can be alarming to a wheelchair user and also somewhat degrading as the element of independence is being removed from them.

The number and position of the push pads is also an issue. If a user of a motorised wheelchair has an ineffective right hand/arm as ours did, then operating the push pad was very difficult. Having a push pad on each side, thus allowing flexibility for users who have physical disabilities on one side would allow them the opportunity to use the pad on their good side.

The option that presented the best solution for access, and the one recommended, would be a door similar to the present side opening door but moved to a position immediately to the left of the revolving door, as viewed from the outside of the building, so that wheel chair users could approach the door in a straight line and have easy and clear access. Providing left and right push pads for users placed so that they stop outside the arc of the door opening is also recommended.

Finally, we were told of the problems of access for wheelchair users being dropped off from a car/taxi at the front of Marischal College. There is no dropped or lowered kerb in front of the entrance so when a wheelchair user is being dropped off by taxi that has a rear access door and ramp the wheelchair user is dropped down on to the road with no access up on to the pavement. This does not apply to side access door taxis as the wheelchair ramp is placed down on to the pavement. It was considered that a dropped kerb should be available in front of the entrance on Broad Street'.

5.5.2 A number of issues arise from this report:-

- (1) That the hitherto accepted design solution is not now seen as acceptable.
- (2) That the trial exercise is deemed as typical which is open to question.
- (3) It is acknowledged that use of the revolving door is inappropriate.

(4) The issue of the drop kerb has been rejected previously on grounds of road safety and provision is made on Queen Street for safe drop kerb utilisation.

(5) Installation of another door would, critically require Listed Building and Planning Consents and would represent a significant number of construction issues.

(6) Installation of another door would lend to further compromise of attempts to control the environmental conditions within the pend, and reception area i.e. staff and visitors would tend to routinely use this door.

5.6 Historic Scotland

5.6.1 Historic Scotland was consulted on the issue but at this juncture did not express a view relative to acceptability or otherwise on any issue related to reconfiguration.

6. IMPACT

An Equality and Human Rights Impact Assessment was undertaken and Is included with this report.


7. BACKGROUND PAPERS


Incorporated within the report.

8. REPORT AUTHOR DETAILS

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Agenda Item 6.1

CONTROLLED PARKING AREAS – WORKING PARTY

ABERDEEN, 3 February, 2012. - Minute of Meeting of the CONTROLLED PARKING AREAS WORKING PARTY. Present:- Councillor Corall, Convener; and Councillors Collie, Donnelly, Laing and Jennifer Stewart.

Apologies for absence had been intimated from Councillors John Stewart and Wisely.

Also in attendance:- Councillors Adam, Allan, Cormack, Greig, Robertson and Yuill.

FACT SHEET AND CURRENT CONTROLLED PARKING ZONE AREAS

1. The Working Party had before it a fact sheet which detailed the controlled parking zone areas, as well as all parking entitlement and charges for each of the available parking options both on and off street.

The Working Party resolved:-
to note the fact sheet.

PROPOSED REVISIONS TO ON STREET PARKING CHARGES FOR FORRESTERHILL AND GARTHDEE

2. Reference was made to article 43 of the minute of the meeting of the Enterprise, Planning and Infrastructure Committee of 31 January 2012, at which time it was agreed to refer, for discussion, the proposed revised charges for the Forresterhill and Garthdee on street parking zones. Members had before them on this day, the report as presented to the Enterprise, Planning and Infrastructure Committee.

The Working Party resolved:-
to request officers to:- (a) investigate the level of parking on streets in the vicinity of Forresterhill during the day; (b) provide further details on the availability of the existing voucher system utilised in the Forresterhill and Garthdee zones and to investigate ways to increase awareness and availability of the vouchers, as well information on other alternative systems which could be implemented in particular pay by phone; and (c) continue discussions with both NHS Grampian and RGU regarding the proposed new increased pricing structure for on-street parking, and to consult with local community councils and local business on this proposal also; and (d) to report back to the next meeting of the Working Party on these matters.

ALLOCATION OF BUSINESS PERMITS TO OFFICES WITH MULTIPLE OCCUPANCY

3. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which provided a summary of the current status of the allocation of business permits to offices and for offices with multiple occupancy; and sought members' views on this.

The report recommended:-

that the Working Party -

- (a) note the content of the summary report;
- (b) advise officers of their views on how to proceed in relation to the allocation of business permits to offices and for offices with multiple occupancy; and
- (c) instruct officers to investigate the procedures that other Local Authorities used to issue such permits and report back to the next meeting of the Controlled Parking Areas Working Party with a more detailed report.

The Working Party resolved:-

to approve the recommendations contained in the report.

CURRENT ANOMALY WITH THE PROCEDURE FOR ISSUING BUSINESS PERMITS

4. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which advised of an anomaly within the current parking permit procedures where the issuing of business permits did not comply with existing Traffic Regulation Orders.

The report recommended:-

that the Working Party -

- (a) note the content of the report and the options that officers have considered;
- (b) recommend the Enterprise, Planning and Infrastructure Committee to approve that the current policy for issuing business permits be changed to allow a business a maximum of two permits per property (in the zones where two residential permits are allowed) and one of these permits may be of the flexible type; and
- (c) recommend the Enterprise, Planning and Infrastructure Committee to instruct officers to amend the required Traffic Regulation Orders accordingly.

The Working Party resolved:-

to approve the recommendations contained in the report.

ALBERT TERRACE – AREA BOUNDARY CONSIDERATIONS

5. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which considered the request by the residents of Albert Terrace for Albert Terrace to be removed from zone B and to be included within zone P to allow the introduction of residents only parking.

The report recommended:-

that the Working Party note the contents of the report and recommend to the Enterprise, Planning and Infrastructure Committee that no further action on this matter is taken.

The Working Party resolved:-

to request officers to further explore the option of Albert Terrace to be removed from zone B and instead included within zone P to allow the introduction of residents only parking, and report to the next meeting of the Working Party.

CROSS BOUNDARY PARKING WITHIN THE AREAS OF CONTROLLED PARKING IN ABERDEEN CITY

6. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which presented the history behind the current relaxation of parking enforcement to allow cross-boundary parking, and highlighted problems that were now being encountered as a result of this.

The report recommended:-

that the Working party -

- (a) instruct officers to further investigate the option/s to alleviate the current problems and report back to the next meeting; and
- (b) to otherwise note the contents of the report.

The Working Party resolved:-

to approve the recommendations contained in the report.

ENTITLEMENT TO ON STREET CITY CENTRE PERMITS

7. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which sought consideration of the on street residential parking permits entitlement within the city centre and in particular the variation in the number of permits between adjacent zones within the Crown Street / Ferryhill area - a matter which had been raised by a local resident, through the local members and an MSP.

The report recommended:-

that the Working Party note the contents of the report and recommend to the Enterprise, Planning and Infrastructure Committee that no further action on this matter is taken.

The Working Party resolved:-

to request officers to further investigate the variation in entitlement of on street residential parking permits, in particular the variation in the number of permits between adjacent zones within the Crown Street / Ferryhill area, and to report back with possible options in this regard to the next meeting of the Working Party.

ON STREET PARKING PERMITS FOR PRIVATE CARE HOME PROVIDERS

8. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which considered the issues raised by private home care providers with regard to the difficulties and costs associated with parking within controlled parking areas.

The report recommended:-

that the Working Party consider the extension of “contractor’s permits” to private home care companies, and otherwise note the contents of the report.

The Working Party resolved:-

to request officers to evaluate the possible introduction of parking permits, or extension of “contractor’s permits” to private home care companies, discussing the matter with officers in Social Care and Wellbeing, and to discuss with NHS Grampian the scope for increasing the current charge for their permits, and to report back on this matter and possible options to the next meeting of the Working Party.

PROPOSED CHANGES TO THE CURRENT PARKING PERMIT PROCEDURES (OPERATIONAL AND POLICY RELATED)

9. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which advised of the proposed changes to existing parking permit procedures that did not comply with the Traffic Regulation Orders and which would reduce the Council’s revenue costs.

The report recommended:-

that the Working Party -

- (a) note the content of the report and the options that officers had considered;
- (b) recommend that the Enterprise, Planning and Infrastructure Committee approve a limited number of GP Medical permits to be issued to any practice within a controlled parking zone where no off-street parking exists (for the sole use of GP’s.);
- (c) recommend that the Enterprise, Planning and Infrastructure Committee approve that two additional flexible permits might be purchased to any repair and maintenance garage / business within all controlled parking zones within the city. (However these additional permits must be for trade working practices only and not for staff. A business with adequate off-street parking would not be eligible for these additional permits);
- (d) recommend that the Enterprise, Planning and Infrastructure Committee approve that a final reminder letter be issued at their next renewal to all permit holders advising them that this is their last and that no future reminders will be issued; and
- (e) recommend that the Enterprise, Planning and Infrastructure Committee instruct officers to amend the required Traffic Regulation Orders accordingly.

The Working Party resolved:-

to approve the recommendations in the report.

ABUSE OF RESIDENTIAL PERMITS

10. The Working Party received an oral report from Mr. Andrew Smith on existing issues regarding abuse of the permit system, including blue badges, and sought members’ views on this matter.

Mr Smith advised that although not widespread, the matter of non residents using permits for zones within which they were not resident still remained an issue. Thereafter, he provided an overview of the lengthy enforcement process which would be required to resolve this issue.

The Working Party resolved:-

to request officers to investigate measures taken by other local authorities to address abuse of parking permits and to report back with this information, as well as a recommended way forward for the Council to the next meeting of the Working Party for consideration.

EMISSIONS BASED PARKING CHARGES

11. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which explored the possibility of establishing a city wide emissions-based permit system.

The report recommended:-

that the Working Party note that the proposal would be developed in conjunction with the review of 2012/2014 parking charges to be undertaken in Autumn 2012 and reported to Committee in Spring 2013.

The Working Party resolved:-

to approve the recommendation.

DATE OF NEXT MEETING

12. The Working Party agreed that its next meeting be held at the end of March/early April, 2012.

- **COUNCILLOR JOHN CORALL, Convener.**

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CONTROLLED PARKING AREAS – WORKING PARTY

ABERDEEN, 5 April, 2012. - Minute of Meeting of the CONTROLLED PARKING AREAS WORKING PARTY. Present:- Councillor Corall, Convener; and Councillors Donnelly and Laing.

Apologies for absence had been intimated from Councillor Allan.

Also in attendance:- Councillors Kiddie (from article 1 to 4) and Robertson.

MINUTE OF PREVIOUS MEETING OF 3 FEBRUARY 2012

1. The Working Party had before it the minute of its previous meeting of 3 February 2012

The Working Party resolved:-

to approve the minute as an accurate record.

FACT SHEET AND CURRENT CONTROLLED PARKING ZONE AREAS

2. The Working Party had before it a fact sheet which detailed the controlled parking zone areas, as well as all parking entitlement and charges for each of the available parking options both on and off street.

The Working Party resolved:-

to note the fact sheet.

INTRODUCTION OF PARKING CONTROLS/CHARGES AT DUTHIE PARK

3. The Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which sought members' views on the proposed changes to the current parking procedures within the Duthie Park car parking areas, and the opportunity to introduce a maximum three hour stay for visitors.

The report recommended:-

that the Working Party -

- (a) note the content of the report and the options that officers had considered;
- (b) recommend that the Enterprise, Planning and Infrastructure Committee approve the introduction of a maximum stay of three hours (no return within one hour) within each of the car parks within Duthie Park between the hours of 8am and 4pm Monday to Friday; and
- (c) recommend that the Enterprise, Planning and Infrastructure Committee instruct officers to commence the required Traffic Regulation Order procedures.

The Working Party discussed the parking issues currently experienced at Duthie Park, as well as the proposals to alleviate such issues. Members supported the proposal, however sought reassurance that:- (a) the enforcement suggested was appropriate and practical from the city wardens' perspective; and (b) that the measures could be suspended for a special event.

The Working Party resolved:-

to approve the recommendations contained in the report.

CROSS BOUNDARY PARKING AND RESIDENTIAL PARKING PERMIT ELIGIBILITY

4. With reference to article 6 of the minute of the meeting of the Working Party of 3 February 2012, the Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which considered:- (a) a revision to the existing boundary parking restrictions to allow cross-boundary parking to address the current problems of enforcement and consistency of application; and (b) the eligibility of residents for parking permits and the issues relating to existing allocations.

The report recommended:-

that the Working Party –

recommend that the Enterprise, Planning and Infrastructure Committee -

- (a) approve the proposed changes to the Zone N boundaries and the integration of Zone P within Zone N;
- (b) approve the amalgamation of the parking Zones as noted in paragraph 5.31 for the purpose of cross boundary movements and that this be restricted to between the hours of 4pm and 10am each day; and
- (c) instruct officers to monitor the impact of the proposed changes and to review the eligibility of residents to on street parking should cross boundary parking be seen to be unresolved.

The Working Party resolved:-

to approve the recommendations contained in the report.

ALLOCATION OF BUSINESS PERMITS TO OFFICES WITH MULTIPLE OCCUPANCY

5. With reference to article 3 of the minute of the meeting of the Working Party of 3 February 2012, the Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which provided a summary of the findings regarding the misuse of business permits, that being both the allocation and abuse of permits and the potential remedies.

The report recommended:-

that the Working Party –

- (a) note the content of the report and the options that officers had considered; and
- (b) consider the options detailed in the report and recommend that the Enterprise, Planning and Infrastructure Committee approve option 2 outlined in the options appraisal contained at section 5.5.2 in the report, to restrict the

issue of business permits those businesses classified as retailers under the Town and County Planning (Use Clauses) (Scotland) 1997, only and which did not have any off-street parking.

The Working Party noted the ongoing issue regarding the misuse of business permits and welcomed the options contained in the report. Members supported option 2 contained therein, that being to restrict the issue of business permits those businesses classified as retailers under the Town and County Planning (Use Clauses) (Scotland) 1997, only and which did not have any off-street parking. However, members highlighted that this strict definition would create significant difficulties for professional services such as estate agents, architects etc who relied on the use of vehicles for their business, and wished for these businesses to be allowed business permits.

The Working Party resolved:-

- (i) to request officers to consider the possibility of restricting use of business permits to residential parking bays only and report their findings to the Enterprise, Planning and Infrastructure committee on 31 May 2012 for consideration;
- (ii) to request officers to provide a clear definition of businesses referred to as “professional services” and consult with members on this; and
- (iii) to recommend the Enterprise, Planning and Infrastructure Committee to restrict the issue of business permits to those businesses classified as retailers under the Town and County Planning (Use Clauses) (Scotland) 1997, and to “professional services”, (the definition of which to be drawn up by officers by the Enterprise, Planning and Infrastructure Committee meeting on 31 May), only and which did not have any off-street parking.

PROPOSED REVISIONS TO ON STREET PARKING CHARGES FOR FORRESTERHILL AND GARTHDEE

6. With reference to article 2 of the minute of the meeting of the Working Party of 3 February 2012, the Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which provided an update on the implications of the implementation of the proposed pricing structure for the Forresterhill and Garthdee on street parking zones.

The report recommended:-

that the Working Party -

- (a) note the contents of the report and the options that had been considered by the officers;
- (b) recommend that the Enterprise, Planning and Infrastructure Committee instruct officers to increase charges of on-street parking within both the Forresterhill and Garthdee zones in a phased manner, with option one to be implemented as soon as possible and a further review undertaken to raise charges in line with public transport costs; and
- (c) recommend that the Enterprise, Planning and Infrastructure Committee instruct officers to amend existing traffic regulation orders where required.

The Working Party considered the proposals extensively during which officers advised that they had not yet had the opportunity to consult with local businesses on the proposal, however they would now undertake this and report the outcome to the Enterprise, Planning and Infrastructure Committee on 31 May 2012.

On consideration of the proposal, Councillor Laing proposed that the Enterprise, Planning and Infrastructure Committee agree to postpone a decision on the introduction of increased charges within the Forresterhill and Garthdee zones until a review of the NHS Grampian's new parking scheme being introduced on 1 June, 2012, had been undertaken and the implications of this for on-street parking confirmed.

With regards the implementation of any increase in charges, officers advised that should an increase in charges be approved this would not require to go the traffic regulation order procedure, instead this could be implemented by street notice.

The Working Party resolved:-

to approve the recommendations contained in the report.

ABUSE OF RESIDENTIAL PERMITS

7. With reference to article 10 of the minute of the meeting of the Working Party of 3 February 2012, the Working Party had before it a report by the Director of Enterprise, Planning and Infrastructure which advised of the extent of the problems with the residents parking permit scheme.

The report recommended:-

that the Working Party instruct officers to undertake further investigation to determine the extent of residential parking abuse and to formulate options for consideration at a future meeting of the Working Party.

The Working Party resolved:-

to approve the recommendation contained in the report.

AOCB

8. The Chair opened the meeting to any other business, at which point some members raised operational matters in their own wards which officers undertook to look into.

- **COUNCILLOR JOHN CORALL, Convener.**

COUNCIL

16 MAY 2012

APPOINTMENT OF MEMBERS TO SPECIFIC SUB-COMMITTEES, GROUPS, COUNCIL SUPPORTED TRUSTS AND BOARDS - CG/12/056

6. The Council had before it a report by the Director of Corporate Governance which sought to assist elected members in making appointments to (a) certain sub-committees, working groups and task groups; (b) Council supported trusts; and (c) other organisations which required elected members to be appointed to them.

The report recommended:-

that the Council -

- (a) consider appointing members to the sub-committees, working groups and other groups shaded grey and in bold text within Appendix 1 to the report, by virtue of them requiring to be appointed by Council, or that they have meetings scheduled prior to the next meeting of the relevant committee;
- (b) refer the remaining sub-committees, working groups and other groups within Appendix 1 to the first meeting of the relevant committee as detailed in the far right hand column;
- (c) consider appointing members to the trusts listed in Appendix 2 to the report, noting the meeting dates detailed; and
- (d) consider appointing members to the boards listed in Appendix 3 to the report, noting the meeting dates detailed.

The Council resolved:-

- (i) to appoint members to the sub-committees, working groups and other groups shaded grey and in bold text within Appendix 1;
- (ii) to refer the remaining sub-committees, working groups and other groups to the first meeting of the relevant committee as detailed in the far right hand column; and
- (iii) to defer consideration of the appointment of members to the trusts and boards appended to the report for discussion between Group Leaders.

(Please note - for the purpose of this minute extract, only the groups relevant to the Enterprise, Planning and Infrastructure Committee have been included in the following Appendix)

APPENDIX 1

**APPOINTMENT OF REPRESENTATIVES
TO SPECIFIC SUB-COMMITTEES AND GROUPS**

Group	Council Representatives Required	Additional Information	Relevant Committee
Controlled Parking Areas Working Party	5 (plus any member who has controlled parking areas in their ward)		Enterprise, Planning and Infrastructure

DRAFT

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise Planning & Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	EP&I Service Asset Management Plans
REPORT NUMBER:	EPI/12/118

1. PURPOSE OF REPORT

To seek approval of the Service Asset Management Plans for Enterprise Planning & Infrastructure.

2. RECOMMENDATIONS

It is recommended that the Committee approve the Service Asset Management Plans for Enterprise Planning & Infrastructure.

3. FINANCIAL IMPLICATIONS

There are no immediate financial implications flowing from this Report. However, if implemented the Service priorities identified within the Service Asset Management Plans will have revenue and capital implications.

4. OTHER IMPLICATIONS

The Service Priorities identified may have staff time, condition and suitability implications.

5. BACKGROUND/MAIN ISSUES

5.1 Service Asset Management Plans

Service Asset Management Plans for EP&I were approved by this Committee in May 2011. It is recognised as best practice to update these on an annual basis.

Service AMPs are an integral part of developing corporate asset management planning as they identify key asset problems facing individual services. Each Service makes an assessment of the key property issues arising from the Council's initiatives and strategies, and also from existing knowledge on issues of property condition and suitability in each area. Service Managers have analysed the available

information about their portfolio so that gaps in provision and future needs are brought to the surface.

These Service AMPs will be used to inform bids for capital investment and enable the Council to develop a strategic approach for addressing property portfolio gaps through its Property Asset Management Plan.

5.2 Scope of EP&I Service Asset Management Plan

Service AMP's for Asset Management and Operations were approved in 2011 and have now been updated. These are shown in Appendix A and Appendix B. An additional Service AMP has also been produced to cover Planning & Sustainable Development and is shown in Appendix C.

It should be noted that certain EP&I property assets will be dealt with out with this report. The relevant assets and the reasons why are as follows:-

- 1) Vehicle Workshop Kittybrewster – Will form part of the Fleet Asset Management Plan, which will be reported after the summer recess.
- 2) Corporate Office Accommodation – This is covered by the ongoing office accommodation review, with updates on this going to the Finance & Resources Committee.

5.3 Outcomes from the Service AMPs

The Operations and Planning & Sustainable Development Service AMP's were approved at a meeting of the Corporate Asset Group (CAG) on 25 April. The CAG were also consulted on the Asset Management Service AMP with no comments being submitted.

From the Service the Preferred Solutions Are:-

Asset Management

- 1) Complete Car Park asset review to determine preferred solutions. Target Completion – December 2012.

Operations

- 1) Establish benefits of moving Traffic Management Team along with the team for Penalty Charge Notices and Permits to ground floor of Spring Garden. Consider moving other appropriate teams from Marischal College to the first floor of Spring Garden. Then carry out moves as appropriate. Target Completion – July 2012
- 2) Consider the following options for our Roads Depots as part of the ongoing Depot Review. Target Completion – August 2012

- Because of the need to have the same storage, garaging etc as there are at the moment it would not seem a worthwhile option to give up the three locations to move to a single location unless it fulfilled all the current requirements and its location was suitable, if a property of a similar requirement came on the market then a move would need to be considered. Target Completion – August 2016
 - An alternative would be to spend money on the West Tullos Depot to change the area of staff facilities, move store to ground floor and increase garage area. Target Completion – August 2014
- 3) Consider Mobile Working such that men do not need to return to the depot to receive work instructions. e.g. Works Programmer, Integrated Computer Systems, etc. Target Completion – April 2014

Planning & Sustainable Development

1) Park and Ride Sites

- The permanent site for the Bridge of Don Park and Ride will be determined by a further report to the Enterprise Planning and Infrastructure Committee. Target Completion – May 2012.
- Park & Choose site at the A96 Dyce Drive will follow the AWPR timeline which is detailed in section 3.4. Target Completion - The timing cannot be determined at present time.

2) Berryden Improvement Corridor Assets

- The Powis Terrace units would benefit from early demolition. However, there is no capital budget allocation for the Berryden improvement corridor – this will be reviewed on an annual basis
- Replacement of Berryden Business Centre sub station - there is no capital budget allocation for the Berryden improvement corridor – this will be reviewed on an annual basis
- Hutcheon Street - remain vacant but will be reviewed monthly by the Corporate Asset Group

3) PTU Parking Facility

- Continue to work with Asset Management to identify an alternative site. Target Completion - TBC

5.4 Next Stage

The approved Service AMP's along with all the other Directorates Service AMP's will feed into the 2012 Property AMP and help identify future capital spending demands.

6. IMPACT

Corporate - The continued implementation of the Property Asset Management Plan will ensure that the Council is utilising its property portfolios to support Services in implementing the Single Outcome Agreement. The approved property visions and asset objectives already support *Vibrant, Dynamic & Forward Looking*.

Public – Subject to Service Priorities

7. BACKGROUND PAPERS

Property Asset Management Plan Update 2011
Property Asset Management Plan 2009

8. REPORT AUTHOR DETAILS

Alastair Reid
Team Leader – Asset Management
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☎ 01224 52(2627)

Service Asset Management Plan 2011

Directorate – Enterprise Planning & Infrastructure

Service – Asset Management & Operations (Asset Management)

Officers Completing Form – Stephen Booth, Property Estates Manager
Alastair Reid, Team Leader – Asset Management

Section 1 – Introduction

Completion of Service AMP

The AMP Team coordinated the data collection and completion of the form. Meetings with other stakeholder Managers took place as and when required.

Service Managers involved in the process

Stephen Booth, Mike Duncan, Alastair Reid, Janice Lyon.

Links to Business Planning and Budget Process

The Service AMP recognises both the 5 Year Business Plan and the EP&I Business Plan 2010 – 2013. This Service AMP has direct links to a number of the strategic priorities identified in the 5 Year Business Plan.

Section 2 – Description of Service

The structure for the future delivery of Asset Management was approved in April 2011 and is now in place.

The structure identifies Asset Management is split across four managerial areas. These are:

- Estates Service
- Fleet Services
- Asset and Capital Management
- Property Investment Portfolio (TNRP)

The following summarises the main work areas and also identifies the property assets under their management. Not all the assets held require to be included in this Service AMP; where this is the case an appropriate comment has been made.

The Services are primarily located within Marischal College and Kittybrewster (approx. 100 staff). There are currently no identified issues in relation to office accommodation.

Estates Service (Property Estates Manager)

Main Functions

- Provision of Corporate property advice
- Valuation of Council portfolio
- Acquisition of property by sale, lease or statutory methods
- Rating advise for Council owned property
- Development advice in relation to Council owned property
- Sale of surplus property
- Strategic overview of car parking function

Assets

- Off Street car parking – Included in Service AMP
- Surplus property – No requirement to include in Service AMP

Fleet Services (Fleet Service Manager)

Main Functions

- Fleet Maintenance
- Taxi Inspections

Assets

- Vehicle Workshop – To be included in Fleet Asset Management Plan which is scheduled to be completed by Autumn 2012.

Asset and Capital Management (Corporate Asset and Capital Manager)

Main Functions

- Capital Management
- Asset Management
- Energy Management
- Corporate Asset Management System
- Design

Assets

- Corporate Offices – Covered by ongoing Corporate Office Review.

Property Investment Portfolio (Property Investment Manager)

Main Functions

- Management of Council's Investment Portfolio
- Management of Properties held for the Common Good

Assets

- Tenanted Non Residential Portfolio (TNRP). No requirement to include in Service AMP. This will be included within the 2012 Property Asset Management Plan.

Section 3 – Anticipated Changes to the Service

The only assets that require to be included in this section of the Service AMP are off street parking facilities.

Car Parking

The Council's interest in off street car parking is due to a variety of reasons, some historic and some opportunistic. The strategic rationale for holding sites is focused on three main areas. These are:-

- Income generation
- Economic development
- Traffic management

Given the number of issues associated with the current responsibilities, management, operation and development of facilities it is intended to undertake a full asset review of parking provision in the City. This is underway and will be completed during 2012.

This review will address the future delivery of services in the context of:-

- Overall corporate responsibility
- Future policy
- Site specific issues and assessment
- Competition
- Delivery methods and collection

The review will also focus on a number of key issues in relation to the portfolio, in particular the age and deteriorating condition of the estate, need for re-investment and the strategic development role some sites have in relation the City Centre Masterplan. The review will also consider the impact of the Local Transport Strategy 2008-13.

Section 4 – Asset Demand Profile (What do we need?)

The only assets that require to be included in this section of the Service AMP are off street parking facilities.

Car Parking – off Street

Appendix A

The future Demand profile is currently under consideration and will be influenced by the outcome of the Asset Review.

The Asset Review will address all key issues in relation to the current facilities, current market, operational issues, long term development issues, future policy and delivery.

Section 5 – Asset Supply Profile (What do we have?)

The only assets that require to be included in this section of the Service AMP are off street parking facilities.

Car Parking – off Street

Site	Type	Suitability Rating	Condition Grade	No. of Spaces
West North Street Multi Storey	Covered	B	B	160
Chapel Street Multi Storey	Covered	B	B	500
Denburn Car Park	Covered	B	B	326
St Nicholas House	Closed			
Total Covered				986
Virginia Street	Uncovered	B	B	46
Greyfriars/Gallowgate	Uncovered	B	B	138
Broomhill Road	Uncovered	B	B	17
Fonthill Road	Uncovered	B	A	8
Summer Street	Uncovered	B	C	42
Jack's brae	Uncovered	B	A	20
Golden Square	Uncovered	B	A	32
East North Street - Closed	Closed			
Total Uncovered				303
Marischal College – PT Under Development	Covered	A	A	92
Frederick street – Under development	Covered			185
Total New				276

Asset Performance

Site	Income 2011/12	Spaces	Net Income (EBITDA*)	Income per space
West North Street Multi Storey	£69,945	160	£47,008	£293
Chapel Street Multi Storey	£490,000	500	£380,210	£760

Appendix A

Denburn Car Park	£388,484	326	£299,219	£917
St Nicholas House	-		-	
Virginia Street	£32,267	46	£12,820	£278
Greyfriars/Gallowgate	£292,342	138	£231,732	£1,679
Broomhill Road	£19,755	17	£17,666	£1,039
Fonthill Road	£6,522	8	£5,574	£697
Summer Street	£79,658	42	£65,109	£1,550
Jack's Brae	£6,284	20	£2,024	£101
Golden Square	£120,654	32	£71,169	£2,224
East North Street - Closed	-		-	
Marischal College – P/T Under Development	-	92	-	-
Frederick street – Under development	-	185	-	-

* Earnings before interest, taxes, depreciation and amortisation (Net Profit)

In Financial year 2011/12 the net income from off-street parking rose from creating a surplus of just over £1 million to a surplus just over £1.2 million, around 20%. This was against a hoped increase of around £1.5 million. (It should be noted that this figure excludes income (and costs) associated with permits and PCN's). This is because these entitle the holder to use a number of car parks so they can't be allocated to a particular car park.

Section 6 – Supply / Demand Comparison

To be determined by the Car Park Asset Review.

Section 7 – Preferred Solutions & Development of Outline 5 Year Plan

1) Complete Car Park asset review to determine preferred solutions. Target Completion – December 2012.

A number of issues have already been identified which will be addressed more fully. These include:-

Car Park	Development pressures/ Issues
West North Street Multi Storey	Ongoing maintenance and upgrading issues for 1960's structure.
Chapel Street Multi Storey	1960's structure in need of upgrading and new investment. Site was presented to the market and attracted interest in early 2000's. Development brief requires replacement of parking spaces for public use which restricts potential in current market.

Appendix A

Denburn Car Park	1960's structure in need of upgrading and new investment. Building forms part of city centre development site and it is in shared ownership with NHS who have a health facility on the upper deck. The site also encompasses around 144 spaces held on the Housing Revenue Account. The future of the site will be formally reviewed during 2012 to consider the redevelopment potential based on an updated development brief approved in 2011. Any redevelopment should reflect the significant income generated.
St Nicholas House	Facility closed in 2011. Impact on income.
Virginia Street	No current issues
Greyfriars/Gallowgate	No current issues.
Broomhill Road	No current issues.
Fonthill Road	No current issues.
Summer Street	Car park needs re-surfaced. Declining income pattern.
Jack's brae	No current issues
Golden Square	Car Park management has recently transferred to Council.
East North Street	Facility closed. Impact on income.
Marischal College – PT Under Development	New development on part-time basis. Investment made in 2011/12
Frederick street – Under development	Development currently on site and should be complete in late 2012. Investment to be made in collection capability.

Section 8 – Approval by SMT

To be presented along with other EP&I Service AMP's at appropriate meeting.

Section 9 – Comments of the Corporate Asset Group

The Service ANP was circulated to CAG as part of report consultation. No comments were submitted.

Service Asset Management Plan 2012 Update

Directorate – Enterprise Planning & Infrastructure

Service – Asset Management & Operations (Operations)

Officer Completing Form – Michael Cheyne, General Manager Operations

Section 1 – Introduction

Completion of Service AMP

Meeting took place with the Roads Operations Manager, Traffic Management Manager, Structures Flooding and Coastal Protection Manager and Facilities Manager.

Service Managers involved in the process:-

Richard Blain, Alan Robertson, Andy Smith and Simon Williams

Links to Business Planning and Budget Process

The Service Asset Management Plan is part of the Service Plan, it will be used to determine the establishment that will be required to deliver the service taking into account Best Value requirements in times of reducing budgets.

Section 2 – Description of Service

Major areas of operations are included here, the Roads Asset Management Plan will be used to determine works required against available budget.

1.0 Operational

Main Functions

(a) Emergency Response Service

A 24 - hour, 365 days a year service established for dealing with situations, which relate to public safety. Instructions usually instigated by the police in response to Road Traffic Accidents, public complaints or weather emergencies.

(b) Lighting Maintenance

Dealing with 31,000 streetlights, requiring cable faults, changing bulbs chokes etc and replacing defective lighting columns. A programme of lighting column testing has been set up to look into the amounts of corrosion in the existing lighting stock, replacement of corroded columns.

Appendix B

(c) Signing Operations

Manufacture of new street and direction signs and their subsequent erection. Temporary signage for control and warning of motorists of road works within the City. Temporary signage for local functions.

(d) Lining Operations

Installation of road markings in white or yellow thermoplastic or epoxy paints.

(e) Gully Cleaning

Planned programme of maintenance of approx. 31,000 gullies, need to re-establish planned programme of cleaning against response maintenance to reduce costs

(f) Road Maintenance

Footpath and carriageway repairs, pothole patching and drainage works

(g) Winter Maintenance

The salting and ploughing of roads as and when required, main timescale November to April. Work carried out in accordance with the Winter Services Plan as approved by EP&I Committee.

(h) Work for External Clients

Trunk Road operations were removed from local authority control and awarded under tendered contract to BEAR (Scotland) Ltd. In order that we did not lose out on these works we have entered into agreement with BEAR to carry out emergency traffic management and winter maintenance operations. Additional works carried out for Developers are the installation of lining, signing and street lighting.

West Tullos is the main depot for operations with the admin, technical and 60% of operational staff and plant working from there, Bucksburn and Culter are satellite depots with approx 30% and 10% of the operational staff working from them.

West Tullos, Bucksburn and Culter all play a major part in winter operations, providing local salt storage with additional storage at Garlogie (10,000 tonnes) and Park Road (5,000 tonnes).

Signing, Lining, Lighting and Gully Cleaning are all carried out from the West Tullos Depot. In order to handle the Waste from Gully Cleaning operations a reed bed facility has been installed in Nigg.

Assets: **Roads Depots and Yards,**
 Other Assets to be included in the Roads Asset Management
 Plan

2.0 Traffic Engineering

Main Functions

(a) Road Safety Schemes

Various contracts involving speed humps, mandatory and advisory speed restrictions, 20's Plenty, Pedestrian Islands and the installation of Traffic Lights, Puffin and Toucan Crossings

(b) Intelligent Traffic Systems

The maintenance of existing traffic lights, pedestrian crossings. Carrying out day to day monitoring of the road network to ensure that the system is fully functional. To move from a reactive system to a proactive system of traffic management

(c) New Roads and Streetworks

Using computer based system to record all the road occupations that are taking place within the city, to programme works to ensure that there are no conflict of operations and that delays to the travelling public are kept to a minimum

Assets: None

Traffic Lights, Ped Crossings etc to be included in the Roads Asset Management Plan

3.0 Structures, Flooding and Coastal Protection

Main Functions

(a) Structures

To maintain current structures, to carry out an annual inspection the structure in order that the RAMP is kept up to date

(b) Flooding

To ensure that the Council is complying with the "The Flood Risk Management (Scotland) Act 2009"

(c) Coastal Protection

Ensuring compliance with the Coastal Protection Act and The Marine Scotland Act

Assets: – None.

Bridges, Retaining Walls, Culverts to be included in the Roads Asset Management Plan

4.0 Facilities Management

Main Functions

Soft FM

1. Cleaning Services

Provide a cleaning service to schools and other non housing properties across the city. School cleaning is carried out generally in term time with deep cleans being carried out during main holiday periods; other buildings are currently cleaned in line with operational requirements. Generally cleaning storage areas are inadequate and not fit for purpose. Equipment requires to be updated moving away from the bucket and mop to, where practicable, mechanical means.

2. Distribution Services

These include Couriers, transporting post and goods between sites, Drivers transporting staff and goods between sites and Post Room Staff, providing post room and other office based duties.

3. Catering Services

Catering Services provide catering to ACC and 3R run Primary and Secondary Schools, some care premises Staff vending and restaurant facilities at Town House and Kittybrewster. The school operation is in term time only and based predominately on a school kitchen unit which provides an in house service to the host school, although some kitchen units provide hot food for transport and serving to other schools.

4. Janitorial Services

Janitors are based on groups which are in turn based around the ASG's with each school being allocated a number of input hours based on pupil numbers, the operation also includes grounds janitors who carry out mowing of the grass, pitch marking etc. Work activities include litter picking, security, snow clearing equipment moves etc.

5. School Crossing Patrollers

School Crossing Patrollers generally work outside in all weathers, guiding children and parents across the street. Their work hours vary depending on a morning and afternoon shift with some areas also having a lunch time operation

Hard FM

1. Non Housing Property

The current arrangement is based on Non Housing Property and provides a repair and maintenance service to all non housing properties owned by or managed by Aberdeen City Council. It has been agreed that the works

Appendix B

associated with Non housing will all now be carried out by Building and Works and that a number of Inspectors currently in this team will transfer to Building and Works with the remainder (advisors) carrying out an audit on the best value of the work provided. Asbestos officer to be employed to manage asbestos surveys, register and reports for Non Housing Property

Contract Team

This is a new team that is being set up to cover the completion of the 3R's project and to manage the Hard and Soft FM facilities that are being provided under that contract. In order to carry out this role they will call on the services of the Hard and Soft FM teams within Facilities Management. This team will also have responsibility to procure other contracts that Aberdeen City Council is required to have, either on its own or as part of a shared service, to comply with legislation.

Assets: None

Traffic Management are currently based in Corporate Office accommodation at Spring Garden. Structures, Flooding and Coastal Protection and Facilities Management are currently based in Corporate Office accommodation in Marischal College.

Section 3 – Anticipated Changes to the Service

The only assets that require to be included in sections 3-6 of the Service AMP are Roads Depots and Yards under the Operational side of the Service.

Operational

3.1 Government Policy

Non known at present but this will change as a requirement for additional spend to an ever deteriorating road network is pursued. Additional costs due to Disabled Parking Legislation.

3.2 Changes in Service Standards or Delivery Methods

Facilities Structure and Service Delivery is currently under review. Service delivery under review for Roads Operations. Soft Marketing Testing carried out in 2011 with a staff workshop in early 2012 looking at what do we do well in-house? What would be better outsourced? How can we improve it? Road usage charges are set and reviewed annually. Energy efficiency, consider lowering lamp wattage, LEDs, Reduce ITS communication cost by use of wi-fi or fibre optics being looking at.

Appendix B

Re-evaluate the way non housing property is managed, change service delivery to Building services along with inspection services.

It is proposed that the team for Penalty Charge Notices and Permits are to be line managed by the Traffic Engineering Manager, this will have them working alongside there two main customers.

3.3 Re-focus towards Corporate Priorities

Traffic Management & Safety: reduced funding means that safety schemes will need to be prioritised, look at School Crossing Patrollers, re-assess locations and alternatives. Need to look at what is statutory and what isn't, although to reduce to statutory requirements would cause major problems with traffic flows.

3.4 Budget Restrictions

Revenue Budget fixed and in most cases isn't covering the maintenance operations that are required. Capital budgets well below that which is required to keep road deterioration at a standstill never mind starting to reduce the backlog.

3.5 New Funding

Currently there is no new funding available for Roads, monies made available for the severe winter and emergency repairs help but do not cover increased costs.

3.6 Best Value or Other Review

Facilities and Roads take part in APSE (Association for Public Service Excellence) Performance Networks, Internal Audit Review on Road Maintenance taking place in 2011.

3.7 Partnerships with other Organisations

None at present but there is a shared procurement in place with Police, NHS, Fire Brigade and Aberdeenshire for common facilities contracts. Shared salt and Met Office contract for winter operations with Aberdeenshire and Moray. Scotland Excel have carried out a series of "tenders" for the supply of material, we are currently using some of those with more due to come on stream later in 2011.

Section 4 – Asset Demand Profile (What do we need?)

Operational

Currently to deliver the operations service we require four main items, Labour, Plant, Materials and Depots.

Garaging is required for about 150 pieces of plant, asphalt pavers, jet patcher lorry, 18 & 26 tonne lorries, hydraulic platforms, gully emptiers, lining lorries, pick ups and vans along with 8 mainline gritter and 6 demountable gritters and associated plough blades etc.

If the service continues to be delivered in-house a new build in a suitable location giving quick and easy access to all parts of the city, having the ability to store 15,000 tonnes of salt, garaging for plant and office accommodation for all roads operations would provide a more integrated service delivery plus a possible saving in staff due to all employees operating from the one location.

Description / Proposed Use - Office, Depot and Salt Store

Location - Site allowing easy access to the Aberdeen Western Peripheral Route.

Approximate size - 4 Hectares

Ideal Layout - Garages and Workshops, storage facilities, Salt storage areas, Office accommodation for 60 staff, welfare facilities for 180 staff.

Customer Needs - The location would be best from a service delivery point of view, customer service would best be maintained from call centre, web or city centre service desk.

Image - A corporate service delivery for all operations would provide an easy cross team delivery point, access for customers would be difficult but a good service desk would help.

Appendix B

Section 5 – Asset Supply Profile (What do we have?)

West Tullos is the main depot, based to the south of the City, providing accommodation for about 120 staff and has storage facilities for both salt, slabs, kerbs etc and all the electrical equipment to maintain and renew the street lighting across the City and garaging for approx 70% of the plant.

Bucksburn Depot is to the north of the City, providing accommodation for about 30 staff, has storage facilities for both salt, slabs, kerbs etc and garaging for approx 20 % of the plant.

Culter Depot is to the west of the City providing accommodation for about 10 staff has storage facilities for both salt, slabs, kerbs etc and garaging for approx 10% of the plant.

Depots:-

1) West Tullos Roads Depot

Description/Use	Roads Depot and Office Accommodation
Currently 1 of 3 depots across Aberdeen, the requirement for each depot is storage, garaging, salt storage and work areas, along with offices for roads staff at this depot.	
Title	In Council Ownership
Suitability Grade	B
Depot provides the basic facility for the delivery of the service, salt storage is lower than would be desired after the last 3 winters, salt is partially covered. Depot has been changed from a service delivery by inhouse contractor to a shared work area for the old DLO and Client, this operation reduced the flexibility of the buildings without spending a large amount of capital. Location gives easy access to most of the main city roads, this also means that at peak times it is difficult to get emergency response quickly. No disabled access to the second floor of the building.	
Condition Rating	B
Building is in satisfactory condition overall. Areas of roof, flooring and fixtures/fittings are in poor condition. Welfare facilities are reasonable but in are cramped in areas, internal fire systems all work but will probably require replacement within the next 5 years.	

2) Bucksburn Roads Depot

Description/Use	Roads Depot and Office Accommodation
Currently 1 of 3 depots across Aberdeen, the requirement for each depot is storage, garaging, salt storage and work areas.	
Title	In Council Ownership
Suitability Grade	B
Depot provides the basic facility for the delivery of the service, salt storage is lower than would be desired after the last 3 winters, salt is uncovered. Depot consists of mainly garages, stores, mess facilities around a central large open area, to modify it would be worthwhile	

Appendix B

demolishing and rebuild. Location gives easy access to most of the main A96, this also means that at peak times it is difficult to get emergency response quickly.	
Condition Rating	B
Building is in satisfactory condition overall. Windows are in very poor order throughout and welfare facilities are in need of money being spent currently these are portacabins which will need replacement within the next 2 years. Although ongoing monies will need to be spent on roller doors etc.	

3) Culter Roads Depot

Description/Use	Roads Depot and Office Accommodation
Currently 1 of 3 depots across Aberdeen, the requirement for each depot is storage, garaging, salt storage and work areas.	
Title	In Council Ownership
Suitability Grade	B
Depot provides the basic facility for the delivery of the service, salt storage is lower than would be desired after the last 3 winters, salt is uncovered. Depot consists of mainly garages, stores, mess facilities around a central large open area, to modify it would be worthwhile demolishing and rebuild. Location gives easy access to most of the main A93 & A944, this also means that at peak times it is difficult to get emergency response quickly.	
Condition Rating	B
Building is in satisfactory condition overall. Windows, flooring, external doors and decoration are generally in poor order.	

Storage Yards:-

1) Garlogie

Description/Use	Salt Store
Title	Leased In
Suitability Grade	D
5 Storage bays for storage of salt, salt completely uncovered. Easy access, good loading facilities, good storage capacity, long way from city where material is needed. Council Tax Rates for the area very high. Distance from City means high travel costs. Welfare facilities not provided, health and Safety reasonable. External roads around salt bays allow easy access. Large area for the store of salt, adequate facilities to stop salt leaching into surrounding countryside. Does not impinge on surrounding area, kept reasonably tidy. Location gives easy access to most of the main city roads but distance to travel on a snow day is very high.	
Condition Rating	N/A

Appendix B

ACC are responsible for the maintenance of this store, these are minor obligations as currently all roads are in good condition and interceptors and drains are all working well. When the lease is terminated we are required to remove the salt bays and return the area to farm land

2) 54 Park Road

Description/Use	Salt Store
Title	Leased In
Suitability Grade	B
Easy access, good loading facilities, good storage capacity. Location gives easy access to most of the main city roads, short loading and return to route in times of snow and ice.	
Condition Rating	C
Building in poor condition. Vast majority of elements identified as poor condition. Lease conditions require the Council to maintain the property in no worse condition.	

3) Mundurno

Description/Use	Granite Storage Yard
Title	In Council ownership
Suitability Grade	C
Location reasonable as it is not being used on a day to day basis, Council Tax Rates make this an expensive storage yard and will need to be cleared. Yard provides basic storage. Loading and unloading operations only, no welfare facilities.	
Condition Rating	D
Surface is rated as life expired.	

4) Bankhead

Description/Use	Storage yard attached to the Bucksburn Depot
Title	In Council ownership
Suitability Grade	
This area proves 1500 tones of salt storage along with smaller storage capacity for drainage, kerbs, slabs etc.	
Condition Rating	
Area is adequately surfaced, with external floodlights for loading operations. Interceptor and drains are working well	

Section 6 – Supply / Demand Comparison

The current set up is suitable but not ideal for delivery of a service that has become “more city wide” over the last 10 years as the workforce has diminished. When the staff complement was about 150 manual staff and work was plentiful, operations were location based with Tullos covering works from the River Don south, Bucksburn would cover work from north of the River Don to the A944 and Culter would cover works along the Dee valley into Anderson Drive but with diminished staff and workloads the operations are no longer area based but related to work and labour availability. This is why three depots are no longer required but their combined storage, garaging etc are.

Section 7 – Preferred Solutions & Development of Outline 5 Year Plan

- 1) Establish benefits of moving Traffic Management Team along with the team for Penalty Charge Notices and Permits to ground floor of Spring Garden. Consider moving other appropriate teams from Marischal College to the first floor of Spring Garden. Then carry out moves as appropriate. Target Completion – July 2012
- 2) Consider the following options for our Roads Depots as part of the ongoing depot review. Target Completion – August 2012
 - Because of the need to have the same storage, garaging etc as there are at the moment it would not seem a worthwhile option to give up the three locations to move to a single location unless it fulfilled all the current requirements and its location was suitable, if a property of a similar requirement came on the market then a move would need to be considered. Target Completion – August 2016
 - An alternative would be to spend money on the West Tullos Depot to change the area of staff facilities, move store to ground floor and increase garage area. Target Completion – August 2014
- 3) Consider Mobile Working such that men do not need to return to the depot to receive work instructions. e.g. Works Programmer, Integrated Computer Systems, etc. Target Completion – April 2014

Section 8 – Approval by SMT

To be presented along with Asset Management Service AMP at appropriate meeting.

Section 9 – Comments of the Corporate Asset Group

The Service AMP was approved at meeting of the group on 25 April.

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Directorate: Enterprise Planning & Infrastructure
Service: Planning & Sustainable Development
Officer Completing Form: Kathleen Fraser

Section 1 – Introduction

Completion of Service Asset Management Plan (AMP)

The P&SD SMT Team coordinated the data collection and completion of the Service AMP. Consultation / approval by team Managers occurred as and when required.

Service Managers involved in the process:

Daniel Lewis, Gale Beattie, John Wilson, Gordon Spence

Links to Business Planning and Budget Process

The Service AMP recognises both the 5 Year Business Plan and the EP&I Business Plan 2010 – 2013 along with the P&SD Service Plan 2012 – 2013.

Section 2 – Description of Service

2.1 P&SD service portfolio

P&SD deliver a wide range of inter-related functions, within a remit that covers land use, environment and transportation. These Services must be delivered within the context of wider social and economic objectives and city-wide regeneration:

- Development planning
- Outdoor access
- Environmental policy
- Climate change
- Carbon management
- Transportation strategy
- Transportation programmes
- Roads design
- Road Construction Consent
- Public transport
- Master-planning and design
- Conservation
- Development management

Appendix C

- Building standards
- Major projects
- Aberdeen Western Peripheral Route (AWPR)

2.2 P&SD current service structure

A revised organisational structure was implemented in August 2011, primarily designed to reduce cost (headcount), whilst improving integration and co-ordination and maintaining service levels.

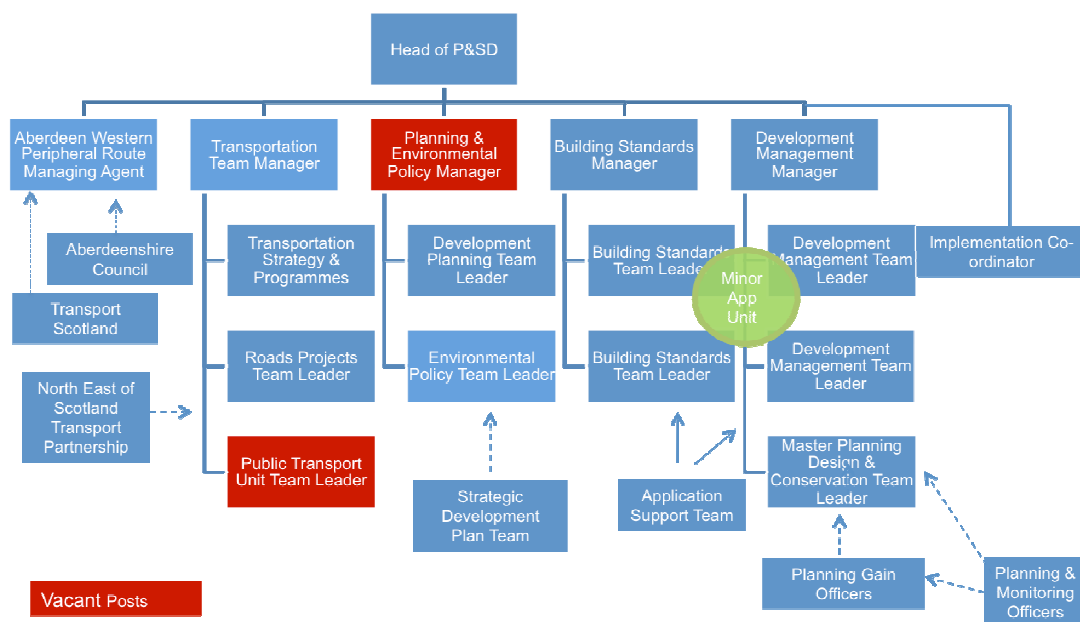
P&SD has a total of 166 Full Time Equivalent (FTE) posts and is divided into 5 teams, each managed by an individual that reports directly to the Head of Service:

- AWPR Team
- Transportation Team
- Planning & Environmental Policy (PEP) Team
- Development Management Team
- Building Standards Team

The Implementation Co-ordinator works across all of these teams and reports directly to the Head of Service.

An organisation chart, depicting the relevant structure within each of the above teams is illustrated in figure 1

Figure 1 P&SD structure



Key for the above structure:

- The Application Support Team which is part of P&SD splits its time evenly supporting Development Management and Building Standards
- Minor App Unit = Minor Applications Unit – the aim is to develop a multi disciplined team between Development Management and Building Standards to deal with minor applications
- The dotted arrows in the aforementioned structure charts illustrate Services or external bodies that P&SD has a close partnership working arrangement. They are as follows:
 - Transport Scotland
 - Aberdeenshire Council
 - North East of Scotland Transport Partnership
 - Strategic Development Plan Team = Aberdeen City and Shire Strategic Development Planning Authority
 - Planning Gain Officers = 3 x Officers based in Aberdeenshire Council with one officer specifically funded by Aberdeen City Council
 - Planning and Monitoring Officer = Based in Asset Management, EP&I - part of this role is devoted to fulfilling the duties of a Planning Gain Monitoring Officer

2.3 Team portfolios and Assets

The following summarises the main work portfolios for each Team at Manager level and also identifies the property assets under their management.

The Service is primarily located within Corporate Office buildings (currently Marischal College (Ground Floor North). The Public Transport Unit of the Service is located in Level 2 North Marischal College, whilst the AWPR Managing Agent's team is located within the Aberdeen Business Centre, Willowbank House, Willowbank Road.

2.3.1 Development Management

Main Functions

The Development Management Team is responsible for: the management of the processing and determination of planning and other related applications; management of planning appeals; management of systems, including the development and implementation of eplanning; carrying out of enforcement procedures relating to breaches of planning consent; management of the Application Support Team for planning applications and building warrants; preparation and implementation of non-statutory development management policy; and provision of support to the Local Review Body.

It is also responsible for the preparation and co-ordination of masterplans and development briefs and their implementation; management of the joint

Aberdeen City and Aberdeenshire Design Review Panel; implementation of the Council's design campaign; management of the Council's conservation responsibilities, including the City Heritage Trust and Townscape Heritage Initiative; provision of support to the Local Review Body; and ensuring appropriate links to regeneration functions.

Assets

None.

2.3.2 Building Standards

Main Functions

The Building Standards Team is responsible for the development and implementation of building standards regulations and associated procedures within the City; carrying out of enforcement procedures within the Building (Scotland) Act; management of the operation of systems in support of building warrant compliance applications, notices and orders; provision of advice and guidance on the operation of building regulations; advising on and promoting issues in relation to disabled access; management of the building standards input to the licensing of buildings or land within the City; and management of the Property Enquiries Unit.

Assets

None.

2.3.3 Planning & Environmental Policy

Main Functions

The Team is responsible for the preparation, implementation and monitoring of the Local Development Plan and related supplementary planning guidance; input to the preparation and implementation of the Strategic Development Plan; preparation, implementation and monitoring of the Council's carbon management plan, Core Paths Plan, open space strategy, nature conservation strategy and other environmental plans and policies; management of Strategic Environmental Assessment responsibilities of the Council, development and implementation of EU demonstration and learning projects; and management of the Council's development management tree functions.

Assets

- Suite 1, 27 – 29 King Street – NESTRANS, ACSEF & Aberdeen City & Shire Strategic Development Planning Authority (SDPA), responsibility for which is shared with Nestrans, ACSEF and Aberdeenshire Councils.

2.3.4 AWPR

Main Functions

The AWPR team is responsible for the management and co-ordination of the obligations of Transport Scotland, Aberdeen City and Aberdeenshire Councils (the funding partners) for the delivery of the AWPR in accordance with the Management Agent Agreement and Memorandum of Understanding; management of all necessary procedures and contracts for the construction of the AWPR (design procurement, consultation, planning and road order procedures, Public Local Inquiry, land and property acquisition, works procurement, implementation); and monitoring of progress and expenditure on the AWPR.

Assets

Residential properties acquired on the AWPR route and currently available for rental

- Dellwood, 2A Milltimber Brae, Milltimber, Aberdeen, AB13 0DY
- The Sidings, Station Road, Milltimber, Aberdeen, AB13 0DP
- Beech Hill, 309 North Deeside Road, Milltimber, Aberdeen, AB13 0DL
- Carskeoch, 252 North Deeside Road, Milltimber, Aberdeen, AB13 0DJ
- Pine Lodge, 315 North Deeside Road, Milltimber, Aberdeen, AB13 0DL
- Kerloch, 2 Milltimber Brae, Milltimber, Aberdeen, AB13 0DY
- Clarewood, 4 Milltimber Brae, Milltimber, Aberdeen, AB13 0DY
- Birchlea, 250a North Deeside Road, Milltimber, Aberdeen, AB13 0DJ
- Millbrae House, 6 Milltimber Brae, Milltimber, Aberdeen, AB13 0DY
- Newhall House, Portlethen, Aberdeen, AB12 4RT
- Bridgebank, 3 Milltimber Brae, Aberdeen, Milltimber, Aberdeen, AB13 0DY
- Crofton, 5 Milltimber Brae, Aberdeen, Milltimber, Aberdeen, AB13 0DY
- Broomhill, Blacktop, Kingswells, Aberdeen, AB15 8QL
- 1 Sunnybank Cottage, Craibstone, Aberdeen, AB21 9SP
- 2 Sunnybank Cottage, Craibstone, Aberdeen, AB21 9SP
- Gairnlea, Blacktop, Milltimber, Aberdeen, AB15 8QL

Non-residential properties acquired and potentially available for rental

- Parkhill Stables and Cattery, The Laurels, Corsehill, Newmachar, Aberdeen, AB21 7XA

Non-residential properties acquired not available for rental

- International School of Aberdeen, Milltimber Brae, Milltimber, Aberdeen

AWPR office location

- The AWPR team currently occupy three rooms in the Aberdeen Business Centre, Willowbank House in Willowbank Road

2.3.5 Transportation

Main Functions

The Transportation Team is responsible for the preparation, implementation and monitoring of the Local Transport Strategy and other related strategies (cycling, walking etc), including project development; inputting to the preparation and delivery of the Regional Transport Strategy and other work of Nestrans while carrying out of traffic modelling to inform strategy and project development as well as determining project feasibility and assessment; promotion and implementation of green travel planning; management of roads design, roads development management and construction consent functions; management of the Public Transport Unit; and project management of Council and Nestrans transport schemes and site supervision of Council roads projects.

Assets

Office Location

- 27 – 29 King Street – NESTRANS, ACSEF & Aberdeen City & Shire Strategic Development Planning Authority (SDPA), responsibility for which is shared with Nestrans, ACSEF and Aberdeenshire Councils.

Park and Ride facilities

- Kingswells Park and Ride (own land and building)
- Bridge of Don Park and Ride (rent land and own building)
- A96 Park and Choose (own land – facility due to be built in near future)

Public Transport Unit

- Bus stops in terms of shelters, bus stop flags on lampposts or bus stop poles are the responsibility of the Public Transport Unit.
- Parking facility (currently car park at the former Causewayend Primary School)

Roads Projects – buildings currently held for the Berryden improvement corridor

Residential properties

- 168a Hutcheon Street
- 170 Hutcheon Street

Non-residential properties

- 78-90 Powis Terrace Workshops
- Remains of Berryden Business Centre, Berryden Road

Section 3 – Anticipated Changes to the Service

3.1 Office Locations

The majority of Teams are located on the Ground Floor North in the corporate headquarters – Marischal College. The PTU is currently located on the Second Floor West; consideration is being given to co-locating the team with the rest of P&SD on the Ground Floor North within the next 12 months. It should be noted that staffing numbers within P&SD are predicted to remain relatively stable over the next financial year. The AWPR Team is located in Willowbank House. NESTRANS and Aberdeen City and Shire SDPA are located in King Street. It should be noted that Aberdeen City Council is only one of the funding partners for the AWPR, NESTRANS and Aberdeen City and Shire SDPA. The office locations outwith Corporate Headquarters will be reviewed each year for suitability and sustainability. It is possible that following completion of the legal process, including an appeal to the Supreme Court, the AWPR team will require further accommodation for the implementation phase of the project. Consideration will also be given to the location some staff on site during the construction period. Suitable accommodation would need to be identified for this purpose.

3.2 Park and Ride facilities

There is no anticipated change to the Kingswells park and ride site. There is, however a feasibility study currently being carried out on the possible relocation of the Bridge of Don Park and Ride. The A96 Park and Choose site has also been acquired, planning permission has been granted, and work is underway to deliver this within the AWPR contract.

3.3 Background on the Bridge of Don Park and Ride relocation feasibility study

The current Bridge of Don Park and Ride car park was opened in 1994 on land adjacent to the Aberdeen Exhibition and Conference Centre. This land has been leased to Aberdeen City Council over the intervening years on the understanding that a new car park will be constructed on a permanent site.

The process of identifying new site options for Park and Ride provisions for the north of the city centre commenced at the start of 2011. A multi-disciplined project team comprising officers from Aberdeen City and Shire Councils, Nestrans and the Strategic Development Planning Authority along with stakeholder involvement has led the project.

The outcome from the initial work undertaken by the project team was a proposed short list of options to be investigated further. These options are as follows:

Appendix C

1. Do nothing
2. Do minimum
3. Developer Led site at Blackdog
4. Developer Led site at Berryhill/ Cloverhill
5. Satellite sites
6. AECC car park

The final assessment of the above is near completion and the outcome will be reported to the Enterprise Planning and Infrastructure Committee on 31 May 2012.

3.4 AWPR

In order to facilitate the development and construction of the AWPR scheme a number of predominantly residential properties have been acquired by Scottish Ministers. A total of 18 properties have been acquired to date with further properties required to be purchased prior to commencement of construction. As these further properties are purchased, they will be added to the Service's asset portfolio.

Following conclusion of the statutory process for approval of the AWPR, 10 of these properties will be demolished prior to commencement, or during the early stages of, construction of the AWPR. At that time these will be removed from the Service's asset portfolio.

A number of these properties are currently let on a Scottish Short Assured Tenancy basis through property agents, while a small number are currently vacant.

It is expected that the properties not required for demolition will be capable of being resold following completion of the AWPR. It is anticipated that they will continue to be leased during the construction phase, reviewed on a property specific basis prior to construction commencing.

A further two non-residential properties, Parkhill Stables and Cattery and the former site of the International School of Aberdeen at Fairgirth, have also been acquired by Scottish Ministers in connection with the AWPR scheme. Additionally, a further 6 non-residential properties are to be acquired by Scottish Ministers, and thus will be included within the AWPR property portfolio prior to the commencement of construction.

Additionally, Scottish Ministers are currently considering the inclusion of the following Aberdeen City Councils infrastructure projects;

- i) The A96 Park and Choose project and associated Link Road, a new 999 space site along with a 0.5km link road, joining the A96 to the Dyce Drive at its junction with Argyll Road (which leads to the Aberdeen Airport), and

Appendix C

- ii) The Third Don Crossing, a new 2.4km single carriageway road and river crossing scheme aside Grandholm/Tillydrone Aberdeen.

Should this be approved, new property assets will be generated into the Service's portfolio. The delivery of both these projects will be determined by the completion of the statutory process for the AWPR, which is currently facing the possibility of legal appeal to the Supreme Court. If the desire was for the City Council to deliver these schemes sooner then it would need to give further consideration to other procurement routes.

3.5 Public Transport Unit

Bus stops and shelters are the responsibility of the Public Transport Unit. New bus stops/shelters are erected as and when required in response to changes in the bus service or following requests for new stops/shelters (where funding permits).

There are 1354 bus stops in Aberdeen City, 245 of these are shelters owned by Clearchannel, the rest, 1109, are owned by ACC. The 1109 bus stops belonging to ACC are made up of 741 bus stop flags on lampposts or bus stop poles and the remaining 368 are shelters.

The current location for parking vehicles is surplus to the Councils requirements and is being marketed for sale. The eventual sale will leave the PTU with no central parking facility.

3.6 Roads Projects - Buildings held for road improvements:

Roads projects hold a number of buildings for road improvements which are reviewed and purchased as new schemes are approved. It should be noted that the Compulsory Purchase Order has been confirmed for the Third Don Crossing, thereafter, the Asset Management Team enter into land negotiations and the outcome of these negotiations will determine if there are any properties to be purchased.

Section 4 – Asset Demand Profile (What do we need?)

In terms of further requirements, the AWPR and Roads Projects teams review / purchase and sell properties as required through different stages of project implementation within the agreed construction timeframe.

New bus stops/shelters are erected as and when required in response to changes in the bus service or following requests for new stops/shelters (where funding permits).

Following the planned sale of the former Causewayend School site there will not be an appropriate central parking facility for the PTU vehicles. An equivalent sized centrally located parking facility is required.

Appendix C

The possible relocation of Bridge of Don Park and Ride will be determined by a further report to the Enterprise Planning and Infrastructure Committee on 31 May 2012.

As stated previously, there may be a need for further office accommodation for the AWPR team, including on site, following completion of the statutory process.

Section 5 – Asset Supply Profile (What do we have?)

Appendix 1 details information on all aforementioned assets. A summary of the assets listed in appendix 1 is as follows:

- 1 x corporate staff office
- 2 x satellite staff offices
- 2 x park and ride
- 3 x workshops
- 2 x residential properties
- 1 x demolished business centre with sub station
- 1 x PTU parking area (Temporary)

A summary of part ownership of assets for the AWPR is as follows:

- 16 x residential properties
- 1 x stables & cattery
- 1 x school

Section 6 – Supply / Demand Comparison

As noted the current PTU parking facility is only temporary and new facility is required. Initial discussions between PTU and Asset Management have taken place.

The Powis Terrace units are in a very poor state of repair and would benefit from early demolition. However, it may be more cost effective to demolish them as part of the main roadworks contract for the Berryden improvement corridor as at present there is no capital budget allocation available to demolish the buildings.

The Berryden Business Centre was demolished in 2008/09. However, during the process it became apparent that the electricity sub-station served not only the Business Centre but also surrounding privately owned properties. Therefore the sub-station could not be removed without being replaced first. Due to the associated legal issues, the legal work and negotiations will need to be carried out well in advance of any on site work. The identification of appropriate funding for the Berryden Dualling would allow this work to be scheduled. Meantime the issue will be reviewed on an annual basis to identify if more immediate action is necessary.

Appendix C

The two flats on Hutcheon Street are currently vacant as the legal recommendation was not to rent on a residential basis due to tenure concerns. It is unlikely that the flats would suit any operational use, so the likelihood is they will remain vacant. As with all vacant assets they are circulated bimonthly to the Corporate Asset Group within the Vacant list.

The current mechanisms in terms of assessing the provision of future assets within P&SD are adequate for the future delivery of the Service. In summary:

- Staff office accommodation is assessed on a yearly basis for suitability and sustainability and it is acknowledged that as the AWPR progresses into the implementation phase there may be a requirement for further office accommodation.
- The AWPR and Roads Projects teams review / purchase and sell properties as required through different stages of project implementation plans.
- New bus stops/shelters are erected as and when required
- The possible relocation of Bridge of Don Park and Ride will be determined by a further report to the Enterprise Planning and Infrastructure Committee on 31 May 2012.

Section 7 – Preferred Solutions & Development of Outline 5 Year Plan

1) Park and Ride Sites

- The permanent site for the Bridge of Don Park and Ride will be determined by a further report to the Enterprise Planning and Infrastructure Committee. Target Completion – May 2012.
- Park & Choose site at the A96 Dyce Drive will follow the AWPR timeline which is detailed in section 3.4. Target Completion - The timing cannot be determined at present time.

2) Berryden Improvement Corridor Assets

- The Powis Terrace units would benefit from early demolition. However, there is no capital budget allocation for the Berryden improvement corridor – this will be reviewed on an annual basis
- Replacement of Berryden Business Centre sub station - there is no capital budget allocation for the Berryden improvement corridor – this will be reviewed on an annual basis
- Hutcheon Street - remain vacant but will be reviewed monthly by the Corporate Asset Group

3) PTU Parking Facility

Appendix C

- Continue to work with Asset Management to identify an alternative site.
Target Completion - TBC

Section 8 – Approval by SMT

To be presented for approval at appropriate meeting.

Section 9 – Comments of the Corporate Asset Group

Approved by the Corporate Asset Group on 25 April 2012. P&SD requested to include requirements for parking of PTU vehicles.

Appendix 1 – P&SD Portfolio of Assets

Staff Office Locations (excluding Marischal College)

Property Address	Property Type	Title	GIFA (sqm)	Area Hectares	Construction Year Approx	Condition Grade	Required Maintenance	Suitability Grade	Approx Running Costs 2010/11
27 – 29 King Street	Office	Freehold	107	0.0283	1818	B	0	B	21,300
Aberdeen Business Centre, Willowbank House, Willowbank Road	Office	Freehold	25.5	0.0607	1795	B	0	B	30,000

Park and Ride Facilities

Property Address	Property Type	Title	GIFA (sqm)	Area Hectares	Construction Year Approx	Condition Grade	Required Maintenance	Suitability Grade	Approx Running Costs 2010/11
Kingswells Park and Ride	Park & Ride	Freehold	66	7.28	2004	A	0	A	45,600
Bridge of Don Park and Ride	Park & Ride	Leasehold	66	1.86	2000	A	0	A	55,400
A96 Park and Choose	Park & Ride								

Road Project Developments – Properties held for the Berryden improvement corridor

Property Address	Property Type	Title	GIFA (sqm)	Area Hectares	Construction Year Approx
78-90 Powis Terrace	3 x workshops	Freehold	1,739	0.3	1960
168a Hutcheon Street	Vacant Flat	Freehold	31	0.004	1960
170 Hutcheon Street	Vacant Flat	Freehold	84	0.0061	1960
Berryden Business Centre, Berryden Road (electricity sub station)	Remains of a business centre	Freehold	30	0.2755	1920

Appendix C

AWPR Asset portfolio – It should be noted that the AWPR properties are owned by a consortium and the Council's share of any future sell-off following construction of the AWPR would be 9.5%.

Property Address	Property Type	Title	Occupied by tenants	Rental Band	ACC Council Tax Band	Demolition (Yes/No)
Dellwood, 2A Milltimber Brae, Milltimber, Aberdeen, AB13 0DY	Residential	Freehold	Yes	E	ACC Band H	No
The Sidings, Station Road, Milltimber, Aberdeen, AB13 0DP	Residential	Freehold	Yes	E	ACC Band G	No
Beech Hill, 309 North Deeside Road, Milltimber, Aberdeen, AB13 0DL	Residential	Freehold	Yes	E	ACC Band H	TBC
Carskeoch, 252 North Deeside Road, Milltimber, Aberdeen, AB13 0DJ	Residential	Freehold	Yes	D	ACC Band H	Yes
Pine Lodge, 315 North Deeside Road, Milltimber, Aberdeen, AB13 0DL	Residential	Freehold	Yes	D	ACC Band H	Yes
Kerloch, 2 Milltimber Brae, Milltimber, Aberdeen, AB13 0DY	Residential	Freehold	Yes	D	ACC Band H	No
Clarewood, 4 Milltimber Brae, Milltimber, Aberdeen, AB13 0DY	Residential	Freehold	Yes	B	ACC Band H	Yes
Birchlea, 250a North Deeside Road, Milltimber, Aberdeen, AB13 0DJ	Residential	Freehold	Yes	B	ACC Band E	No
Millbrae House, 6 Milltimber Brae, Milltimber, Aberdeen, AB13 0DY	Residential	Freehold	Yes	B	ACC Band G	Yes
Newhall House, Portlethen, Aberdeen, AB12 4RT	Residential	Freehold	Yes	B	ACC Band G	No
Bridgebank, 3 Milltimber Brae, Aberdeen, Milltimber, Aberdeen, AB13 0DY	Residential	Freehold	Yes	A	ACC Band H	Yes
Crofton, 5 Milltimber Brae, Aberdeen, Milltimber, Aberdeen, AB13 0DY	Residential	Freehold	Yes	A	ACC Band F	Yes
Broomhill, Blacktop, Kingswells, Aberdeen, AB15 8QL	Residential	Freehold	Yes	A	ACC Band E	No
1 Sunnybank Cottage, Craibstone, Aberdeen, AB21 9SP	Residential	Freehold	Yes	A	ACC Band D	Yes
2 Sunnybank Cottage, Craibstone, Aberdeen, AB21 9SP	Residential	Freehold	Yes	A	ACC Band D	Yes
Gairnlea, Blacktop, Milltimber, Aberdeen, AB15 8QL	Residential	Freehold	Not to be leased	NA	NA	Yes
Parkhill Stables and Cattery, The Laurels, Corsehill, Newmacher, Aberdeen, AB21 7XA	Non Residential		NA	NA	NA	No
International School of Aberdeen, Milltimber Brae, Milltimber, Aberdeen	Non Residential		NA	NA	NA	Yes

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise Planning and Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Community Council Business Development Grant Competition
REPORT NUMBER:	EPI/12/117

1. PURPOSE OF REPORT

This report highlights an opportunity to enable local Community Councils to support business development within their communities

2. RECOMMENDATION

It is recommended that committee support the delivery of a Community Council Grant Competition as outlined in the report.

3. FINANCIAL IMPLICATIONS

The Senior Officer for Employability, Skills and Community Enterprise has been on secondment to Aberdeen Business Improvement District from October 2010 to April 2012, during which full salary costs were recovered for the period of August 2011 – April 2012. In addition as there were no internal resources to continue the delivery of this service savings were also made from the Employability, Skills and Community Enterprise project budget. Agreement has now been received from finance that a sum of £ 30,000 can be carried over to be used for specific project delivery. This project enables revenue earned through the secondment of an Economic and Business Development staff member to be re-circulated into activities designed to promote enterprise within local Aberdeen communities. As such this activity is being funded at no net cost to the Council, using earmarked reserves.

4. OTHER IMPLICATIONS

Personnel

It is proposed if the recommendations in this paper are approved that staff from within Economic and Business Development Service will promote and manage the competition. It is anticipated that this will not

add any additional workload as the competition falls within the work plan of the Employability Skills and Community Enterprise Team.

Legal

Checks will be made to ensure each Community Council is properly constituted. The competition administered by a set of terms and conditions which will be passed to Legal & Democratic Services for consideration prior to issue

5. BACKGROUND/MAIN ISSUES

- 5.1 Community Councils are the most local tier of statutory representation in Scotland. They bridge the gap between local authorities and communities, and help to make public bodies aware of the opinions and needs of the communities they represent.

Their primary purpose is to ascertain and express the views of the community to the local authority and other public bodies. Many Community Councils also involve themselves in a wide range of other activities including fundraising, organising community events, undertaking environmental and educational projects, attraction of business to their area and much more.

The Scottish Government, in partnership with COSLA and the ASCC (Association of Scottish Community Councils) have developed 5 pilot projects involving Community Councils, focussing on budget management, elections and asset management, widening their areas of responsibility and increasing engagement with a more diverse cross section of their community. Although none of the pilot projects are within the Aberdeen city and shire area a precedent has been set to encourage the growth of the remit of Community Councils and ensure their ability to influence and positively impact the economic sustainability of their area.

Further to this a recent survey by the Association of Scottish Community Councils identified over 1,000 different activities which community councils have undertaken. These included road improvements, youth projects, environmental initiatives, upgrading of amenities, management of assets, business community engagement and community initiatives. Torry community council has been particularly active in expanding their role by establishing "Enterprising Torry".

- 5.2.1 Enterprising Torry is a new initiative from the Torry Community Council, designed as a forum for positive dialogue between businesses in Torry and the local community. The aim is to establish projects to encourage

business development in the area. To date a business breakfast and a cheese and wine evening have been held.

5.2.2 Currently there are 24 active Community Councils across the city, who may offer additional community services beyond their statutory function. This paper proposes that a one off grant pot of £ 30,000 be established to assist Community Councils develop the tools and support systems to support local business development and start up (commercial, community and social) within their local community. Increased business activity within the communities will increase vibrancy, create employment and training opportunities and work towards area regeneration.

5.2.3 Grants may be used to:

Develop information websites

Host business start up networking meetings

Support Business Plan Development

Promote Business Opportunities within the Community

Promote Business premises within the Community

Purchase legal advice

Offer Business Start up workshops within the Community

Initiate Community Asset management

Produce Marketing Materials etc.

5.3 Process

All 31 Community Councils within the city will be advised on the availability of the grant funding and invited to submit a proposal for funding up to £ 3,000

Officers will follow up with visits to those indicating an interest to outline the process and eligibility criteria.

Application packs will be issued to all those who note an interest

Applications will be assessed against a pre set criteria

Maximum of 10 x £ 3,000 grants will be issued.

Legal framework used for “Retail Rocks” will be modified to suit the project and reduce further impact on officer resources.

5.4 Outcomes

A successful project will work towards the establishment of new community based commercial, social and community businesses.

Profits may be reinvested back into the community

Increased opportunities for employment, training and work experience within the communities.

Opportunities to reduce the negative economic impact of the “Welfare reform Bill” on local communities.

Potential reduction of vacant properties

Increased community vibrancy and civic pride

Successful participation may generate interest and a desire to re convene the 7 inactive Community Councils

Reduction of unemployment ratio within communities

Increase in local entrepreneurial activity

Potential Co-operative formation

Increase in local services

Increased interaction between the Community Councils and the local Community Centre.

Local Community councils are empowered to assist the development of community based business

Reduction of Aberdeen City Council Staff resource requirement for Community Business Development

5.5 Finance

The senior officer for Employability, Skills and Community Officer has been on secondment to Aberdeen BID from October 2010 to April 2011, during which full salary costs were recovered for the period of August 2011 – April 2011. In addition as there were no internal resources to continue the delivery of this service savings were also made from the ESCE project budget. Agreement has now been received from finance that a sum of £ 30,000 can be carried over to be used for specific project delivery. Therefore this proposed activity can be delivered at no financial cost to the council.

6.

IMPACT - CORPORATE

This report relates to the Single Outcome Agreement:

- National Outcome 1 : We live in a Scotland that is the most attractive place for doing business in Europe;
- National Outcome 2: We realise our full economic potential with more and better employment opportunities for our people.
- Local Outcome – Diversification of the Economy

Vibrant, Dynamic & Forward Looking

Ensure the sustainability of the successful economy of Aberdeen City and Shire

Continue to drive regeneration and take advantage of regeneration opportunities city wide as they present themselves and identify, in consultation with the local communities, immediate priorities for regeneration projects

The report also supports the Five year Business Plan strategic priority:

Ensure a sustainable economic future for the city.

The report also supports the main aim of Enterprise, Planning and Infrastructure, which is:

Enable Aberdeen city to achieve increased sustainable economic growth for the benefit of all citizens, (by) ensuring people and communities are genuinely engaged in decisions relating to enterprise, planning and infrastructure issues in the city.'

IMPACT – PUBLIC

This paper is likely to create interest from members of the public who either participate in or have an interest in their local Community Council.

7. BACKGROUND PAPERS

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	31 st May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Positive Procurement Programme– Supplier Development Programme
REPORT NUMBER:	EPI/12/116

1. PURPOSE OF REPORT

This report will highlight the benefits of withdrawing from the national “Supplier Development Programme” and delivering a locally developed “Positive Procurement Programme” that will fit closer with local business needs and offer support to an increased number of local businesses.

2. RECOMMENDATION(S)

- a. It is recommended that committee support the withdrawal from the national “Supplier Development Programme” which currently costs Aberdeen City Council £ 7,000 per annum
- b. It is recommended that committee give approval for the funding for the development and delivery of a Localised “Positive Procurement Programme” of up to £ 7,000 for the year 2012/13 to be funded from within the existing Employability, Skills & Community Enterprise budget

3. FINANCIAL IMPLICATIONS

If approval is received for the above recommendations then a sum of up to £ 7,000 will be allocated from within the Employability, Skills and Community Enterprise budget. Agreement has also been received from Aberdeenshire Council to partner in this activity to the sum of £ 7,000 which may see the annual costs for this initiative significantly reduced.

4. OTHER IMPLICATION

Risk Management

Given the current economic environment, one critical feature of the immediate future will be the importance of the public sector procurement for an increased number of our local business, charities and third sector. Failure to provide the appropriate tools for these sectors to participate

effectively in this process will reduce their opportunities for sustainability and business growth.

Personnel

It is proposed if the recommendations in this paper are approved that external support will be used to develop appropriate open learning and workshop materials. Staff from within both Aberdeen City and Aberdeenshire Council's Business Development Service will promote the new service and co-ordinate the monthly clinics. It is anticipated that this will not add any additional workload when the management of the Supplier Development Programme is dropped. Commitment has also been made by the Joint Procurement service that they will participate within the monthly clinics and offer support and guidance in the development stages where necessary.

5. BACKGROUND/MAIN ISSUES

The Supplier Development Programme was developed as a local authority partnership programme to offer small and medium enterprises the opportunity to grow and diversify through participation in public procurement.

Enterprise, Planning and Infrastructure Committee agreed on the 20th April 2010 to support the delivery of the Supplier Development Programme for a period of 1 year and if a positive evaluation was received, this service should continue to be made available to local businesses, third sector organisations and charities.

Both Aberdeen City and Aberdeenshire Councils committed to the delivery of this programme and were satisfied that the product met the needs of our local businesses and would ultimately enhance their success in public procurement tendering.

On evaluation of the services offered during 2011/2012 both Aberdeen City and Aberdeenshire Councils felt that the level of service provided by the Supplier Development Programme did not constitute "value for money" and that delivering 3 programmes across the North East did not merit the joint cost of £ 14,000.

During the contract period of 2011/2012, 25 companies attended a Tender Ready in 10 steps workshop in October 2011 and another 25 in March 2012. There were also 19 at How to Win More Tenders and Influence People. This equates to a per capita cost of £ 202.90 per business for a non accredited course, which does not represent value for money.

Additionally four other key concerns re the continuation of the contract are:

a) Companies based within the central belt can physically access programmes across a wide number of local authorities, improving the variety of accessible programmes and delivery dates that suit business needs. North East businesses would have to incur considerable travel costs and time investment to access this enhanced offering.

b) A number of local businesses that have tried to register with the Supplier Development Programme have been refused access due to

their size or turnover. In the current climate where public procurement contracts can exceed £ 50,000,000 it would be irresponsible of Aberdeen City Council to refuse assistance to local companies that have a desire to participate within this marketplace, and ensure they have an appropriate tool box at their disposal to ensure the optimum success rate and continued employment for their local employees.

c) The delivery of this programme still requires a significant input from Council officers in relation to administration and attendance at events.

d) There is often a considerable time lapse from notifying the Supplier development programme of the desire to run a workshop in the City and the offer of a date.

Going Forward

From full registration for the limited amount of workshops offered in 2011/12 we can justify an evidence of demand for this type of business support within both Aberdeen City and Shire. Evaluation of the workshops has been positive with many indicating that the tools identified will be used in future tendering activities and that many of the participants that had not previously participated within the public procurement arena now felt fully equipped to participate in the future. Our proposal going forward is that both Aberdeen City Council and Aberdeenshire Council jointly agree to support the development and delivery of a new “Positive Procurement Programme”. This initiative will be equally funded by both councils to the maximum sum of £ 7,000 each per annum, with the potential for reducing costs after the initial development period.

The key annual activity would be the delivery of an annual “Positive Procurement” conference. This conference would create opportunities for all attendees to meet with a wide range of product and service buyers from both the public and private sectors on a one to one clinic basis, host key note speakers and also a range of workshops on relevant procurement development techniques.

Although the workshop style delivery method has been successful it is felt that a larger number of businesses would participate if the relevant information was available online, with 24/7 access, offering the options to select modules that are appropriate for their level of expertise. It is our intention to work with the Joint Council Procurement Team and external procurement specialists to develop the online materials.

Supplementary to this it is our intention to host monthly procurement clinics with networking opportunities, this should both increase skills and knowledge in specialist areas and also encourage collaborative tenders in the future.

Suggested Topics for “Positive Procurement Programme” monthly clinics

- PQQ (Pre Qualification Questionnaire)
- Community benefit Clauses
- Third Sector Engagement
- Overseas tender process

Scottish Government Procurement Team
Financial capacity
Corporate Social Responsibility
Collaborative tendering
Environmental Impact/Carbon Footprint
Tendering No Nos
Quick wins
Successful tendering examples

In addition to relevant speakers it is our intention to have representation from the key local public procurement agencies e.g. Aberdeen City & Shire Councils, Grampian Police, NHS Grampian, Department for Work & Pensions, Fire Brigade to aid discussion and promote best practice.

Participants would be encouraged to suggest topics for future events on a regular basis.

Workshops

Prior to commencement of the delivery of the “Positive Procurement Programme” we will survey businesses that have registered with the Supplier Development Programme to ensure their needs will be met by our proposals. Feedback may identify the need to run occasional workshops for some companies at the early stages of procurement. This can be added to the programme at a later date if demand is sufficient.

This proposal if supported should offer a diverse range of business sustainability and development tools in mediums that would encourage greater participation and benefits for the same level of financial input. Companies can still access information on supplier procurement from other sources, including free of charge sources, such as www.publiccontractsscotland.gov.uk and the intention is that this local, tailored programme will complement, support and enhance companies’ efforts to win public contracts.

6. IMPACT

Corporate

The delivery of the “Positive Procurement Programme” will support the delivery of **Vibrant, Dynamic and Forward Looking** via the Economic Development stand by: Ensuring sustainability of the successful economy of Aberdeen City and Shire

Strive to buy goods and products locally (where this is possible) and encourage other public sector bodies and businesses in the city to adopt a similar approach.

Linkages to **Partnership for Aberdeen** will be by “Co-operating with neighbouring authorities and others to promote inward investment in the North-East.

Single Outcome Agreement

The delivery of the “Positive Procurement Programme” will also assist in delivery against the following outcomes within the Single Outcome Agreement.

National Outcome 1. We live in a Scotland that is the most attractive place for doing business in Europe

National Outcome 2: We realise our full economic potential with more and better employment opportunities for our people

National Outcome 3: We are better educated, more skilled and more successful

Public

This paper will be of interest to the local business community who are seeking alternative markets to ensure sustainability and growth.

All proposed activities within PPP have been assessed in relation to Equalities and Human Rights Impact Assessment.

7. BACKGROUND PAPERS

None

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise Planning and Infrastructure Committee
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	North Sea Commission and External Funding Update
REPORT NUMBER:	EPI/12/093

1. PURPOSE OF REPORT

This report provides an update on the Council's external funding initiatives.
This report will provide:-

- a briefing regarding the Council's membership of the North Sea Commission (NSC),
- nominate member for representation on NSC, thematic groups: Culture and tourism group, Innovation Education group, Marine Resources group, Sustainable Development group, Transport group and CPMR
- Nominate member for representation for East of Scotland European Consortium (ESEC)
- seek approval for Member travel for North Sea Commission and CPMR
- request approval for the development of new transnational projects
- request approval for Officer travel in connection with the development of said projects and activities
- provide an update on current transnational projects
- provide an update on the Grantfinder software package
- provide an update on the current status of European Funding Programmes
- and also to note the success of the External Funding Unit in drawing down £287,231 of in external money with the potential of £18,906,455 should the decision pending applications be approved and funded
- Joins the International Local Governments for Sustainability (ICLEI) as a Full Member.

RECOMMENDATION(S)

That the Committee:

1. Nominates and approves both Member and Officer travel and attendance to relevant NSC meetings (There will be 4 NSC Executive meetings at various times throughout 2012) Officer travel to European Project development meetings within Europe Member States in 2012/2013. Locations and timings of project development meetings will vary in the year as and when the lead partners call for the meetings.
2. Approves officer travel to project meetings associated with the "Future Participation in EU Funded Projects" outlined in section 6 of this report.
3. Notes the success of the Council drawing down £287,231 in 2011/12 of external monies to be utilised in EU projects over a period of three years with the potential of £18,906,455 should the pending decision applications being funded, and
4. Approves that the Council joins the International Local Governments for Sustainability (ICLEI) as a Full Member designating the Council Leader or delegate as primary political contact for ICLEI.

2. FINANCIAL IMPLICATIONS

The programme has been developed in line with a broadly similar level of funding as in 2011/2012.

- Costs for the Elected member attending various meetings of the NSC will be met from the Elected Members travel Budget and will cost approximately £3,000.
- Costs for the Officer accompanying Elected Members (Approximately £3,000), officers attending NSC and project development meetings will be met from the relevant Service's Budget. 50% of these costs will be recovered from the projects if successful. Where the Senior Partnerships, Performance and Funding Officer has to travel on NSC thematic group work all costs are covered by the North Sea Commission
- Match funding to partner EU funded projects will be met from existing budgets, from the relevant Services. The attached spreadsheet breaks down each project and which Service match funding will be met from. Any new projects will be forwarded to Committee for consideration, subject to budgets being available.

- The cost of travel in connection to promoting Aberdeen’s European and sustainable activities has been provided for within the budget and business planning process. Where such promotion relates to a European Projects 50% match funding is available.
- The annual cost to Join The International Local Governments for Sustainability (ICLEI) is currently €2,250. Joining this organisation provides access to a variety of conferences, research and development, information and services including training at discounted prices This cost is able to be made from existing budgets.

4. SERVICE & COMMUNITY IMPACT

European policy, representation and best practice supports the Council’s objective of representing the interests of the people and organizations of Aberdeen within North East Scotland, the UK, and Europe. The Europe 2020 Strategy aims for a Smart, Sustainable and Inclusive Europe which is reinforced in the Council’s 2012-2017 five Year Business Plan. It also facilitates greatly the promotion of Aberdeen’s City Vision, to be “a city which is vibrant, dynamic, forward looking – an even better place to live and work, where people can expect high-quality services that meet their needs”.

Several of the challenges of the Community Plan identified in the Single Outcome Agreement are also supported, such as: Leading the City, Being Informed, Getting Involved, Land Use and the Environment and Clean City, Transport and Connections, Prosperity and Jobs, and Aberdeen’s Image.

5. OTHER IMPLICATIONS

- Travel

There will inevitably be some risks associated with overseas travel. As far as possible, measures relevant to the destination will be taken to reduce any risks in line with Council Policy.

- Risk Management

It is important that Aberdeen is fully represented in all European matters and that all opportunities for influencing policy and the development of transnational projects for the City’s benefit are grasped in order that opportunities are maximised. This level of participation will help to increase the understanding of the new programmes and will assist in developing the capacity to successfully develop new projects and work with other European cities that will benefit Aberdeen City.

- Human Rights/Equalities/Diversity

European Policy takes full account of the Equal Opportunities Policy of the Council.

- Sustainability

Aberdeen City Council has a clear contribution to make to sustainable development with an emphasis on enhances productivity through improved

resource efficiency, new business opportunities in the developing market for sustainable technologies and delivering better and more efficient Council services. By attending these events, it will give us the opportunity to look at the possibilities of best practice and drawing down more funds from EU programmes to raise the quality of life through increasing economic opportunity for all on a socially and environmentally sustainable basis.

- Environmental

European activities are implemented in line with the aim of achieving a quality and sustainable environment.

- Social

The principles of Social inclusion underpin European cohesion policy.

- Economic

European representation and inter-regional networking contributes to the City's overall effort to maximize opportunities and to promote its interests, both at home and overseas with the aim of creating a vibrant and sustainable economy.

6. REPORT

- **Conference of Peripheral and Maritime Regions and North Sea Commission (CPMR) - Travel and attendance at CPMR, NSC and Project Development meetings**

In order for the Council to take full advantage of our role in influencing European Policy, and EU Funding Programmes. As Such it is important that the Council, is a member of the CPMR

Six commissions make up the CPMR to promote and defend the interests of 150 member regions.

The CPMR decided to organise itself into geographical commissions in order to make it easier to articulate the particular circumstances of each of the major sea basins.

Each geographical commission has its own organisational structure, so that it can promote its specific identity and cooperate on subjects of common interest, while contributing to the cohesion and unity of the Conference.

The CPMR "**Energy - Climate**" working group was created by the CPMR in 2004.

Its objectives are:

- In general, to develop a comprehensive approach to the energy issue, but

with special attention on the energy balance and the use of energies of marine origin.

- To reflect on the nature of the relationship between production sites and places of consumption of energy in the context of a market economy and within the framework of the European Union and in particular, to promote the energy potential of Peripheral and Maritime Regions.
- To better understand, given the experience of the various regions, the places of power and decision-making in terms of energy.
- To promote better forms of governance amongst other things, to better manage conflicts that may occur in a region where the development of energy production is fraught with various oppositions.

To this end, the "Energy - Climate" Working Group intends:

- To inform and advise all the CPMR on matters relating to energy,
- To lead in accordance with decisions taken by the CPMR bodies, the actions necessary to defend the specific interests of Peripheral and Maritime Regions by intervening as appropriate to the European institutions during the development and implementation of European laws.
- To promote the exchange of experiences and good practices between the Peripheral and Maritime Regions.

The CPMR Energy - Climate Working Group is currently chaired by the [Autonomous Region of Navarre](#).

Its work is developed by a Steering Committee and coordinated by Mr Jean-Didier Hache.

- **North Sea Commission Information**

The North Sea Commission was founded in 1989 to facilitate and enhance partnerships between regions which manage the challenges and opportunities presented by the North Sea. Furthermore, to promote the North Sea Basin as a major economic entity within Europe, by encouraging joint development initiatives and political lobbying at European Union level.

The North Sea Commission has determined that its activities must be action orientated, involving co-operation programmes, research activities, funding applications, and joint policy statements which bring positive benefits to the people of the North Sea Basin.

By being a member of the CPMR, the Council is also a member of the North Sea Commission, with Cllr Dean being the Scottish Representative (Elected by the Scottish Local Authority members of the NSC) and Chair of the Innovation and Education Group.

It is of utmost importance that we continue to be well represented on the North Sea Commission and on the various EU funding programmes in order that we are kept fully informed of all new European Policy and funding opportunities through its continuing attendance and input into the Conference of Peripheral

Maritime Regions (CPMR), NSC, the various thematic groups and project development meetings. Such activity will also contribute in us being able to draw down external monies to develop and deliver projects for the Council.

The advisor to the Sustainable Development Group of the NSC (Currently the Senior Partnerships, Performance and Funding Officer) will be travelling to various meetings at various times on behalf of the NSC and all costs are covered by NSC.

It should be noted that the NSC has recently adopted a NSC 2020 strategy and have identified the following challenges that face the North Sea Region..

- Managing Maritime Space
- Increase Accessibility and Attractiveness
- Tackling Climate Change
- Promoting Innovation and Excellence
- Sustainable Liveable Communities

- **Other external funding opportunities**

The external funding team is currently working with a variety of city development projects to seek external funding opportunities through other funding sources eg the United Kingdom and Scottish Governments. Many of these opportunities are focussed on enabling the Smart Cities agenda.

- **International Local Governments for Sustainability (ICLEI)**

In the past years Aberdeen City Council has participated in many ICLEI events. ICLEI is a partner in the Interreg North Sea Programme LOWCAP Project (Lead Partner) and the Care North Project (Partner). Over 1200 cities, towns, counties, and their associations worldwide comprise ICLEI's growing membership. ICLEI works with these and hundreds of other local governments through international performance-based, results-oriented campaigns and programs. In the UK cities such as Glasgow, Birmingham, Bristol and London are members of ICLEI. There are many benefits that ICLEI provides members with the major ones being:-

1. connecting members with other local government leaders and to other relevant actors, including businesses and the academia,
2. help to accelerate cities' actions towards more resilience at the local level, which will contribute to enhanced sustainability globally,
3. providing Members with access to a gateway to solutions, which supports mobilising local visions to actions, and
4. an opportunity to contribute to forming the voice of local authorities before international institutions such as the European Union and United Nations
5. Help respective member develop Policy and also help influence EU Policy

In relation to Aberdeen, membership to ICLEI will assist building partnerships that will enable future external funding bids and influence policy at European level.

East of Scotland European Consortium (ESEC)

ESEC's activity is delivered in accordance with an agreed annual workplan. There are six main areas of activity contained within it including:

- Structural Funds (Implementation)
- Project Development
- Regional Policy
- Environment and Energy
- Rural and Coastal Issues
- Smart Cities and Communities

- **Provide an update on current transnational projects**

1. Complex Challenges Innovative Cities (CCIC) (Interreg IVC)

This newly funded project under Interreg IVC will aim to work in partnership with the two Aberdeen Universities in order to promote innovation. Aberdeen City Council currently have an active public open data programme which requires a better approach to understand uses that business and community could make of datasets that ACC hold, this project will address this issue.

Aberdeen City Council will also engage with the ICT sector, life-sciences, energy sector, telecommunication network operators and the wider SME community.

This project will allow ACC to enhance the flow of innovative ideas from Universities and Businesses by working together with the existing EU partnership.

This project links to the following Council priorities as set out in the Five Year Business Plan:

- Ensure a sustainable economic future for the city
- Ensure efficient and effective delivery of services by the council and with its partners
- Encourage the growth of local businesses through support of existing business sectors and development of new sectors
- Working with partner organisations

2. A Low Carbon Action Plan for the North Sea Region (LOWCAP) (Interreg IVB North Sea Region)

The Low Carbon Regions in the North Sea (LOWCAP) cluster project will exchange knowledge and experiences from four carbon reduction and energy efficiency projects (Build with Care, Care North, North Sea Sustainable Energy Planning and Carbon Capture & Storage) in the North Sea Region.

Aberdeen City Council, as project lead will bring together key results from the partner projects and other related North Sea Region projects. The findings will be shared with stakeholders in the North Sea Region, including decision makers in the public sector and end users from business and communities.

Through a review of EU programmes and the most recent literature, LOWCAP will produce policy advice for the North Sea Region in regards to carbon reduction and energy efficiency.

3. Sustainable Urban Fringes (SURF) (Interreg IVB North Sea Region)

SURF is in the preparation stages for the project final outputs. The toolkit is in draft and the final report with policy recommendations and governance insights is due to go to print at the beginning of May. The final conference, where the project outcomes will be disseminated will be hosted by Aberdeen City Council on the 27th - 29th June. Presently SURF is pursuing a 6 month extension to allow promotional opportunities for the project to continue including hosting seminars at conferences, testing the toolkit and promotion of the SURF position paper towards the European Commission. Emma Watt has written a separate Committee Paper which further details the SURF project progress and the extension details.

4. Build with CaRe (BWC) (Interreg IVB North Sea Region)

BWC officially ended in March 2012. Two Council officers attended the final conference in Brussels & the Netherlands. Work is being undertaken on the writing of the final activity report and collating the work package results which will be distributed to interested parties in the coming months.

The final financial claim is also being collated which will be submitted at end of April.

5. North Sea Sustainable Energy Planning (NSSEP) (Interreg IVB North Sea Region)

NSSEP is due to end in August 2012, however the project has applied to the NSR secretariat for a 3 month prolongation until November 2012. At present we are awaiting official confirmation of the prolongation however this will not result in any additional funding for Aberdeen City Council. The prolongation is due to another partner pulling out of the project and the workload being redistributed.

6. Innovative Management for Europe's Changing Coastal Resource (IMCORE) (Interreg North West Europe)

The IMCORE project activities officially ended in December 2011 and the final financial report has been submitted to the lead partner Cork County Council. Correspondence will still continue with the Council and Cork County Council until full confirmation and approval has been given by the North West Europe Secretariat that they are satisfied with the final findings. There are currently no plans to extend on the findings from IMCORE in terms of European projects.

7. Motorways of the Sea (Interreg North Sea Region)

Motorways of the Sea has now officially ended and all finance and activity reporting has been completed. There will be no follow up to this project from the Council.

8. Mitigation in Urban areas to create Solutions for Innovative Cities (MUSIC) (Interreg North West Europe)

The MUSIC project is currently at the half way stage mark and will conclude in March 2014. Aberdeen has already hosted one of the partner conferences which proved successful in demonstrating to partners the Aberdeen element to the project. The project is on track in terms of finance and activity. An officer for the funding time will be attending a Music conference in Ghent in November this year which will provide an opportunity to meet with individuals who were instrumental in helping Ghent achieve a high status in terms of cities and culture. This will all be beneficial to Aberdeen in particular when bidding for City of Culture status in 2017.

9. Lively Cities (Interreg North West Europe)

The Lively Cities project is currently in the first year development stages and will conclude in December 2014. Lively Cities has played a role in the commissioning of public art policy for the city. One aspect of the project concentrates on staff exchanges in which staff from Aberdeen City Council events team will travel to Eindhoven to meet with EU counterparts who were involved with Eindhoven's City of Light. This will be a good exchange of best practice and in particular providing ACC staff with knowledge and expertise to replicate into the City of Culture bid in 2017.

10. Care North (Interreg IVB North Sea Region)

Care North is in the final year of project completion, there has been discussions with the lead partner in Bremen to extend the project for a further 6 months. The Aberdeen City Car Club launch was a success and the Care North project has been linked closely to the car club with a great deal of promotional work been undertaken. The LEZ which was originally associated with the project has since been shelved as per a separate committee report by Louise Napier.

11. Peri Urban (Interreg IVC)

The Peri Urban project is in the final months, due for completion in September 2012. The project is on track to deliver activities and financial expenditure. A final report on the Peri Urban project and the impacts on Aberdeen City will be written and delivered in the near future.

- **Future Participation in EU Funded Projects**

The Council has been invited to lead and participate in the following projects:

1. Hydrogen Transport Economy for the North-Sea Region (HyTrEc) (Interreg North Sea Region) (Lead Partner)

This project will support the validation, promotion and adoption of innovative hydrogen technologies across the NSR and enhance the region's economic competitiveness within the transport and associated energy sectors. The project will provide a platform to support the collaborative development of strategy and initiatives (at regional, supra-regional and transnational level) and that will inform and shape the development of infrastructure technology, skills and financial instruments to support the application of hydrogen based technologies across the North Sea Region.

The project partnership is made up of 8 partner organisations representing the United Kingdom, Germany, Belgium, Denmark and Sweden.

2. High. V.Lo City – Cities speeding up the integration of hydrogen buses in public fleets (Joint Tecnology Initiative) (Partner)

Several European bus manufacturers consider the hybrid fuel cell (FCH) bus as the most promising technology to facilitate the decarbonisation of public transport. By leveraging the experiences of past fuel cell bus projects, implementing technical improvements that increase efficiency and reduce costs of FCH buses, as well as introducing a modular approach to hydrogen refuelling infrastructure build-up, the High V(Flanders).L(Liguria) O(Scotland)-City project aims at significantly increasing the “velocity” of integrating these buses on a larger scale in European bus operations.

The project will address the following key issues: Increase energy efficiency of the buses and reduce cost of ownership:

- hydrogen consumption down to 7–9 kg H₂/100km
- integrating latest drive train and battery technologies
- availability of 90% without the need of permanent support
- >12.000 hours warranty and decreased additional warranty cost
- increase lifetime of key components as fuel cells and batteries.
- Investment cost <1,3 million euro
- Reduce the cost of hydrogen supply:
 - Aberdeen: making use of an existing hydrogen production and distribution channels
 - Consolidate past, current and future fuel cell bus demonstration activities by creating an active dissemination
 - network of Hydrogen Bus Centres of Excellence in collaboration with the Hydrogen Bus Alliance, Global
 - Hydrogen Bus Platform, HyRaMP and JTI hydrogen bus demonstration projects.

More specifically High V.LO City will:

- Build on the experience of Van Hool the USA (21 buses 2005-2010)
- Link Liguria, Antwerp, and Aberdeen, with already existing activities in United Kingdom (London), the Netherlands (Amsterdam and Arnhem), Germany (Cologne, Hamburg Berlin) Spain (Madrid, Barcelona) and Sweden (Stockholm and Malmoe).

The partnership in this project consists of 12 partnering organisations representing the UK, the Netherlands, Italy, Belgium and Denmark.

4. HyTransit – European Hydrogen Transit Buses in Scotland (Joint Technology Initiative) (Partner)

HyTransit will trial a fleet of eight hybrid fuel cell buses in intercity fleet services, together with one state of the art hydrogen refuelling station in Aberdeen for four years. This project is designed to contribute to the commercialisation of hydrogen buses in Europe by:

- Bringing together an industrial consortium from across Europe to deliver the project, including buses from Van Hool (Belgium) and state of the art refuelling technology from Linde (Germany).
- Develop eight A330 hybrid fuel cell buses specifically modified for intercity and shuttle driving.
- Generating new Intellectual Property for Europe by developing the concept design for the world's first hybrid fuel cell coach for long-range transit applications.
- Exposing the eight buses to real world operation with exactly the same service requirements as diesel buses, with 14 hours and 200km per day operation.
- A state of the art hydrogen refuelling station will be constructed to serve the bus fleet. The station will be based on ionic compressors, configured to allow a refuelling speed of up to 120 grams per second.
- Taking the first step for a large-scale rollout of hydrogen buses in Scotland. The next logical step after this project is Scottish Government support for the deployment of a minimum of 50 buses. This project will be the first step to realising this vision for Scotland.

The overall project objective is to prove that a hybrid fuel cell bus is capable of meeting the operational performance of an equivalent diesel bus in intercity service, whilst considerably exceeding its environmental performance.

This will be achieved by bringing together a primarily industrial consortium from five member states to develop, deploy and then monitor the buses in day to day service, with an overarching aim to demonstrate an operational availability for the buses equivalent to diesel (over 90%).

The project will also address the main commercial barrier to the technology (namely bus capital cost) by deploying state of the art components, which will reduce the unit cost of the bus to below 1.1 million euros for the first time. Results of the project will be widely disseminated to the general public. In addition, a more targeted approach will be adopted towards the key stakeholders who will be responsible for decisions on the next steps towards commercialisation of the technology.

The partnership consists of 9 partnering organisations representing the UK, Germany, Belgium and Denmark.

5. Stopping the Cycle of Gender Based Violence (Daphne III Funding Programme)

Working in close partnership with Grampian Police, Drugs Action, Aberdeen Cyrenians, Alcohol Support, NHS Grampian, Aberdeen Foyer, Crown Office and Procurator Fiscal this project will seek funding in order to adopt and create a safe and supportive environment for children and young people to be

able to make positive choices relating to relationship and domestic/gender based violence in their lives.

This project links to the following Council priorities as set out in the Five Year Business Plan:

- Reduce antisocial behaviour, crime and the fear of crime
- Working in partnership
- Maintaining high profile within communities
- Addressing domestic violence
- Addressing sexual exploitation
- Provide for the needs of the most vulnerable people

The project partnership consists of partners across Europe representing the UK, Malta, Slovenia and Italy.

It should be noted that the above projects have all been submitted to their respective funding bodies and are currently awaiting approval. Should the projects be successful they will commence later in the year of 2012. The table below, as well as the attached funding spreadsheet outlines the financial implications and income generated should they be successful.

6. Access NSR (Enhancing Accessibility by Sustainable Urban Mobility in the North Sea) (Interreg North Sea Region)

Good and sustainable accessibility is of vital importance for a successful economy, contributing to economic growth, job creation and competitiveness in the North Sea Region. Many cities and urban areas act as regional, national and international gateways and carry extensive movements of goods and people. However the growth of the transport sector and the enlarged mobility of people also entail big problems such as congestion and emissions pollution, involving increasing social, environmental and economic costs. The project "ACCESS-NSR" seeks to address these issues and to offer solutions in order to improve accessibility and deliver economic, social and environmental benefits, promoting the sustainable development of the North Sea Region at large. For this project Aberdeen would intend to look at expansion of Journey Time Information for all road users on strategic routes into the City and look to continue feasibility and design work for redesigning the City for when the AWPR comes on line.

7. U.do more Car Sharing (Car Club Project) (Intelligent Energy Europe IEE)

U.do more car sharing (car club in UK) is a project focussed solely on car clubs. Having newly launched our car club with Commonwheels, Aberdeen would look to replicate best fit projects from Edinburgh and Bremen (Germany) in Aberdeen. Going in as a learning City Aberdeen would benefit from experienced cities in adopting alternative fuels, target marketing, urban regeneration and sustainable expansion of the car club. Aberdeen is also

able to contribute to good practice with regards to supplementary Planning guidance and policy background and this project would allow the City to match fund implementation of the Planning aspects of the car club.

- **Provide an update on the Grantfinder software programme**

For the past eight years the External Funding Team has held the license for the grant software programme Grantfinder. This tool is a useful resource for the team to search available funding streams. The license also provides a tool for the Community to access a similar, less complex, site GrantNet which they can search for specific areas of funding for the community. This service is regularly used by community groups and local charities.

The license was up for renewal at the beginning of this year and it was decided to continue the license for the next three years. The cost for the license over the three years is £11,791. The benefit of Aberdeen City Council hosting this will be in the form of the Community being able to search for external funding as opposed to relying on the internally operated grants that have been relied on in the previous years.

- **Provide an update on the current status of European Funding Programmes**

The current Interreg funding programmes come to a close at the end of 2013 as they are part of a funding period lasting 7 years. With this in mind it should be noted that the funding allocated for the 7 years will be diminishing with little remaining for the final year. This, in turn, means that it is unrealistic to expect any significant investment for the City during the remainder of the current programme.

The new programme period 2014-2020 is currently in the development stages where the new priorities will be outlined, there is no information on the new priorities at the time of writing this report. The focus of the new programme will be aligned with the EU 2020 strategy and this is something that the External Funding Team is aware of and anticipating. The team are currently developing a Smart Aberdeen 2020 document which will ensure that future development and projects within the City are in line with the EU 2020 strategy, therefore aligning with the future funding programme with the goal being to secure additional funding over that period.

- **Notes the success of the Council in drawing down £287,231 in external monies to be utilised in EU projects over a period of three years.**

The Council was successful in drawing down **£287,231** in 2011/2012. These funds are for the following projects, which cover a period of up to three years.

Awarded Projects 2012	ACC Money	EU Money
CCIC	£32,927	£98,790
LOWCAP – Aberdeen Project	£73,921	£73,921
LOWCAP – Management Fee Income		£114,520
Total	£106,848	£287,231

The Council have submitted 5 new funding applications which are awaiting award decision, there is the potential to drawn down a further **£18,906,455** in external monies in 2012 for a period over three - five years.

Projects Awaiting Grant Decision 2012	ACC Money	EU Money	Private Sector/ other Public Sector	Total Income
Hytrec – Aberdeen Project	£182,451	£182,451		£642128
Hytrec – Management Fee Income		£459,677		
Stopping the Cycle of Gender Based Violence	£26,579	£106,317		£106,317
High V Lo City & HyTransit (12 Buses)	£2,000,000	£8,529,000	£9,629,000	£18,158,000
Total	£2,209,030	£9,277,445	£9,629,000	£18,906,445

It must be noted that most European Funding programmes require the Council to identify and put in place match funding and the rate of match funding can vary from programme to programme. The attached spreadsheet identifies the Services in where the match funding is being met from, the spreadsheet includes existing projects as well as the potential project which have been submitted for 2011/2012 to be implemented over the next three to five years.

7. REPORT AUTHOR DETAILS

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8. BACKGROUND PAPERS

None

Projects Funded 2012	Funding Programme	Project Dates	Project Contact	Service	Total Project Cost	ACC Contribution
LOWCAP - Aberdeen Project	Interreg - NSR	Jan 2012 - June 2013	Andrew Win/Charlotte Kedsle	E,P&I	£147,842	£73,921
LOWCAP - Management Fee	Interreg - NSR	Jan 2012 - June 2013	Andrew Win/Charlotte Kedsle	E,P&I	£114,520	£0
CCIC	Interreg - IVC	Jan 2012 - December 2014	Elaine Robertson/Susan Fraser	E,P&I	£159,964	£39,991
HyTrec	Interreg - NSR	June 2012 - June 2015	Yasa Ratnayeke	E,P&I	£364,902	£182,451
HyTrec - Management Fee	Interreg - NSR	June 2012 - June 2015	Yasa Ratnayeke	E,P&I	£459,677	£0
HyTransit & High V Lo City	FP7		Yasa Ratnayeke	E,P&I	£20,158,000	£2,000,000
Total					£21,404,905	£2,296,363.00

Projects Approved for Funding
Projects Pending Approval Decision

EU Funding Contribution	Private Sector Contribution
£73,921	n/a
£0	n/a
£119,973	n/a
£182,451	n/a
£0	n/a
£8,529,000	£9,629,000
£8,905,345	£9,629,000

ABERDEEN CITY COUNCIL

COMMITTEE Enterprise, Planning and Infrastructure

DATE 31st May 2012

DIRECTOR Gordon McIntosh

TITLE OF REPORT International Partnerships & Twinning Applications
REPORT NUMBER: EPI/12/0124

1. PURPOSE OF REPORT

The purpose of this report is to bring before Committee applications for financial assistance from the International Twinning Budget 2012-2013 and to make recommendations.

2. RECOMMENDATION(S)

That the Enterprise, Planning and Infrastructure Committee recommend the approval of the following grants from the International Twinning Budget 2012-2013:

- i) £2,500 towards the participation of strong-men in the 'Aberdeen Highland Games Tri-Twinning Challenge', from the 15th to 18th June 2012.
- ii) £2,600 towards the 21st Aberdeen Scouts visit to Gomel, 10th to 17th October 2012.
- iii) £7,000 maximum contribution towards the visit of a youth theatre group from Regensburg to produce a theatre production with Aberdeen Performing Arts, 31st July to 12th August 2012.
- iv) £2,180 maximum contribution towards the participation of an artist from Clermont-Ferrand in the Aberdeen Art Fair, 17th to 19th August 2012.
- v) £6,950 towards the participation of a football team from Stavanger in the Denis Law Tournament, 15th to 21st July 2012.
- vi) £1,000 contribution towards sending a loan exhibition and the curator from The Gordon Highlanders to the Regional Gomel Military Museum and to be a representative in the 'Intermuseum' competition, 7th to 16th October 2012.

3. FINANCIAL IMPLICATIONS

Full council approved the International Twinning Budget for 2012-2013 of £137,350. The approved budget from the Common Good Fund, provides £102,000 of monies towards the support of twinning projects and/or visits.

	Balance of Budget Available	£102,000
i)	Contribution towards the participation of two strong men from Regensburg, Clermont-Ferrand and Stavanger in the Aberdeen Highland Games, 15 th -18 th June 2012.	£2,500
ii)	Contribution towards the 21 st Aberdeen (Cults) Scout visit to Gomel, 10 th - 17 th October 2012.	£2,600
iii)	Maximum contribution towards the visit of a youth theatre group from Regensburg to participate in a production by Aberdeen Performing Arts, 31 st July - 12 th August 2012.	£7,000
iv)	Maximum contribution towards the participation of an artist from Clermont-Ferrand in the Aberdeen Art Fair, 17 th -19 th August 2012.	£2,180
v)	Contribution towards the participation of a team from Stavanger in the Denis Law Tournament, 15 th -21 st July 2012.	£6,950
vi)	Contribution towards sending a loan exhibition from Gordon Highlanders to the Regional Gomel Military Museum, 7 th - 16 th October 2012.	£1,000
	TOTAL	£22,230
	Amount remaining in budget if recommendations approved:	£79,770

4. OTHER IMPLICATIONS

The health and safety implications of all visits are taken into account during planning, with any exceptional implications addressed at the time of application.

The International Partnerships Officer regularly reviews the travel advice provided by the Foreign and Commonwealth Office and is in close contact with counterpart officers in the twin cities to ensure information provided to participants is accurate and up-to-date.

Where individuals/groups from Aberdeen are travelling overseas, comprehensive pre-visit briefings are offered by the International Partnerships Officer and all have the opportunity to ask questions and request support.

When participating in overseas visits, Council employees are covered by the Council's insurance policy while non-Council employees are instructed by the International Partnerships Officer to obtain comprehensive travel insurance.

Long established twinning links will not be maintained without regular reciprocal visits and ongoing projects being initiated by communities, groups and individuals and there is the potential for the city's international image to be damaged if we don't continue to support established relationships.

Without support from the twinning budget, exchanges and visits of this nature could not take place and people in Aberdeen would lose a wide range of opportunities to engage with the international community. In addition, incoming visitors to the city on twin city projects support the local economy through their participation at events and hospitality and retail expenditure while visiting the city.

The International Partnerships Officer will provide a strong link between Aberdeen and our twin cities who also provide organisational support, advice and guidance when required.

They will also make recommendations on funding available to the group and offer support to foster a relationship with the city so that it is long lasting and beneficial to the people of Aberdeen without requiring on-going financial support from the public purse.

All groups participating in twin city activities are encouraged to develop sustainable relationships with their partner groups. Links between most of the twin cities of Aberdeen are active, and links which have not recently been engaged have been contacted to re-establish communications. It is important that all visits and projects through twinning are long-standing and fruitful to ensure their benefit to the local people of Aberdeen, to establish and promote positively the overseas image of Aberdeen and for best practice to be shared between cities.

5. BACKGROUND/MAIN ISSUES

i) **Contribution towards the participation of two Strong-Men from Regensburg, Clermont-Ferrand and Stavanger in the Aberdeen Highland Games, 15th-18th June 2012.**

Having celebrated its 50th anniversary in 2012, Aberdeen Highland Games is an integral part of the Grampian Games Circuit and also an important event within the Aberdeen City Council's Summer Festival programme. The organisers are looking to enhance and add value to the event which this year is being held on Sunday 17th June through the participation of Aberdeen's twin cities.

Aberdeen Highland Games is a very popular event within the Summer Festival programme and depending on the weather attracts over 10,000 visitors on a regular basis. The event is widely publicised through the local and national media, radio and Aberdeen City Council website.

Two senior strong men from Regensburg, Clermont-Ferrand and Stavanger have been invited to compete in the 'Aberdeen Highland Games Tri-Twinning Challenge' during the day of the event. They will compete against each other in traditional Highland Games activities such as tug o' war, throwing the light hammer, shot putt and tossing the caber. At the end of their tournament, a prize will be awarded for individual winner and the winning team.

The strong men will arrive on Friday 15th June 2012 and will spend Saturday 16th June with the International Partnerships Officer who will take them to Oldmeldrum Highland Games. Here they will have the opportunity to view the activities, learn tips and meet with Officials in preparation for the Aberdeen Highland Games on Sunday 17th June. The participants will leave Aberdeen on Monday 18th June 2012.

Local associations and businesses, which are representative of the competing countries, will be invited to the Aberdeen Highland Games to give encouragement to the twin city athletes. The winning team will be given a trophy engraved with their country and winning year. It will then be kept by the twin city council and used as a tool to raise the profile of Aberdeen as an attractive tourist destination and to publicise traditional Scottish events and sports. This event will then be established as a regular competition at the Highland Games in future years.

The participation of the twin cities will enhance and add value to the programme of sporting events offered at the Aberdeen Highland Games. It will also build on the growing interest from Europe in Scottish traditional events, and attract new visitors and fans to the city and surrounding areas

in future years. The local crowd will benefit from the additional entertainment and awareness of the twin cities of Aberdeen will be raised. The attendance of local associations and businesses, representing the twin city, will increase and stimulate their involvement and participation in twin city projects.

To determine the success of the 'Tri-Twinning Challenge' the organisers of the Aberdeen Highland Games will be asked for feedback on the success of the twin cities participation and how it impacted on the crowd and on the event. Feedback will also be asked for from the participants to determine the impact and success of the 'Aberdeen Highland Games Tri-Twinning Challenge' in the annual Aberdeen Highland Games event.

The contribution asks for a maximum £2,500 towards the participation of the six strong men to cover the costs of accommodation, meals and transport. The twin cities would cover the cost of air flights for the participants to and from Aberdeen.

ii) Contribution towards the 21st Aberdeen (Cults) Scouts Visit to Gomel, 10th to 17th October 2012.

Following the success of the 21st Aberdeen (Cults) Scouts Visit to Gomel in 2011, a group of different individuals from the 21st Scout Group in Aberdeen have been invited to visit Gomel to take part in English Teaching projects which has been developed jointly with the teachers in Gomel. The visit will take place from the 10th to 17th October 2012.

The scouts will participate in English Teaching projects with schools which are selected by Gomel City Council. They will also deliver teaching materials during the visit. The schools involved in the project are as follows:

- Gomel State Children's Centre
- Gomel Skarina University
- Gomel Association of Children and Youth
- Linguistic Schools Numbers 10 and 71

Other activities which the Scouts will be involved in during their visit include:

- Visit Technical School 67
- Visit Gomel City Social Centre
- Visit Gomel State Technical School of Arts and participate in a workshop.
- Visit State Centre of Radioactive Medicine

The Scout Visit in October 2012 will develop and promote the existing Scout twin city partnership with Gomel. After the Scout Visit to Gomel in 2011 the participants received excellent positive press coverage where it was stated that the participants 'are a credit to their generation'.

Visits to Gomel are extremely developmental to the young people as they learn the different Belarusian cultural and social ways of life. As the visit involves teaching conversational English and working with people of a range of different ages, the visit will develop their maturity, communication and international relation skills. The skills which the participants will learn and develop during the trip will increase their confidence, develop their lifelong skills and they will better appreciate their place as a global citizen. The participants must also learn to adapt their teaching approach in each school accordingly to the level of the students and how best to engage the Gomel pupils. Adaptability is an essential skill for later life.

Based on previous participants, the young people from both cities forged long-standing friendships and communicate on a weekly basis through email, phone and social network sites. The Aberdeen and Gomel young people participating in 2012 have the opportunity to continue developing their communication and language skills.

As part of the project, the Scouts are required to give a feedback report on the Gomel visit covering information on which projects they undertook, any challenges that they faced and what they learnt from the experience. The report back is essential as it allows them to qualify for part of the Duke of Edinburgh award and various other Scout awards. The feedback report is also used to demonstrate how the donations and public money were used.

Before the Scouts leave for Gomel, they must attend meetings throughout the year to discuss how they will fund-raise the visit, and undertake a series of events indicating their commitment to the project. The participating Scouts also undertake meetings to practice their presenting skills and public speaking, which is done in consultation with the English Teaching staff in Gomel Schools.

The application requests a total of £2,600 to contribute towards the cost of insurance, transportation, visas, interpreters and meals. The total cost of the trip is £7,820 and a total of £5,220 will be raised by the Scouts through sponsored events, jumble sales and a Scout Community Event.

iii) Contribution towards the visit of a group of young people from Regensburg to participate in a production piece produced by Aberdeen Performing Arts, 31st July to 12th August 2012.

Aberdeen Performing Arts (APA) is a charitable company which begun trading in 2004. Audience attendance and income for APA has reached record levels, and their public profile has grown significantly. APA aims to 'touch emotions, lift the spirits, and expand the minds' of both residents and visitors to the North East of Scotland by producing a wide and diverse range of projects.

In summer 2011 APA took part in a highly successful youth exchange project with the Regensburg based dance company, Danceworxxx. This resulted in performances of street theatre created by the young people in both Regensburg and Aberdeen. The projects during 2011 proved to be hugely beneficial and APA aims to build on the success from 2011 and explore new challenges in 2012.

APA is collaborating with Danceworxxx again in 2012 to produce a piece of contemporary physical theatre to be showcased at the Lemon Tree during August 2012. There are eight young people and three adults from Regensburg who will be taking part.

From the 1st -10th August, the Aberdeen and Regensburg groups will devise the piece together after opening a box of stimulus from Visible Fictions Theatre Company, renowned producers of theatre for young people. No preparation work will be able to be done prior to the start of the project therefore the groups must work together intensively to create the performance piece.

The groups will participate in Aberdeen International Youth Festival (AIYF) activities across the city by performing as part of 'Festival in the City' and will offer workshops to AIYF participants. This will give maximum exposure to the APA and Regensburg group performers and provide best value for money.

The participants will substantially improve their social skills and confidence levels. For the participants from Aberdeen, they will feel a sense of pride and ownership of their city and will thrive on the responsibility of hosting a group of young people from another city. For the participants from Regensburg, they will improve their English language skills and find new confidence in their abilities. Both groups will broaden their global horizons and they will be challenged by other young people's perspectives. They must learn how to overcome language barriers within a short space of time to bring about a successful production piece.

The participants will also have the opportunity to discover new ways of working and the proposed visit offers an intense professional development opportunity.

APA has been formally invited to a reciprocal visit to Regensburg by Danceworxxx in 2013. By establishing themselves with partners in the twin cities, APA will be a name that is trusted for providing high quality arts activities for young people and for fostering long lasting and meaningful relationships.

The application requests a maximum total of £7,000 to contribute towards the cost of accommodation and subsistence. Regensburg Council will cover the costs of flights and transport for the young people from Regensburg. The Regensburg participants will provide workshops to the local people of Aberdeen in order to achieve best value from the experience. The young people of Aberdeen will undertake fund-raising activities and sponsored events. A break-down of costs has been given below.

Expenditure	
Flights and Travel	£3,330
Accommodation	£6,050
Staffing	£4,720
Subsistence	£2,350
Production	£750
Rehearsal Space	£800
Venue and Technical Costs	£900
Total	£18,900
Income	
Estimated Fund-Raising	£500
Contribution by participants	£3,330
Other sources of income	£8,070
Total	£11,900
Expenditure-Income	18,900-11,900
Total Applying For:	£7,000

iv) **Contribution towards the participation of an artist and assistant from Clermont-Ferrand to participate in the Aberdeen Art Fair, 17th-19th August 2012.**

The Aberdeen Art Fair, held at the Aberdeen Music Hall, has become an established event in Scotland's Art Calendar attracting artists and galleries from across the UK and Europe. In 2011, the Aberdeen Art Fair hosted exhibitors as far afield as Monaco, London and the twin cities Stavanger

and Regensburg. These two twin cities were chosen due to their rich cultural history, their strong artistic reputation and the well established links with Aberdeen.

An artist and their assistant from Clermont-Ferrand have been invited to this year's Aberdeen Art Fair 2012 given their high quality of exhibition work and their well-established reputation in the twin city. The organisers of the Aberdeen Art Fair also want to highlight the artistic talent from Aberdeen's twin cities and open opportunities for networking within Europe.

The artist and their assistant from Clermont-Ferrand will be in Aberdeen from the 17th to the 19th August 2012. The programme of events for the visitors from Clermont Ferrand include the following activities;

- VIP Preview Evening on the 17th August 2012 which marks the launch of the Fair and comprises an audience of local dignitaries, councillors, sponsors, artists, gallerists, the media and key business people.
- 18th and 19th August are Public Open Days. Last year there were over 2,000 visitors to the Fair, and it is anticipated that in 2012 the event will attract increased number of visitors.
- Exhibitors to the Aberdeen Art Fair will be invited to a 'Meet and Greet' event held at the Music Hall on the Saturday evening. This event is sponsored by 'Artmag' magazine and will provide an opportunity for all the exhibitors to network with gallerists and artists from across the UK and Europe.

The organisers are in discussion with Robert Gordon's University so that the twin city representatives are able to present their work to the Gray School of Art students and provide an opportunity for networking and sharing of knowledge and best practice.

The organisers of the Aberdeen Art Fair want to develop a long-term partnerships with the twin cities due to the quality of art which their artists would bring to Aberdeen. There are also cultural and business benefits that are brought about by networking between the artists from the twin cities, and the businesses and public attending the event.

The application requests a maximum of £2,180 to cover the costs for an exhibition stand at the Aberdeen Art Fair and for the cost of accommodation and food for the participants. It is still to be confirmed if an assistant will be accompanying the artist which is why up to a maximum of £2,180 has been requested. Clermont-Ferrand will pay for the transport of the participating artist and assistant. In 2013 Clermont-Ferrand will play

host to Aberdeen representatives who are participating in their annual 'Court du Métrage Festival', an international short film festival.

v) Contribution towards the participation of a team from Stavanger in the Denis Law Tournament, 15th-21st July 2012.

The Denis Law Tournament 2012 is open to Under 14 and Under 16 Boys, and has already secured teams from the US and England as well as top Scottish Premier League teams in Scotland. In 2011, the Denis Law Tournament was only open to Under 16 teams; therefore the organisers are developing the Denis Law Tournament 2012 into a bigger football event enveloping a greater number of players, audience numbers and talent.

The tournament will engage the local community and businesses with a week long festival of football culminating in the elite tournament finals. With football at the heart of the tournament, Aberdeen Sports Village (ASV) aims to generate a 'festival feel' by providing community events such as 4 v 4's for primary school children, corporate 7's, street football opportunities for spectators as well as the opportunity to watch teams from across the globe compete to win the title and Denis Law Tournament Trophy. The addition of more community based events will encourage local people of all ages and social backgrounds to participate and feel part of the event.

The aim of the Festival is to turn a traditional football tournament into an innovative, inclusive event which captures the imagination of the people of Aberdeen and attracts new visitors to the City. The event will be publicised in local and national media and aims to unite local, national and international young people.

Aberdeen Sports Village wants to maintain the international element of the football competition in Aberdeen as the event aims to showcase the unique attractions of Aberdeen and Scotland by encouraging teams from across the globe to take part in the tournament. It is therefore fitting that a twin city is represented at the inaugural football tournament.

The organisers want to ensure that the young people are connected, have international exposure and to provide them with the opportunity to compete, make friends and have fun. During the event it will promote practicing sport, showcase the unique sporting facilities at ASV and encourage healthy lifestyles. The football coaches will have the opportunity to attend workshops to share knowledge and best practice, and learn new ways to improve the coaching and training of the young people.

Stavanger has close connections with Aberdeen and in 2011 Stavanger invited Aberdeen football teams to take part in their annual football tournament. Aberdeen Sports Village wants to continue and develop this positive relationship by reciprocating the gesture. This is why Stavanger was chosen to participate in the Denis Law Tournament 2012. The Stavanger team, Vardeneset BK, was specifically chosen based the fact that they are a high performing team, therefore are capable of competing with the other participating teams and are part of the league table in Stavanger. The Stavanger team will share a sporting and cultural experience with the other international and national young players. The local people who attend the Tournament and the local young players will also get the opportunity to learn and forge an understanding of other nationalities and cultures.

Aberdeen Sports Village has undertaken a number of fund-raising initiatives and plans to hold a Fund Raising Dinner and Auction in order to help fund the overall cost of hosting the event. They are also actively seeking private sponsorship and will continue to do so in the run-up to the tournament. These fund-raising efforts will assist with the costs of running the whole event. The team from Stavanger will be undertaking fund-raising efforts also to support the cost of their visit to Aberdeen.

An application was previously submitted by the Denis Law Tournament organisers for the participation of two teams from Clermont-Ferrand and was considered at the Enterprise, Planning and Infrastructure Committee the 31st January 2012, however the teams from Clermont-Ferrand are no longer able to participate in the Tournament in 2012. No monies from the International Twinning Budget therefore have been spent as yet, on the Denis Law Tournament 2012.

The application requests a total of £6,950 to contribute towards the cost of transport whilst in Aberdeen, accommodation (full board in Halls of Residence), Tournament Registration Fee and transfer to and from Aberdeen Airport. The Stavanger group and Stavanger City Council will be covering the costs for the flights and travel insurance. Fund-raising efforts by Aberdeen Sports Village will also be contributed towards the Stavanger team visit.

Expenditure	
Flights	£7,400
Transport	£600
Accommodation	£6,000
Tournament Registration Fee	£350
Travel Insurance	£500
Total	£14,850

Income	
Estimated Fund-Raising	£1,025
Contribution by Stavanger Council	£2,180
Contribution by participants	£4,695
Total	£7,900
Expenditure-Income	14,850-7,900
Total Applying For:	£6,950

vi) **Contribution towards sending a loan exhibition from Gordon Highlanders Museum to the Regional Gomel Military Museum, 7th - 16th October 2012.**

The Gordon Highlanders Museum in Aberdeen aims to preserve and share the legacy of the world-famous Gordon Highlanders for future generations. A twinning link was established between the Gordon Highlanders Museum and the Gomel Regional Museum of Military Glory in 2008 when low-levels of co-operation began. In February 2010, a major exhibition, '*Gomel: A Belarusian Town At War*' opened at the Gordon Highlanders Museum, the contents of which had been loaned from the Gomel Regional Museum of Military Glory. This exhibition held artefacts from the Second World War which had never been viewed before outside Belarus. It was an once-in-a-lifetime opportunity to view the subject and a chance for the local people of Aberdeen to find out more about their twin city.

The Gordon Highlanders Museum is sending objects to the Gomel Regional Museum of Military Glory for a major new exhibition in Gomel titled, '*Highlanders in Afghanistan, Then & Now*' which will detail The Gordon Highlanders in Afghanistan in the 19th Century and current operations in Afghanistan from the perspective of The Highlanders (4SCOTS). An exhibition of this kind has not been done before in Belarus. The exhibition in Gomel will open the 9th October 2012 and the Curator of the Gordon Highlanders Museum has been formally invited to open the exhibition by the Director of the Gomel Regional Museum of Military Glory.

The loan exhibition from 2010, '*Gomel: A Belarusian Town at War*' has been entered into a Belarusian museums competition, '*Intermuseum*' by the Gomel Regional Museum of Military Glory. The curator of The Gordon Highlanders Museum has been invited to attend the competition as representative of The Gordon Highlanders Museum. If the Gomel Regional Military Museum is successful, it will mark a major milestone in the cultural relationship between Aberdeen and Gomel.

One of the anticipated outcomes and long-term benefits of this twinning activity will be promoting the military history of The Gordon Highlanders which is Aberdeen's legendary regiment. Their history will be prominently displayed in Gomel, allowing the population of Gomel and visitors to the city to appreciate the legacy of the Regiment associated with their twin city of Aberdeen. The exhibition will also give visitors to the Gomel Regional Museum of Military Glory an unprecedented view of current operations in Afghanistan from the Scottish perspective of 4SCOTS, a view which otherwise they would have no chance of seeing.

The twinning project would strengthen the relationship and cultural ties between The Gordon Highlanders Museum and the Gomel Regional Museum of Military Glory. Future collaborations are being explored currently for 2013 where The Gordon Highlanders Museum will host a mini-exhibition about the experiences of veterans living in Gomel who served in the Soviet Armed Forces in Afghanistan in the 1980s.

The exhibition would also promote and encourage increased partnership and understanding between veteran's organisations in Aberdeen and Gomel which will promote the social links between the two twin cities.

Jesper Ericsson, Curator of The Gordon Highlanders Museum, has provided a more detailed outline of the twinning projects with the Gomel Military Museum. Please see Annex 1.

The application requests a total of £1,000 towards the loan exhibition and sending the curator of The Gordon Highlanders Museum to Gomel. The total cost of the trip is £1,895.70. The Gordon Highlanders Museum is financing the transportation of items to Gomel. The Gomel Regional Military Museum will be covering the costs for the planning, preparation and mounting of the exhibition, all travel costs within Belarus, accommodation during the '*Intermuseum*' competition period and shipping the loan items back to Aberdeen. A higher amount of funding has been requested by the Gordon Highlanders Museum given that they are an independent, self-financed Museum governed by a charitable Trust and Gomel City is not able to contribute as much financially.

6. IMPACT

The use of the International Twinning Budget to support international activity links closely to several of the Community Plan Challenges for example Learning; Arts, Heritage and Sport, Aberdeen's Image; Getting Involved; Being Informed and Leading the City. Many of the recommendations for projects in this report create a positive image of Aberdeen internationally and provide local communities an opportunity to

learn from others, to participate in various, exciting events and to increase local pride in our city.

In the policy statement “Vibrant, Dynamic and Forward Looking”, the Council sets out how it aims to make Aberdeen a better living place to live and work. Promoting and supporting international exchanges, involving various communities of interest contributes to this aim and to the actions of the Single Outcome Agreement. The outlined twinning projects also align with the cultural strategy vision as it allows for improved communication of cultural opportunities, helps with changing the perception of the city, to develop a stronger cultural identity and to have increased levels of effective partnership working.

Aberdeen’s twin city partnership initiatives:

- Foster international understanding and friendship and an appreciation of cultural diversity
- Provide Aberdeen’s citizens with an awareness of, and insight into, international issues and perspectives and their impact on Aberdeen and Scotland
- Promote the image and raise the profile of the city overseas; and
- Provide a forum for the exchange of knowledge, expertise, skills, ideas and best practice in any given field.

Opportunities for groups, communities and organisations to access twin city projects and exchanges and for possible sources of funding are advertised through the Council’s website, press releases, school circulars where appropriate and the Aberdeen Council of Voluntary Organisations e-bulletin.

Human Rights, Equalities and Diversity: Aberdeen City Council has in place a range of statutory and discretionary plans, schemes and policies to promote equality. Officers endeavour to target groups and communities which have not previously had experience of international visits or exchanges, or have been under-represented in twin city activities. Applicants complete an equal opportunities monitoring form as part of the application process.

7. BACKGROUND PAPERS

Annex 1 attached.

8. REPORT AUTHOR DETAILS

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ACCOMPANYING REPORT FOR GRANT APPLICATION

JESPER ERICSSON
CURATOR
THE GORDON HIGHLANDERS MUSEUM

MARCH 2012

In 2008, low-level co-operations between The Gordon Highlanders Museum and the newly established Gomel Regional Museum of Military Glory were initiated by Pavel Zhdanovich, Director of the Gomel Museum, with the assistance of Elaine Robertson, Aberdeen City Council, and Sarah Malone, then Executive Director of The Gordon Highlanders Museum. A small selection of loan items was sent to add to a Second World War-themed exhibition at the Gomel Regional Museum of Military Glory. The loan items were returned after three months.

In December 2008, Jesper Ericsson became Curator of The Gordon Highlanders Museum. One of his first jobs was to receive these returning loan items. In 2009, he approached Aberdeen City Council with the idea of mounting a major exhibition at The Gordon Highlanders Museum about what happened to Gomel during the Second World War, a subject that had never been explored or displayed in Aberdeen before. Working together with Elaine Robertson and Nicolas Ellison, the idea was explored and in June 2010, Jesper Ericsson and Nicolas Ellison travelled to Gomel to formalise the relationship between The Gordon Highlanders Museum and the Gomel Regional Museum of Military Glory, and to plan for the upcoming exhibition in Aberdeen.

In February 2011, '*Gomel: A Belarusian Town at War*' opened at The Gordon Highlanders Museum. Pavel Zhdanovich and a small party of veterans travelled to Aberdeen to formally open the exhibition. '*Gomel: A Belarusian Town at War*' was a ground-breaking exhibition, which allowed everyone, from every level of society and backgrounds in Aberdeen, to find out about their twin city. For the first time, the plight of Gomel was told in the West. Also, objects were loaned from Gomel which had never, ever been seen outside Belarus before.

The exhibition ran from February to June 2011, which also took in the momentous 70th anniversary of the German invasion of the Soviet Union in June 1941. Additionally, '*Gomel: A Belarusian Town at War*' was the first exhibition ever in the UK to explore a twin town's wartime story in such depth and detail. No such exhibition about the Eastern Front in the Second World War has ever been mounted in another city in the UK, not even at the Imperial War Museum or National Army Museum in London.

During the run of the exhibition, information highlighting the twinning and leaflets promoting the Friends of Gomel were prominently displayed. At the end of the exhibition, framed posters, which had been on display, were raffled off, with the proceeds going to the Friends of Gomel.

Also, in June 2011, Jesper Ericsson gave a talk on '*Gomel: A Belarusian Town at War*' at the Scotland-Russia Institute in Edinburgh, further highlighting the twinning.

After the exhibition opening, discussions were held about trying to form a link between veterans' organisations in Aberdeen and Gomel. With information provided by The Gordon Highlanders Museum, veterans in Gomel have since contacted and formed a relationship with the charity Erskine.

The Gordon Highlanders Museum also provided contact information to a school in Gomel for them to potentially establish links with schools in Aberdeen. On another educational front, The Gordon Highlanders Museum worked with the SLOVO Saturday School in Aberdeen, designing and facilitating a special workshop for the pupils which incorporated *Gomel: A Belarusian Town at War*.

Additionally, an agreement on future co-operations between The Gordon Highlanders Museum and the Gomel Regional Museum of Military Glory was signed. The first result of this is directly relevant to this grant application.

This summer, The Gordon Highlanders Museum is sending objects to the Gomel Regional Museum of Military Glory for a major new exhibition in Gomel, titled '*Highlanders in Afghanistan, Then & Now*'.

This exhibition will detail The Gordon Highlanders in Afghanistan in the 19th Century and current operations in Afghanistan from the perspective of The Highlanders (4SCOTS). An exhibition of this nature has never been seen in Belarus before. For the first time, the people of Gomel will discover the full Regimental story of The Gordon Highlanders, with particular detail on their involvement in the Second Afghan War (1878 – 1880) and operations on the North West Frontier in the 1890's. Also for the first time, the people of Gomel will discover the story of The Highlanders (4SCOTS) in Afghanistan in recent years. What will be particularly poignant is that Gomel is home to many veterans of the Soviet involvement in Afghanistan in the 1980's.

'Highlanders in Afghanistan, Then & Now' is due to open in Gomel on 9 October 2012 and Jesper Ericsson has been invited by Pavel Zhdanovich to formally open the exhibition and meet with veterans.

Additionally, '*Gomel: A Belarusian Town at War*', the exhibition displayed at The Gordon Highlanders Museum in 2011, has been entered into a prominent Belarusian museums competition, '*Intermuseum*' by the Gomel Regional

Museum of Military Glory. As part of this prestigious Museums competition, Jesper Ericsson, as a representative of The Gordon Highlanders Museum and Aberdeen, has been invited to attend to support the Gomel bid.

The Gordon Highlanders Museum is an independent charity, receiving no funding from either the Scottish Executive or the Ministry of Defence, and is entirely self-funded through Museum admissions, our Tea Room and hospitality events amongst other incomes. As such, we would be entirely dependant on a successful application of grant funding to be able to fulfil this kind invitation from the Gomel Regional Museum of Military Glory to travel to Belarus.

If the grant application is successful, both before he travels to Gomel and after he returns, Jesper Ericsson intends to highlight the twinning and co-operations between The Gordon Highlanders Museum and the Gomel Regional Museum of Military Glory through the local and national press. Also upon his return, he will undertake a series of talks to community groups to further draw attention to the twinning and raise public awareness of it, thereby gaining new audiences for both twinning and Museum projects.

'Highlanders in Afghanistan, Then & Now' will also help to promote and encourage increased partnership and understanding between veteran's organisations in Aberdeen and Gomel, something which is already underway through the charity Erskine. This will in turn promote social links between the two cities.

The next planned co-operation between The Gordon Highlanders Museum and the Gomel Regional Museum of Military Glory will take place in 2013. Next year, The Gordon Highlanders Museum will host a mini-exhibition of loan items from the Gomel Regional Museum of Military Glory on the Soviet experience of Afghanistan in the 1980's, with particular reference to veterans currently living in Gomel and their stories. This will be an opportunity to tell their story for the first time in the West, and an incredible chance for the people of Aberdeen to find out about what veterans from their twin city went through in comparison with current UK operations in the same country. Hopefully Afghan veterans will be able to travel from Gomel to Aberdeen for the exhibition launch and meet with veterans groups in Aberdeen, as well as meet current serving or recently retired members of the Armed Forces who have served in Afghanistan.

After 2013, there is boundless opportunity for future co-operations, which will only strengthen the twinning between Aberdeen and Gomel, raise awareness of the twinning across social and ethnic groups living here and gain new audiences for both twinning and the Museum. For example, major military anniversaries are fast approaching, for example the centenary of the First World War in 2014 and the 70th anniversary of VE / VJ days in 2015. These will be discussed in due course on a museums level by Jesper Ericsson and Pavel Zhdanovich.

ABERDEEN CITY COUNCIL

COMMITTEE Enterprise, Planning & Infrastructure

DATE 31 May 2012

DIRECTOR Gordon McIntosh

TITLE OF REPORT Sustainable Urban Fringes (SURF) Project Update

REPORT NUMBER: EPI/12/091

1. PURPOSE OF REPORT

The purpose of this report is to provide Elected Members with a progress update on the SURF (Sustainable Urban Fringes) project that Aberdeen City Council (ACC) is Lead Beneficiary for, to seek member representation at the SURF 2012 final conference in Aberdeen and for members to support, in principal the SURF position paper.

2. RECOMMENDATION(S)

It is recommended that the Committee:

1. Notes the positive progress to date of the SURF project, with specific acknowledgement of the intention of the partnership to extend the project by a further 6 months
2. Approves the attendance of the Convener and Vice Convener of the Enterprise Planning and Infrastructure Committee to represent the Council at the SURF Output Dissemination Conference in Aberdeen, on Thursday 28th June 2012.
3. Supports in principal the SURF position paper to be presented to the EU Commission as an output of the Sustainable Urban Fringes Project.

3. FINANCIAL IMPLICATIONS

The SURF Project is a three year European project that is part funded through the North Sea Region Interreg IVB programme European Regional Development Fund (ERDF). ACC is the Lead Beneficiary for the SURF Project, and leads two of the four workpackages; being Project Management and Communications. In addition, ACC is a project partner within SURF and is running a project to improve the River Don urban fringe corridor.

As Lead Beneficiary, ACC is committed to hosting the SURF final conference, with associated costs being covered by existing budgets from within the project. There will be no cost for Elected Member attending the conference.

SURF Lead Beneficiary Costs – Breakdown

As Lead Beneficiary ACC charges a management fee, this is charged at 10% of the total project budget over three years.

The SURF Project's Total budget - 4,376,396 Euro (£3,583,232) - which is dispersed to partners.

ACC receives from this a Total Management Fee - 437,639 Euro (£358,317).

SURF Aberdeen Project – Budget

The SURF Aberdeen project consists of a 50% contribution to the total budget from ACC with the additional 50% from ERDF funding.

- Total Budget – 419,336 Euro (£343,000)

SURF Extension

The SURF partnership has requested a project extension of an additional 6 months. ACC have received verbal approval of this from the secretariat and should receive official endorsement in the coming weeks. This extension will be delivered within the existing project budget and management fee.

4. OTHER IMPLICATIONS

ACC has and will continue to benefit from the SURF project through improved spatial planning guidelines and the exchange of transnational learning with the SURF project partners.

The project has had a positive impact for the city particularly the development of the Draft 'River Don Corridor Framework' which will be adopted as Supplementary Planning Guidance within the Aberdeen Local Development Plan. This will lead to the improvement of the quality of life for many Aberdeen residents along the River Don through a more accessible green environment.

Through the project, Aberdeen City has enhanced its reputation within Europe and opens the opportunity to attract new funding streams to deliver environmental and low carbon projects for the city.

This project supports the Council's commitment to sustainable development with an emphasis on improved land use opportunities in the developing sustainable urban fringes and delivering more efficient Council services for the residents of Aberdeen.

5. BACKGROUND/MAIN ISSUES

5.1 SURF Project Background

Aberdeen City Council is leading 13 partners from 5 European North Sea Region countries (Belgium, Germany, Sweden, The Netherlands, and The UK) to analyse management approaches for the urban fringe that strengthen the link between the opportunities provided by these areas (e.g. green infrastructure, connectivity between urban and green spaces and access to health & lifestyle activities) and city region competitiveness.

The project has analysed and tested innovative applications of new policy and governance tools and has identified good practice for dissemination to other regions. In addition, a toolkit for effective urban fringe management is in development.

The project has also put sustainable urban fringe development higher on the political agenda in all partner regions, the national and regional governments and at EU policy level. Activities are being derived to assist partners with local and national lobbying for new urban fringe policy and ACC is leading on the activities required to generate interest at the European level and follow up with a position statement requesting new flexible urban fringe policy for Europe.

The position paper is attached for member support in principal and the members will be updated as this paper progresses throughout relevant European organisations in consultation (therefore please note that this paper will be subject to minor amendments throughout this process). The proposed result for this paper is that the European Commission adopts it in principle and forwards it to the European Parliament for new policy creation.

5.2 SURF Project – Management

The Council is leading Work Package (WP) 1 Project Management and WP2 Communications & Publicity. An officer from the Partnerships & Funding team manages the financial element of the project. WP1 also has responsibility for managing the other external work package leaders. Significant progress has been made throughout the project and SURF is now at the stage of collating the analytical data and translating it into the project outcomes which are:

- A toolkit for effective urban fringe management
- A final report highlighting the project learning including:
 - Policy recommendations
 - Effective approaches to the development and management of urban fringe areas
 - Governance insights and scenarios
- European policy paper to lobby for new urban fringe policy to be created at EU level

5.3 SURF Aberdeen Project – River Don Corridor

As well as having the lead beneficiary role for the overall SURF project, ACC is running a local 'demonstration' project; SURF- Aberdeen. The project aims to develop and promote new methods for assessing and managing open

spaces along the River Don corridor, for the benefit of the local economy, environment and communities.

The project focused on the 'River Don Corridor' – the urban fringe area made up of the River Don and surrounding open spaces. The area is under pressure from development and likely to undergo significant change in the years ahead through developments identified in Aberdeen Local Development Plan.

The *project's objectives* are to:

1. Develop an accessible Geographical Information Systems (GIS) tool to assist with spatial planning of open space;
2. Conduct a participative programme of community engagement activities, using GIS, to improve local decision making;
3. Develop a spatial plan, in collaboration with the community, to guide development & direct improvements to open space;
4. Access funding and work with communities to implement several improvements to open spaces in line with the spatial plan;
5. Work collaboratively with transnational partners to meet the aim and objectives of the parent SURF project.

This project has brought together residents, communities and organisations together and has led to the development of a more collective approach to the planning & management of open spaces along the River Don corridor.

SURF Aberdeen - Achievements

The SURF project has assisted the Council to design and undertake the City's Open Space Audit. The Audit assessed the different types of open spaces across Aberdeen, including the urban fringes in the SURF Aberdeen project area. This information was used to inform the City's Open Space Strategy and the audit information is available on the Council's corporate GIS. This has helped to establish a valuable baseline to inform open space policy, planning and management locally.

The project has taken several approaches to engaging with stakeholders in the River Don area. These have included;

- A Strategic Steering Group - A focused group of ten key stakeholders that help direct the project and monitor progress;
- River Don Communities Network - A network of invited representatives from stakeholder groups for two-way communication on decisions / plans;
- Outreach Sessions - Meetings with a range of stakeholder groups, by invite, to outline the project and obtain their input;
- New Media (River Don facebook page) – An forum open to the general public to participate in the 'conversation'.

The project has received overwhelmingly positive feedback about our engagement at all levels and has clearly made stakeholders feel they can have an influence on the decisions being made within the River Don area.

The project has delivered the draft 'River Don Corridor Framework', following a process of local community engagement and full statutory public consultation. This is the first Framework of its type in Aberdeen; setting out the required 'soft' infrastructure associated with City development in this area.

The aim is to adopt the Framework as formal Planning Guidance for Aberdeen, making it a material consideration for developers to include in their plans when carrying out any development in the project area – many of which are due in the coming years.

An Implementation Plan is being developed to complement the Framework. The plan has identified a range of short and medium-term items that could be delivered, or scheduled, over the time-scale of this project. Short-term items are being delivered by a mix of quick responses from Council services, or through volunteer community action events in the area. These have all had a very good response.

Medium-term and long-term projects are being prepared and will be delivered through collaboration with partner organisations, beyond the time-scale of this project and through major developments in the area

The SURF Aberdeen project has benefited from good collaboration between public sector agencies and positive public engagement activities, including input from the private sector. All these factors combined have allowed us to produce good quality outputs which, in part, help to set the framework that, in time, should deliver positive change for the River Don Corridor's environment.

SURF Conference

The Final Conference for SURF will take place on 27-29 June 2012 in Aberdeen.

Elected Members are invited to attend the main conference day of the 28th June where the project outputs will be showcased and where the conference guests will see first hand the work completed on the River Don project area.

The conference will be for three days and will involve the following:

27th June - Day 1 is the day for project partners where the partnership will present on the progress made within their projects and the plans they have during the SURF extension period. The partnership will also hear about the future programmes for Interreg and be able to start a process of new project application development.

28th June - Day 2 is the main conference day with invited guests and speakers. SURF is in the process of inviting a senior Scottish politician as key note speaker to present on the Scottish Cities strategy. Already confirmed as guest speakers include:

- Reinhard Henke from the PURPLE organisation, which is a European based group lobbying the European Commission for policy development and policy change to take into account the green environment in urban and rural planning.
- Alister Scott, who is a member of a national wide project RELU. The Rural Economy and Land Use Programme (RELU) aims to advance understanding of the challenges rural areas face, using interdisciplinary research to inform future policy and practice.
- Sabrina Lucatelli, from the European Commission, Sabrina works for DG REGIO and is a Directorate for Policy conception and coordination.

29th June - Day 3 will provide the opportunity for the partnership to compare and contrast the River Don corridor with the River Dee corridor with an optional morning excursion and allow Aberdeen to showcase the beautiful countryside on the city doorstep.

The SURF outputs will also be showcased with a facilitated discussion group from the SURF workpackage leaders to provide an open exchange on how the findings from the project were achieved and what the next steps are for improving recognition and management of the urban fringe in Europe.

6. IMPACT

The SURF project facilitates Aberdeen's City's Vision, to be "a city which is vibrant, dynamic, forward looking – an even better place to live and work, where people can expect high-quality services that meet their needs".

This project supports the City's Community Plan and the priority Outcomes within the City's Single Outcome agreement. In particular Outcome 12 "we value and enjoy our built and natural environment and enhance it for future generations".

7. BACKGROUND PAPERS

Economic Development Sub-Committee Minutes, 23rd February 2009,
Approval of Aberdeen City Council to lead the SURF project

<http://committees.aberdeencity.gov.uk/CeListDocuments.aspx?Committeed=323&MeetingId=702&DF=23/02/2009&Ver=2>

River Don Corridor Framework – Report reference EPI/12/110 – also submitted to this committee

9. REPORT AUTHOR DETAILS

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Sustainable
URBAN FRINGES
S U R F

“Competitive advantage for city regions – the need to create new urban fringe policy”

Summary

A key to creating competitive advantage for European Cities and Regions is to recognise the opportunities and value that the urban fringe area presents through making smarter use of space to enhance sustainable urban development.

With a focus on either urban or rural policy, current European policy does not consider the importance that the urban fringe connection has to the success of cities and regions in Europe.

The Sustainable Urban Fringes (SURF) project recognises that there is an opportunity for the European Union to bridge the gap between urban and rural policy and create new policy which can specifically benefit the development urban fringe areas. Complimenting existing urban and rural policy, this would recognise the characteristics and contribution of these areas to the sustainability and competitiveness of Europe's Cities and Regions.

The SURF project therefore supports the European Union Regional Policy 'Cities of Tomorrow'¹ report which states that "a coherent approach to smart, inclusive and green strategies must be adopted so that conflicts and contradictions between these different objectives can be overcome and accomplishment of one objective is not detrimental to meeting others".

The report continues to stress the importance of "a harmonious development of the city with its surrounding area"

SURF recognises the importance of green spaces to cities and green infrastructure in enabling connectivity and enhancing sustainable urban planning. It is becoming more evident that Europe would benefit from further recognition the level in which cities rely on the ecological services that the surrounding fringe area provide.

In order to advance the Europe 2020 aim for sustainable growth in particular, the Flagship initiative for a 'Resource efficient Europe', the SURF Project has identified significant opportunities for cities and regions offered by the urban fringe area, which if recognised could contribute to a higher quality of life in the EU and a more globally competitive European community.

SURF calls upon the Commission to ensure that future policy proposals and Directives unlock possibilities to secure the **economic, environmental and societal** future of these areas.

THE SUSTAINABLE URBAN FRINGE (SURF) PROJECT

¹ 1. "Cities of Tomorrow – Challenges, Visions, Ways Forward" report – October 2011

The Sustainable Urban Fringe (SURF - www.sustainablefringes.eu) project is part of the Interreg IVB North Sea Programme. The project has revealed examples of opportunities that the urban fringe area can provide to create competitive advantage while positively contributing to the resilience and sustainability of the cities and regions they connect with. The project, which runs from 2009 to 2013, comprises of 13 partners from five countries, involving local and regional authorities, municipalities and universities.

The urban fringe can be described as 'the zone between urban and rural areas, where urban and rural functions and qualities meet and interact. This interaction between urban and rural functions and qualities can have both positive results; (synergy between the functions and qualities and added quality) and negative results; (negative interference; loss of quality because of the interaction between different functions).'

SURF has identified that the urban fringe suffers from consistent obstacles, such as identifying ownership of land, issues with connectivity, lack of identity, fragmentation and encroachment.

The benefits of the urban fringe area encompass the three pillars of sustainable development (economic, environmental and social aspects), which is at the heart of European policy. Through the SURF partnership, practical examples of sustainable development have been analysed, providing a greater understanding of the tools and approaches used to effectively develop and manage urban fringe areas and the obstacles that many urban fringe areas have to overcome.

The key to the competitive advantage offered by the urban fringe lies in their unique structure, flexible possibilities and multi-functional opportunities offered by these areas that can compliment the ambitions of city regions. SURF has analysed examples of cities creating economic opportunities for entrepreneurs, engaging and collaborating with stakeholders to enable projects through community engagement and providing access to the green environment through enhanced connectivity to the city.

SURF has also found that urban fringe development can advance the priority for 'Inclusive growth - a high employment economy delivering social and territorial cohesion.' through providing opportunities for communities to make positive impacts on their own environments, create local employment, provide access to nature & recreation and support biodiversity. This compliments SURF's findings relating to the success within the project partnership of collaborative activities to enable urban fringe projects through 'soft' governance approaches.

EUROPE 2020

Europe 2020¹ calls for smart, sustainable, inclusive growth. Effective urban fringe management and development can directly advance all three priorities through the efficient use and management of the urban fringe to meet the needs of today's cities but not at the cost of future generations.

The recognition of opportunities presented by urban fringe areas has been discussed at European level for many years: The **European Spatial Development Perspective**² highlighted the necessity to integrate the surrounding countryside in the spatial development strategies of urban areas to improve the efficiency of land use planning.

More recently, **Janez Potocnik**, European Commissioner for the Environment has said: “There are many urban challenges... local authorities... have a key role to play: in the protection of the urban environment; in making a better quality of life for growing urban populations; and in ensuring good urban planning for the cities of tomorrow. Economic growth and urban, green revitalisation are now more important than ever for cities – especially when we all have to look carefully at our finances.”³ In terms of the economic climate and the timing of this proposal, it seems that we cannot ignore the opportunity to make the most of the natural resources already at our fingertips when we see the benefits they could bring.

SURF also notes that within The European Commission Sustainable Urban Development Cohesion Policy 2014 – 2020, the wider significance of the environment as a conduit to urban success is highlighted. It states that “the various dimensions of urban life – environmental, economic, social and cultural – are interwoven and success in urban development can only be achieved through an integrated approach. Measures concerning physical urban renewal must be combined with measures promoting education, economic development, social inclusion and environmental protection.”

Finally, **Dacian Ciolos**, Commissioner for Agriculture and Rural Affairs has added that “Above all, we need to get the research out of folders and into fields... Put very simply, Nature works in integrated ways. We now have to learn to do the same. We need an integrated approach to sustainability and resource efficiency.”⁴

Therefore the timely conclusion to the SURF project comes at a crucial time in the new and ever challenging economic climate of Europe to provide a flexible solution to increasing the competitiveness of Europe in the global market and providing a route to sustainable cities and regions.

CONCLUSION

The SURF Project has revealed that the opportunities presented by the urban fringe area are multi-functional, flexible to meet the demands of the cities and regions they connect with and offer opportunities for sustainable living for generations to come. SURF therefore stresses the importance of greater recognition in European policy planning of these areas.

² European Spatial Development Perspective, 1999 Agreed at the Informal Council of Ministers responsible for Spatial Planning

³ Janez Potocnik, European Commissioner for the Environment, Foreword of the Best Practice Catalogue – Award Cycle 2010 & 2011, European Green Capital Award

⁴ Dacian Ciolos, European Commissioner responsible for Agriculture and Rural Development, speech made at the Koli Forum, Joensuu, Finland, 15 Sept 2011

The SURF findings prove that in urban fringe areas the environment can be an asset not only for environmental reasoning but can also be a conduit to both economic and social prosperity. Effective guidance to its management can be an enabler for social cohesion, economic growth and sustainable development.

The SURF partnership, consisting of 13 partners from 5 countries calls for:

- Further recognition of the role urban fringe areas can play in the sustainable development of Europe’s city regions through enhancing economic development, enabling social cohesion and monitoring environmental sustainability.
- Commitment from the Commission to integrate the role of urban fringes within future urban policy.
- The development of specific urban fringe policy to support this.



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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise Planning & Infrastructure
DATE	31 st May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Olympic Torch Relay 2012
REPORT NUMBER:	EPI/12/127

1. PURPOSE OF REPORT

To provide details of the actual cost of hosting the Olympic Torch Relay 2012 and to request additional funds to ensure the efficient and effective delivery of this event in Aberdeen on Monday 11th and Tuesday 12th June 2012.

2. RECOMMENDATION(S)

1. To provide a further £45,000 from the Common Good Fund to support the organisation and safe delivery of the Olympic Torch Relay in Aberdeen. £35,000 has already been secured from the Common Good Fund following the approval of Full Council on 23rd February 2011.

These monies are sought from the Common Good Fund one off saving made in financial year 2011/12 of £80,000 from the International Partnerships & Twinning Budget due to a staff vacancy and reduction in project applications.

2. If the E P & I Committee is unable to approve these monies from the Common Good Fund, support is sought to call an Urgent Business Committee to consider a request for funding from another budget.

3. FINANCIAL IMPLICATIONS

Further to the report on 23rd February 2011, the final cost of hosting the Torch Relay in Aberdeen is detailed below.

The Olympic Torch Relay has two elements; the Torch Relay and a large evening celebration for 14,000 at the Castlegate.

Olympic Torch Relay 2012

Traffic Management (11 th & 12 th June)	£33,000
-includes Temporary Traffic Restriction Order, Hire of Variable Message Signs,	
Event Safety Stewards	£ 11,000
Portable Conveniences	£ 7,000
Second Screen, Feed and PA Equipment	£ 6,000
Local Entertainment	£ 5,500
Welfare-First Aid/Ambulance Service	£ 3,700
Event Safety Coordinator	£ 3,000
Support Staff	£ 2,000
Wristbands	£ 2,000
Generators & Safety Equipment	£ 1,500
Backstage Facilities	£ 1,000
Performing Rights Society for Music	£ 1,000
Licences-Public Entertainment/Raised Structure	£ 500
Meeting costs	£ 1,500
Cleansing	£ 500
Total	£79,200

Stage, Lighting, Performers and associated equipment including the cauldron and torch will be supplied by the Olympic Torch Relay organisers.

It had been hoped that Aberdeen City Council would be able to raise sponsorship monies in support of the event however, the London Olympic Games Organising Committee (LOGOC) advised that there are strong restrictions associated with local sponsorship that severely restricted our ability to sell the benefit of support to potential contributors.

Local sponsors are not able to receive any branding sites at the event or have any designation to be affiliated with the Olympic Torch Relay. The council can choose to thank local supporters in the media but it would have to come from the council only and they can't be associated with London 2012 or the Olympic movement.

4. OTHER IMPLICATIONS

Aberdeen City Council entered into a legal agreement with the London Olympic Games Organising Committee (LOGOC) to host the Olympic Torch Relay in 2011 and since then the City Events Team within Economic & Business Development have been working in conjunction with LOGOC to ensure the safe delivery of the event when it arrives in Aberdeen on Monday 11th and Tuesday 12th June 2012.

5. BACKGROUND/MAIN ISSUES

Since LOGOC's initial approach to Aberdeen City Council in 2012, officers within the City Events Team have been working to deliver a truly once in a lifetime opportunity for the city as an Olympic event host and also a great opportunity for visitors and residents of Aberdeen City and Shire to support local torchbearers who have been recognised for their outstanding achievements in the fields of charity, sport and youth work.

By hosting this event, Aberdeen will be presented to a global viewing audience of three billion people and will forever be associated with the London 2012 Olympics. A number of broadcasting opportunities are also available to Aberdeen to promote the event and those who have been chosen to act as torchbearers in the region.

From 6.24pm on Monday 11th June the Olympic Torch will arrive in Peterculter before travelling through Bielside at 6.39pm, Cults at 6.52pm and passing along Holburn Street before arriving at the city's evening celebrations in the Castlegate at 8pm.

As well as the Olympic Torch Relay, Aberdeen in conjunction with the Presenting Partners Coca Cola, Samsung and the Bank of Scotland and Supporting Partner BP will deliver an evening of personalities, entertainment and music that has never been staged in the city before. From 5-8.30pm, 14,000 people will witness Emeli Sande headline a family focussed event in the city centre that will make Aberdeen proud and provide exposure on a local, national and international scale that could otherwise never be achieved.

On Tuesday 12th June, the Olympic Torch Relay will set off at around 7.20am from BP's headquarters in Dyce, making its way via Bucksburn, the Haudagain Roundabout then onto Hilton Drive where the first torchbearer of the day will carry the Olympic Flame. The relay will then continue via North Anderson Drive and Anderson Drive, heading to the Bridge of Dee area and then onto the A90.

Specific information relating to the relay and evening celebrations continues to be confidential to ensure the integrity of the event but will be released when approved by LOGOC in the coming weeks.

To support the safe delivery of the Olympic Torch Relay across the region a Strategic Coordinating Group has been established to ensure clear, concise messages from all relevant agencies are released to businesses and the public to allow those wishing to attend the celebrations to make the necessary arrangements safely.

Information on the Olympic Torch Relay in Moray, Aberdeen City and Aberdeenshire can be found at www.readygrampian.org

6. IMPACT

Corporate

Vibrant Dynamic & Forward Looking

- Continue to support the best city festivals.
- Recognise the contribution of Sport, Culture and Arts to promoting the area as a tourist destination.
- Promote the City as a tourist destination.
- Economic promotion of Aberdeen locally, nationally and internationally, promoting all aspects of city life, wherever the City is represented.

Single Outcome Agreement

- National Outcome 1: We live in a Scotland that is the most attractive place for doing business in Europe.
- National Outcome 12: We value and enjoy our built and natural environment and enhance it for future generations.
- National Outcome 13: We take pride in a strong, fair and inclusive national identity.

Community Plan

- 10 % increase in the numbers of recreational and business tourists.
- Work with other agencies, including Visit Scotland, to encourage tourism and the provision of facilities for tourists.

Public

This will be a public event and it will be fully accessible to residents and visitors to Aberdeen on the 11th and 12th June 2012.

7. BACKGROUND PAPERS

None

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE:	Enterprise, Planning and Infrastructure Committee
DATE:	Thursday 31 st May 2011
DIRECTOR:	Gordon McIntosh
TITLE OF REPORT:	Draft Supplementary Guidance on 'Infrastructure and Developer Contributions Manual', 'Children's Nurseries and Sports Facilities' and 'Bats and Development'.
REPORT NUMBER:	EPI/12/112

1. PURPOSE OF REPORT

- 1.1 The Planning etc. (Scotland) Act 2006 paragraph 22 (1) states that a planning authority may adopt and issue guidance in connection with a local development plan. Aberdeen City Council has previously prepared a number of draft Supplementary Guidance documents and has consulted on 49 of these alongside the Proposed Aberdeen Local Development Plan for a period of 16 weeks (between 24th September 2010 and 17th January 2011). Following examination the Aberdeen Local Development Plan was adopted on 29 February 2012 with 24 associated Supplementary Guidance documents adopted just over a month later. It is intended that further tranches of approved Supplementary Guidance will be forwarded to Scottish Ministers in due course for adoption, whilst other Supplementary Guidance documents, such as those contained in this report, will be reported to Committee as they come forward.
- 1.2 This committee report presents three revised draft Supplementary Guidance documents:
- Infrastructure and Developer Contributions Manual (Appendix 1),
 - Children's Nurseries and Sports Facilities (Appendix 2), and
 - Bats and Development (Appendix 3).
- 1.3 The revised documents have been subject to amendments as a result of analysis of the representations received during the consultation between September 2010 and January 2011. A number of other amendments and updates have also been made by officers to provide greater clarity and to update the guidance where necessary.

1.4 This report also presents the representations received on the Supplementary Guidance documents and the officer responses to these representations (Appendix 4). All three documents are presented to the Council for approval for a 6 week period of public consultation.

2. RECOMMENDATION(S)

2.1 It is recommended that the Committee:

- (a) Note the representations received on the draft Supplementary Guidance documents concerned with the consultation undertaken alongside the Aberdeen Local Development Plan;
- (b) Approve officers' responses to these representations received on the draft Supplementary Guidance documents; and
- (c) Approve the revised draft 'Infrastructure & Developer Contributions' Supplementary Guidance, 'Children's Nurseries and Sports Facilities' Supplementary Guidance, and draft Supplementary Guidance document – 'Bats and Development' for a 6 week consultation period.

3. FINANCIAL IMPLICATIONS

3.1 There are no immediate financial implications arising from this report as the cost of progressing the Aberdeen Local Development Plan can be met from existing budgets.

4. OTHER IMPLICATIONS

4.1 This is the continuation of a significant piece of work that has involved and will impact upon many other council services, public bodies, the business and development industries and the citizens of Aberdeen.

4.2 As a major landowner in the city, proposals for the development of land and assets owned by Aberdeen City Council will be subject to assessment in line with the principles and standards set out in the Supplementary Guidance, where applicable.

4.3 The progression of the Supplementary Guidance documents will provide clearer advice for decision making, with detailed topic-based Supplementary Guidance also having value in reducing officer time spent on pre-application discussions.

4.4 The Supplementary Guidance has been subject to Strategic Environmental Assessment (SEA) alongside the Local Development Plan.

5. BACKGROUND/MAIN ISSUES

- 5.1 The draft Supplementary Guidance documents presented alongside this report has previously been consulted upon as part of the Aberdeen Local Development Plan – Proposed Plan. The Council agreed to the content of the Proposed Plan on 18 August 2010. The Proposed Plan was a critical stage in the plan preparation process and represented the Council’s settled view on what the final content of the adopted Plan should be. The production of the Proposed Plan and the draft Supplementary Guidance was the result of a significant amount of assessment and public consultation. The Aberdeen Local Development Plan has since been adopted by the Council on 29th February 2012.
- 5.2 As part of the Local Development Plan process, a 16 week period of consultation was undertaken between September 2010 and January 2011, during which representations from the public and other organisations were invited on the Proposed Plan and draft Supplementary Guidance documents.
- 5.3 Council officers have taken into account representations that were received in relation to the draft Supplementary Guidance. Please see Appendix 4 for further details of the representations. As a result of the issues raised, a number of changes have been made to the draft Supplementary Guidance, as well as the updating of various policy and factual references. We are now seeking approval for the amended draft Supplementary Guidance to be issued for a further 6 week period of consultation.
- 5.4 The following section outlines the draft Supplementary Guidance documents, the responses received during consultation and the officer responses to these responses.
- 5.5 The aim of the 'Infrastructure and Developer Contributions Manual' Supplementary Guidance is to provide clear guidance to all stakeholders, in particular the development industry, on the infrastructure and developer contributions likely to be required to support development from an early stage in the preparation of the LDP. 11 representations were received. The comments received came from Bancon Developments Ltd, Dundas & Wilson CS LLP, Emac, GVA Grimley, Homes for Scotland, Kincorth/Leggart Community Council, Paull and Williamsons LLP, Scottish Water, SEPA, SportScotland and Stewart Milne Homes. The comments ranged from wholesale disagreement with the idea of the Manual to minor amendments requested to the text. Following detailed analysis of all the responses changes have since been made to the Infrastructure and Developer Contributions Manual.
- 5.6 The aim of the 'Children's Nurseries and Sports Facilities' Supplementary Guidance is to provide clear guidance for developers and planning officers so that they can ascertain the main

considerations involved in assessing the change of use of any land or building into a Children's Nursery or Sports Facility'. One representation was received in relation to this document from Sportscotland. This, and the officer response, can be seen in Appendix 1.

- 5.7 The aim of the 'Bats and Development' Supplementary Guidance is to provide information that will be needed for developers and planning officers so that they can consider the effects of planning and development on bats which are a European Protected Species. There were a number of comments concerned with section 7 'Establishing if a Survey is Required' and changes were made in order to make the section more clear for planning officers.

6. IMPACT

- 6.1 The Local Development Plan continues to support the vision of Aberdeen becoming an even more attractive place to live and in which to do business and will ensure that high quality employment opportunities exist. This process aspires to improve the access that the people of Aberdeen have to high quality services that meet their needs. The development and refinement of fit for purpose Supplementary Guidance to assist the Aberdeen Local Development Plan is paramount to supporting this vision and achieving the goals that Aberdeen aspires to.

- 6.2 The vision for Aberdeen is to be a city which is vibrant, dynamic, forward looking – an even better place to live and work, where people can expect high-quality services that meet their needs. For Planning and Sustainable Development this means making a visible difference to the quality of the city's urban and natural environment by promoting high quality development and providing an effective infrastructure to make us a world class strategic location.

- 6.3 To do this we must think strategically, facilitate development, engage positively with communities and the business sector and be open and transparent in our decision making. We also have a key role in delivering the vision for the City and Shire as expressed through regional plans and strategies. Planning and Sustainable Development is tasked with seeing that Aberdeen stays at the forefront of planning for the future.

- 6.4 The Supplementary Guidance presented in this report relates to the following Single Outcome Agreement objectives:

- 1- We live in a Scotland that is the most attractive place for doing business in Europe;
- 2- We realise our full economic potential with more and better employment opportunities for our people;

- 10- We live in well-designed, sustainable places where we are able to access the amenities and services we need;
 - 12- We value and enjoy our built and natural environment and protect it and enhance it for future generations;
 - 13- We take pride in a strong, fair and inclusive national identity; and
 - 15- Our public services are high quality, continually improving, efficient and responsive to local people's needs.
- 6.5 The Supplementary Guidance presented in this report meets the vision of the Community Plan in promoting a strong image of the City and a sense of civic pride.
- 6.6 The Supplementary Guidance presented in this report supports the Council's 5 year Business Plan in terms of enhancing the built environment, attracting visitors, workers and investment to protect the economic future of the city, and, to facilitate new development projects to improve Aberdeen's living and working environment.
- 6.7 An Equality and Human Rights Impact Assessment has previously been carried on the Supplementary Guidance which shows that there will be some positive impacts of the Plan on a range of equalities groups.

7. BACKGROUND PAPERS

- Appendix 1 - Draft Infrastructure & Developer Contributions Manual
- Appendix 2 - Draft Children's Nurseries & Sports Facilities
- Appendix 3 - Draft Bats and Development
- Appendix 4 - Summary of representations received and officers' responses.
- Aberdeen Local Development Plan 2012
<http://www.aberdeencity.gov.uk/localdevelopmentplan>
- Aberdeen Local Development Plan – Draft Action Programme
http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_action_programme.asp
- Aberdeen City and Shire Strategic Development Planning Authority: Aberdeen City and Shire Structure Plan
<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=423&SID=149>
- Planning etc. (Scotland) Act 2006
http://www.opsi.gov.uk/legislation/scotland/acts2006/asp_20060017_en_1
- Scottish Planning Series: Planning Circular 1/2009: Development Planning
<http://www.scotland.gov.uk/Resource/Doc/261030/0077887.pdf>
- The Town and Country Planning (Development Planning) (Scotland) Regulations 2008
http://www.opsi.gov.uk/legislation/scotland/ssi2008/ssi_20080426_en_1

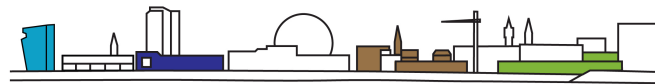
8. REPORT AUTHOR DETAILS

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aberdeen local development plan

Appendix 1

Supplementary Guidance

Topic: Infrastructure and Developer
Contributions Manual

May 2012

Introduction

New development can have a very positive effect on an area, providing new homes, jobs and economic prosperity. However, development can also place additional pressures on resources and infrastructure such as schools, community and leisure facilities, transport infrastructure, health services and the local environment. The delivery of infrastructure alongside new development can help to create balanced, accessible and sustainable communities.

Scottish Government emphasises the need to plan more effectively for the infrastructure required to support new development and published new planning policy guidance (Circular 1/2010 Planning Agreements) to provide the framework for securing the delivery of infrastructure and financial contributions to support development.

Our approach to Infrastructure Delivery

Through the Local Development Plan, Aberdeen City Council's approach to the planning and delivery of infrastructure is open, transparent, and is supported by a robust evidence base. Our aim is to provide clear guidance to all stakeholders, in particular the development industry, on the infrastructure required to support development from an early stage in the preparation of the LDP.

Policy I1 of the Local Development Plan states that all development should be supported by the infrastructure and developer contributions necessary to support the proposed development.

The purpose of this Infrastructure and Developer Contributions Manual is to provide clear guidance on the methodology used to identify infrastructure requirements and the criteria that should be used to calculate developer contributions to support new development. This document is published as Supplementary Guidance to the Aberdeen Local Development Plan.

The Action Programme which accompanies the Local Development Plan outlines further details on the delivery of supporting infrastructure. The Action Programme is a 'live' document that will be updated regularly to take account of changes in circumstances as sites come forward through the planning process.

From an early stage in the preparation of the Local Development Plan, the City Council has worked in close partnership with a wide range of agencies – through the Future Infrastructure Requirements for Services Group (or FIRS) – to establish the infrastructure requirements to support development. The FIRS Group comprises representatives from the following organisations:

- Aberdeen City Council, including colleagues representing Roads, Public Transport, Transport Policy, Housing Strategy, Education, Culture & Sport, Development Management and the Planning Gain Team;
- Transport Scotland;
- Scottish Water;
- Nestrans; and
- NHS Grampian.

The FIRS Group has helped to identify the infrastructure required to support new development and the results of this work are set out in Appendix 4 of the Local Development Plan and the LDP Action Programme. The LDP Transport Group, which comprises public and private sector partners involved in transport issues, also helped to determine transport infrastructure requirements and contributed to the findings of the FIRS Group.

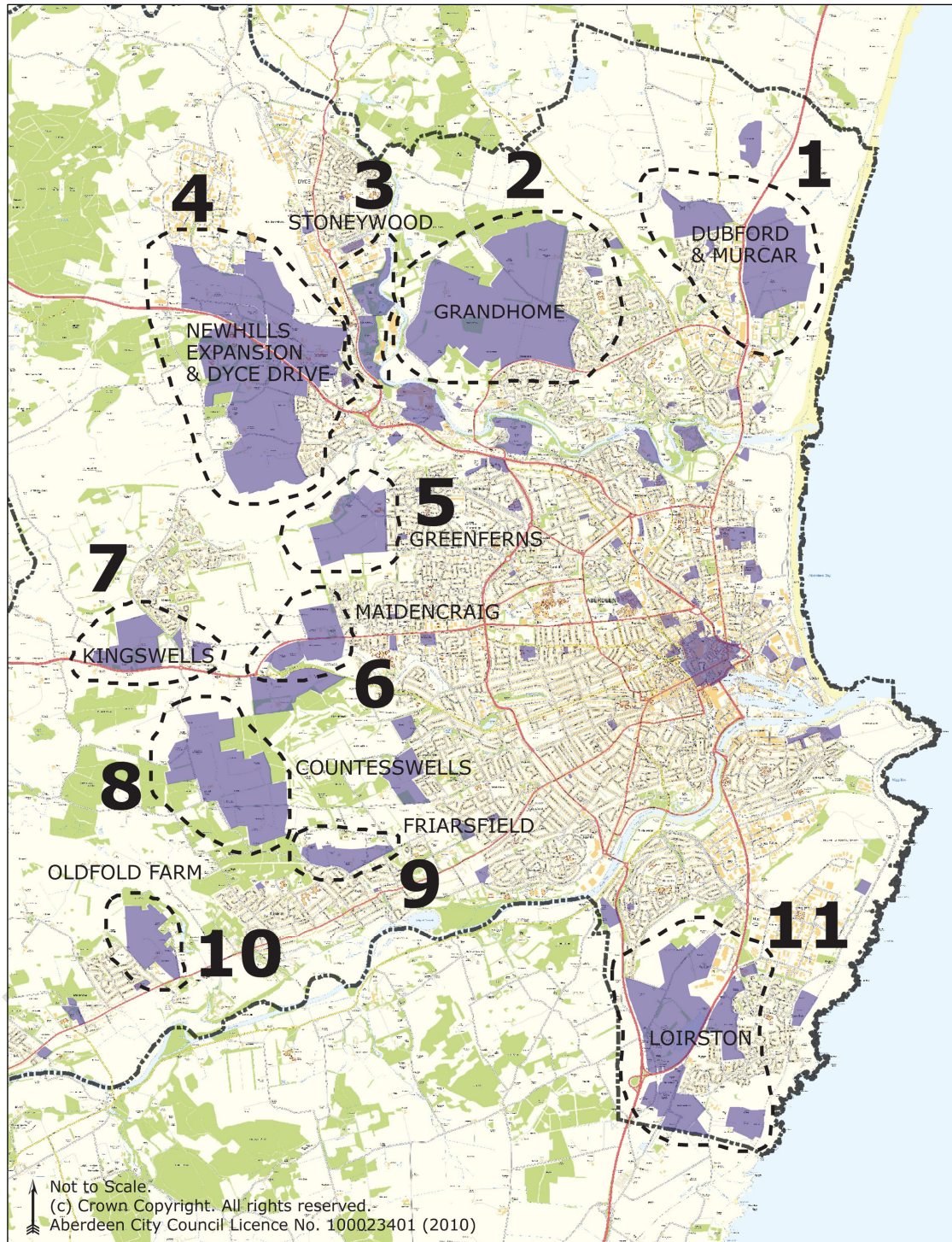
Masterplan Zones

The Council has identified eleven Masterplan Zones, shown in the Table and Map below, as well as Figure 1 in the Local Development Plan, within which developers will be expected to work together to prepare Masterplans for each zone reflecting the planning and delivery of associated infrastructure requirements set out in this document. A map showing the extent of the Masterplan Zones is displayed on page 4. Further details on the timing and delivery of infrastructure identified below are set out in the Action Programme which accompanies this Plan.

The requirements listed are based on the most up-to-date information currently available.

Masterplan Zone	Sites included
1 – Dubford & Murcar	OP25 Dubford OP2 Murcar
2 – Grandhome	OP12 Grandhome OP10 East Woodcroft
3 – Stoneywood	OP24 Stoneywood
4 – Newhills Expansion & Dyce Drive	OP26 Walton Farm & Craibstone North OP29 Craibstone South OP27 Land near Bucksburn School OP30 Rowett South OP28 Rowett North OP31 Greenferns Landward
5 – Greenferns	OP45 Greenferns
6 – Maidencraig	OP43 Maidencraig South East OP44 Maidencraig North East
7 – Kingswells	OP40 West Hatton, Home Farm, Home Farm Kingswells OP41 Kingswells C OP42 Kingswells D and West Huxterstone
8 – Countesswells	OP58 Countesswells
9 – Friarsfield	OP51 Friarsfield
10 – Oldfold Farm	OP62 Oldfold
11 – Loirston	OP78 Charlestown OP76 Souter Head Road OP77 Loirston

Map showing the Masterplan Zones



All development sites, whether listed or not within the Masterplan Zones table in this document, LDP or Action Programme, will have to provide the necessary infrastructure and developer contributions necessary to mitigate the impact of development. The means to how this will be calculated will be based on the criteria set out in the following sections.

Masterplans will need to reflect the infrastructure requirements identified and should include a Delivery Statement setting out details of how the proposed development, and the accompanying infrastructure, will be delivered. Subsequently, actions

relating to the delivery of infrastructure will need to be defined once the Masterplan and Delivery Statement have been agreed with the Council and any key agencies.

Methodology for Infrastructure Requirements and Developer Contributions

This section explains the process undertaken by the FIRS Group to calculate the infrastructure requirements for the Masterplan Zones. It also sets out details on the criteria that will be used to assess the precise level of infrastructure requirements and developer contributions for all development proposals.

These criteria will need to be used when considering the infrastructure and developer contributions required in connection with any development proposal. The precise level of infrastructure provision and developer contributions required from any development will need to be agreed with the Council, in consultation with other statutory agencies. Any costs for infrastructure will be reviewed annually and adjusted according to an agreed indexation.

The methodology seeks to ensure that appropriate contributions are secured from developers to support new communities and to make a fair and proportionate contribution to the cumulative impact of development across the City and, where appropriate, the region as a whole. The Council has been careful to avoid deterring development by making unreasonable demands, and emphasise that any infrastructure or developer contributions sought are proportionate to the development proposed.

Transport

The Transport Framework process, which supported the preparation of the Local Development Plan, helped to identify the transport infrastructure likely to be required to support new development. A number of transport related projects are already being developed by the Council in partnership with key agencies and our neighbouring authority Aberdeenshire Council, to improve the regional transport network. These include the Aberdeen Western Peripheral Route, Haudagain roundabout improvements and The Third Don Crossing, and are likely to bring about significant benefits to people living and working in the North East. Schemes listed in Policy T1 are not expected to be subject to developer contributions and details of how they will be delivered are set out in the Action Programme.

The transport schemes associated with new developments allocated for the first time in this Local Development Plan are separated into two categories:

- Strategic Transport Infrastructure – These projects have been identified by a partnership group consisting of officers from Aberdeen City Council, Aberdeenshire Council, the Strategic Development Planning Authority, Transport Scotland and NESTRANS. Funds realised from this initiative will be managed and delivered by Nestrans.
- Local Infrastructure. - These transport projects are expected to be provided as part of new development and should be reflected in the associated Masterplan and subsequent planning applications

The following provides an overview of the various types of transport related infrastructure items and contributions that may be sought.

- **Strategic Transport Fund**

The City Council and Aberdeenshire Council, in partnership with Nestrans, Transport Scotland and the Strategic Development Plan Team have explored options for delivering strategic infrastructure as development comes forward for implementation across the region.

Transport modelling has been carried out to assess the cumulative impact of development proposed across the North East and to consider the scale of strategic transport interventions likely to be required to support the level of development identified in the Aberdeen City and Shire Structure Plan. This modelling work has identified key points on the transport network where interventions are likely to be required to address the cumulative impact of development. Supplementary guidance has been produced by the Strategic Development Planning Authority which proposes an equitable way of funding these projects through developer contributions from qualifying sites coming forward for development. The purpose of the guidance is to support development while also addressing the impact of new development on the transport network.

The following sets out the process for identifying the opportunity sites within the LDP that will contribute towards the Strategic Transport Fund.

1. If the application site was allocated in the 2008 Local Plan, strategic transport contributions would **not** be required, **unless** an alternative use or larger scale of development is proposed which generates a greater transport impact than that anticipated from the 2008 Local Plan allocation. In that case, the precise level of Strategic Transport Contributions would need to be identified in order to mitigate the wider impact of the development on the transport framework.
2. For any 'new' developments allocated for the first time in the Local Development Plan, these sites would make a strategic transport contribution based on the corresponding standards for housing and employment land set out in the Supplementary Planning Guidance 'Delivering Identified Projects Through a Strategic Transport Fund'.

'Windfall' sites that have not been allocated in the Local Development Plan will also be liable to make a contribution towards the Strategic Transport Fund. For some brownfield developments, there may be instances where the transport impact of the development proposal is equivalent to or less than the impact of its most recent operational use. Such factors would need to be taken into account in the consideration of transport impacts and, where applicable, the negotiation of mitigation measures and developer contributions.

Contributions for transport related infrastructure will be held for a 20 year period to enable the provision of appropriate infrastructure.

Further information is detailed in the Supplementary Planning Guidance document Delivering Identified Projects Through a Strategic Transport Fund which can be viewed through the Strategic Development Planning Authority website at <http://www.aberdeencityandshire-sdpa.gov.uk>

- **Local Transport Infrastructure and Mitigation**

All developments, regardless of Strategic Transport Fund (STF) requirements, will be assessed in terms of their impact on the local transport network and may be required to mitigate these impacts. Indicative costs associated with the most frequently required mitigation measures are outlined in this document. Developments may be required to make an appropriate contribution towards one or several of the mitigation measures outlined in this document or any other measure that the Council deem to be appropriate.

General Principles for All Development:

The following principles will apply to all development within the Aberdeen City Council area:

- Requirements will vary from site to site. The developer will be expected to provide and meet, in full, the cost of all external works identified as requirements through the planning process
- All developments will make an appropriate contribution towards local transport infrastructure and/or services related to that development, to ensure that the required facilities/infrastructure provision is in place in time to mitigate the impacts of the development
- Developer Contributions will be used to provide improvements to public transport, the local road network, traffic management, pedestrian and cycle facilities, accessibility infrastructure or any other such improvements
- Different development sites will have different transport requirements. The exact contribution required for each site will be determined on a case by case basis, for developments where significant impacts are likely a full Transport Assessment (TA) will be required to inform the process. The thresholds for when a TA will be required are set out in the Transport and Accessibility Supplementary Guidance
- Developers should be aware of, and take account of the requirements to provide such contributions. We encourage pre-application discussions with appropriate Council officers at the earliest possible stage
- The principles set out in this Developer Contributions Manual do not negate the requirement for either a Transport Assessment or a Development Framework /Masterplan/Planning Brief and should be applied as a basis for addressing transport impacts alongside and in conjunction with the preparation of these where they are required
- Developers should use these Guidelines in conjunction with the policies and principles set out in the Transport and Accessibility Supplementary Guidance and in terms of walking and cycling, the Open Space Supplementary Guidance

Assessment of Appropriate Level of Developer Contributions:

The full impact of the development on the local transport network will be taken into account when assessing the required levels of local mitigation.

The following mitigation measures are examples of some of the factors which will be considered during the assessment of any development. This list is not exhaustive but provides a clear starting point for discussions between the developer and the Council. An indication of the likely costs involved in these measures is provided in a table at the end of this section.

- **Road Improvements and Public Realm**

A key priority in the assessment of any development proposal is to ensure that sufficient infrastructure is in place to accommodate the development. This means the provision of or upgrading of roads, bridges or other infrastructure, including carriageways and footways. This may be required either within or outwith the development site or both and planning conditions/legal agreements will be secured accordingly.

- **Alterations to Existing Roads**

Any alterations to existing roads that are required as part of a development will normally be undertaken as part of the development construction and progressed as part of a section 56 or a roads Construction Consent application. However, there may be instances where the Council may wish to take on this construction, if this is the case, this will be fully funded by the developer.

- **Footway Crossings**

In order to ensure that footway crossings are adequately constructed, they will normally be undertaken by the Council. The Council will charge the developer for the cost of the construction of a footway crossing.

- **Public Transport Priority**

In order to ensure that public transport services can adequately gain access to and serve a development and to ensure that existing services are not adversely impacted by the development, developers may be required to provide or fund public transport priority measures. These include, but are not limited to, bus lanes, bus-only sections of road and bus priority traffic signals.

- **Supported Bus Services**

Developers may be required to enter into a legal agreement which enables the up front payments to fund new bus services or to underwrite a new service for an agreed period of time. This may involve a completely new service or extending/improving an existing service. New and upgraded facilities may be required in order to deal with increased demand on public transport services, these may include the provision of or upgrade of bus stops, bus shelters and Real Time Information systems.

- **Traffic Signals**

Development of any kind has the potential to impact on the road network. As a result of this impact, developers may be required to provide for the

installation of new traffic signals, controlled pedestrian crossings and the upgrading/refurbishment of existing traffic signal infrastructure.

- **Lighting**

Any development which requires new roads or the upgrading of existing roads will be expected to fund the installation of new lighting or the upgrading of existing lighting infrastructure where appropriate.

- **Traffic Calming Measures**

Development may generate the need for traffic calming measures such as speed humps, chicanes, kerb extensions and shared space schemes which the developer may be liable to fund.

- **Cycling/Walking Routes**

In accordance with the Transport and Accessibility Supplementary Guidance standards for access and permeability, developers will be required to provide safe routes for cyclists and walkers which are planned in accordance with the 5 C's – connected, convenient, comfortable, convivial and conspicuous as referred to in the Aberdeen Local Transport Strategy 2008-2012. This may include both on site and off site cycle parking infrastructure.

- **Safer Routes to Schools**

Developers will be required to provide safe routes to schools through the provision of measures including dropped kerbs, crossing points, speed cushions, central refuges and exclusive cycle/pedestrian paths.

- **Car Clubs**

Car Clubs provide a more sustainable option to individual car ownership and are often beneficial where full parking provision is limited. Developers may be required to make a contribution to the Aberdeen Car Club for the provision of spaces, vehicles or membership for the individuals living/working in their development.

- **Bus Permits/Tickets**

The promotion of public transport use over individual car ownership will always be encouraged, particularly where full parking provision is limited. Dependant on circumstances, developers may be required to enter into an agreement with local Public Transport Operators (First Bus and Stagecoach) so that annual bus passes can be purchased and used to residents of the development.

- **Travel Plan**

Travel Plans contain a package of measures aimed at promoting sustainable transport choices to and from developments. All developments are encouraged to prepare Travel Plans in support of planning applications. The thresholds for developments which require a Travel Plan are set out in the Transport and Accessibility Supplementary Guidance. For major applications it will be necessary for the developer to enter into a legal agreement detailing target aspirations, monitoring and actions for the revision of Travel Plans.

- **Traffic Regulation Orders (TROs)/Stopping –Up Orders**

Where Orders are required in order to facilitate development, the developer will be required to pay the Council's administration costs in addition to paying for the infrastructure to support the Order. This may include bollards, road markings and signage.

Transport Indicative Costs Tables:

The purpose of this table is to provide developers with an indication of the costs, where available, involved in meeting the transport mitigation measures set out above.

The requirements and costs will vary from site to site and developers are expected to meet in full the cost of all external works identified in the Transport Assessment (TA) and/or through the planning process. It is recommended that developers take account of these estimates when costing development proposals to avoid difficulties later in the planning process. However, a guarantee cannot be given as to the actual costs arising from the assessment of individual proposals. Early discussion with the Council is encouraged. The tables below set out the indicative costs of specific items for information.

Transport Mitigation Measure	Applicability	Indicative Cost
Road Improvements and Public Realm	This may be a requirement for retail, residential or commercial developments, dependant on the resulting impact on the local transport network. This may include improvements to carriageways and footways were necessary.	The developer will be required to undertake these works to a specification acceptable to the Roads Authority. The costs will vary significantly depending on the scale of works required. Dependant on circumstances, the developer may be required to contribute funds to improvements being constructed by the Council or others.
Alterations to Existing Roads	Normally this would form part of works constructed by the developer although occasionally the Council may wish to take on construction, at a cost to the developer.	The developer will be required to undertake these works to a specification acceptable to the Roads Authority. The costs will vary significantly depending on the scale of works required. Dependant on circumstances, the developer may be required to contribute funds to improvements being constructed by the Council or others.

Transport Mitigation Measure	Applicability	Indicative Cost
Footway Crossings	This will be a requirement for any development that includes a driveway crossing over an existing footway or road verge. The Council will normally undertake this work, at a cost to the developer.	The approximate cost for a dropped kerb is £800-£1500.
Public Transport Priority	This may be a requirement for major retail, residential or commercial developments, dependant on the extent and nature of existing priority arrangements and how well this could address the impact of development.	It is not possible to provide costs. Where this is a requirement the cost will be determined by the work required.
Supported Bus Services	In principle this may be a requirement for major retail, residential or commercial developments.	<p>It is not possible to provide costs for the extension of existing bus services or the introduction of a new service as this will depend entirely on the service/area in question and the magnitude of change.</p> <p>A new bus shelter would cost approximately £3500.</p> <p>The installation of Real Time Information display would cost approximately £3500-£5000 per site.</p> <p>Upgrading existing infrastructure to include raised access kerbs costs approximately £2000 per site.</p> <p>Upgrading an existing site to include courtesy lighting costs approximately £500-£2000 per site, depending on the availability of electrical feed.</p>
Traffic Signals	This requirement will apply in principle to the majority of	The cost will vary depending on what is required. A single pedestrian crossing costs

Transport Mitigation Measure	Applicability	Indicative Cost
	developments.	approximately £30,000 whereas signalling a four arm junction costs approximately £100,000.
Lighting	This requirement will apply to any development which requires new roads or the upgrading of existing roads.	It is not possible to provide costs. Where this is a requirement the cost will be determined by the work required.
Traffic Calming Measures	The requirement to provide traffic calming on roads may be required where there is likely to be any increase in traffic movements on surrounding residential streets.	The developer will be required to provide these improvements. A road hump or set of cushions costs approximately £1000 per feature and they are required at 75m intervals. An entrance treatment for a 20mph zone costs approximately £1500 but can vary.
Cycle/Walking Routes (excluding those facilities already required through the Roads Improvement and Public Realm category)	This requirement will apply in principle to all new developments. The requirement to provide cycle/walking routes is set out in the Transport and Accessibility Supplementary Guidance. In addition, developers will be required to fund external links to connect with the wider cycle/walking network and this may include both onsite and offsite cycle parking infrastructure.	<p>The developer will normally be required to provide these improvements. Developers should refer to the 'Core Paths Network' section of this document for guidance on likely requirements. The approximate cost of a cycle parking stand (Sheffield standard) is £150.</p> <p>The approximate cost of a cycle locker is £400.</p> <p>The approximate cost of a covered cycle shelter is £2500.</p>
Safer Routes to Schools (excluding those facilities already required through the Roads Improvement and Public Realm category)	This requirement will apply in principle to all new residential developments. Developers may be required to make payments towards mapping of routes, cycle training and walking bus initiatives.	It is not possible to provide costs. Where this is a requirement the cost will be determined by the work required.

Transport Mitigation Measure	Applicability	Indicative Cost
Car Club	This requirement may apply to all city centre residential developments and other developments where full parking provision is limited.	<p>For residential developments:</p> <p>For the majority of residential developments a contribution of £400 per unit will be required. This contribution covers the cost of 2 years annual membership per unit. In certain circumstances a development may wish to make a contribution towards Car Club vehicles. Under these circumstances the overall membership contribution (£400 per unit) may be negotiated.</p> <p>For Non-residential developments:</p> <p>Each development will be dealt with on a case by case basis taking into account location, size and provision of parking.</p>
Bus Permits/Tickets	This requirement may apply to developments where full parking provision is limited.	The current cost of providing an annual bus permit for use with First Bus services is £660 and the approximate cost of providing an annual bus permit for use with Stagecoach services ranges from £510-£1790 (dependant on bus route).
Travel Plan	The thresholds for developments which require a Travel Plan are set out in the Transport and	To be provided by Developer. Through Masterplan process there will be a requirement to produce a Travel Plan Framework with individual

Transport Mitigation Measure	Applicability	Indicative Cost
	Accessibility Supplementary Guidance although all developments are encouraged to prepare Travel Plans in support of planning applications.	developments complying with the framework.
Traffic Regulation Orders/Stopping-Up Orders	This requirement may apply to all developments.	The cost per Order is approximately £1500 which must be paid regardless of whether the Order is successful or not. The developer would also be required to pay the costs of the work.

Note: Costs are correct as of May 2012.

- **Air Quality**

Local Development Plan Policy NE10 states that planning applications for development which have the potential to have a detrimental impact on air quality will not be permitted unless measures to mitigate the impact of air pollutants are included. Mitigation measures should ensure that the development ameliorates against any impact on local air quality, and where the mitigation measures proposed do not fully mitigate the impact of development on air quality, further contributions may be sought by the Planning Authority.

Further details can be sought from the Supplementary Guidance document on Air Quality.

- **Core Paths Network**

New development will be required to incorporate, enhance and link to the Core Paths Network. Infrastructure requirements relating to walking and cycling facilities, such as core paths, are set out under Policy I1 of the Local Development Plan, and section 3 of the LDP Action Programme. These new and improved paths will need to be incorporated into the corresponding Masterplan Zones identified.

The criteria for assessing whether a proposed development needs to contribute to the Core Paths Network is as follows:

- ➔ Development which incorporates and enhances existing core paths and provides links to the Core Paths Network will not require any financial contribution providing that the measures proposed are appropriate for the level of development proposed and are agreed with the City Council as Access Authority.

- ➔ Where there is no provision of core paths or links to the Core Paths Network, developers will be required to make a financial contribution at a rate of £371 per dwelling or per-house-equivalent for non-residential developments (see below).
- ➔ Where a developer proposes provision of, or improvements to, the Core Paths Network and the Council, as Access Authority, considers such measures to be inadequate, a financial contribution will be sought to facilitate and mitigate the level of development proposed.

The rate of £371 per-dwelling or per-dwelling equivalent is based on current costs for the provision of Core Paths in Aberdeen, taking account of the level of provision across a number of communities.

The formula for calculating per-house-equivalent is based on the gross floor area (GFA) of the commercial premises plus 50% of concrete/tarmac areas and 25% of hardcore areas (Gross External Area GEA) to give an equivalent number of house units at 10 units per acre that could have been erected on a similar area. Where the number of car parking spaces has been indicated the standard area required for a parking space of 24 m² will be used.

$(GFA + (GEA/2)) / 400 = \text{HUE (House Unit Equivalent) @ 10 per acre.}$

E.g. 2,000 m² food store + 4,000 m² car park = $(2,000 + 2,000) / 400 = 10 \text{ HUE.}$

An individual site usage weighting factor (WF) is then applied to reflect the probable occupation level of the site. These are as follows:

Bulky Goods- retail w/house	0.25
Fast food	0.75
Hotel	0.5
Leisure etc	0.25
Non Food retail	0.5
Office & other commercial	0.75
Petrol Station/garage	0.25
Restaurant	0.75
Retail food	1
Showroom	0.1
Warehouse	0.05
Workshops/engineering	0.2

Affordable Housing

All new developments of five housing units or more are required to contribute no less than 25% of the total number of units as affordable housing as stated in LDP Policy H5 Affordable Housing. Further detail on the policy is set out in the Supplementary Guidance document on Affordable Housing.

Schools

Officers from the Education Culture & Sport Service, Planning Gain Team and the Local Development Plan Team undertook a joint assessment of the impact of

proposed development on the provision of education services, and the need for new and extended schools. This work took account of the 2009 School Roll Forecasts, published in May 2010, which includes the most up to date information on current school capacities and the likely spare capacity available in the future. Collaborative work between all partners is ongoing. This will ensure that the impact upon educational capacity due to increased housing development is fully taken into account and will be achieved by continued dialogue with the development industry in order to determine likely start dates, build rates and phasing of construction.

In the future a key challenge will be to ensure that the Council has a reliable mechanism for securing appropriate payments from developers. In addition to this it will be important that the Council is prepared and ready to utilise any contributions received from the development industry to make necessary improvements to the City's education provision and to monitor any changes resulting from new development on the demographic mix in different areas of the City.

The actual impact on future development will be determined at the masterplanning and planning application stages and this will depend on:

- continued discussions with Aberdeenshire Council regarding cross-boundary developments and growth;
- update of the school roll forecasts;
- Revision of the school catchment areas; and
- any changes to the provision of educational services.

Infrastructure requirements for education provision is based on the anticipated number of pupils from a proposed development and their effect on the Primary and Secondary school rolls serving that development averaged out over a 5 year period from the anticipated development start date. Up to the point where development proposals will utilise capacity at existing schools, the impact will be calculated on the basis of the pupil-per-household (pph) ratio for the relevant catchment in the School Roll Forecasts. Where new facilities or extensions are required, the impact will be based on 0.25 pph for Primary and 0.175 for Secondary. It is assumed that these figures include an element of 1 bed housing and therefore 1 bed units should not be excluded from the calculations. The required level of contribution is then determined from the 'rate per pupil' required to provide the following:

Relocatable accommodation should be provided where the school roll is expected to, or already, exceeds the capacity of the school and is forecast to fall back to an under capacity position during the forecast period, using the following figures:

Primary – Temporary accommodation	= £7,000 per pupil generated
Secondary – Temporary accommodation	= £9,000 per pupil generated

OR

New build accommodation should be provided where the school roll forecast trend is for it to grow beyond its current capacity or it is already above capacity and the additional pupils from the development will add to this over capacity problem.

Primary – Permanent extension or new build school	= £23,000 per pupil generated
Secondary – Permanent extension or new build school	= £44,000 per pupil generated.

On larger residential developments the applicant may be required to provide a school site. For Primary provision this should be 3.25 hectares or thereby and for secondary schools this should be 10 hectares or thereby of serviced and where possible reasonably flat land. Where both primary and secondary school sites are required the applicant may wish to investigate the possibility of a shared school campus design layout.

Where community related facilities are provided within schools these are reflected in the rates charged. On occasion the requirements for other community facilities may be reduced where these are to be provided in whole or in part within a school campus.

Water

Scottish Water are the statutory agency for water related issues associated with new development and representatives from Scottish Water have provided the Council with a view on the Water and Drainage Infrastructure required to support new development.

Scottish Water can advise of any major infrastructure issues that are known in an area, for example a pump station which is at capacity. However, they cannot provide detailed information on the impact of a development without undertaking modelling investigations. In order for these investigations to be meaningful, detailed information will be required from developers, which is unlikely to be available when they first purchase the land. Regulatory guidelines stipulate that the cost of the modelling work must be met by the developer, however some costs may be reclaimed as part of their Reasonable Cost Contributions (under the Provision of Water and Sewerage Services (Reasonable Cost) (Scotland) Regulations 2006). Costs for upgrading the network would not be available without first undertaking these investigations.

Scottish Water requires developers to demonstrate that they have satisfied 5 essential criteria before a connection offer is made. The five criteria are outlined below.

1. The development is supported by the Local Development Plan and has outline or full planning permission.
2. The developer must confirm land ownership or control through a solicitor's letter.
3. The developer must confirm plans are in place to mitigate any network constraints that will be created by the development through a Minute of Agreement with Scottish Water or alternatively a letter showing commitment to mitigate network impact through Part 3 investment.
4. The developer must confirm any time remaining on current planning permissions with your local council.
5. The developer must be able to demonstrate reasonable proposals in terms of your development's annual build rate.

Upon completion of this feasibility stage the developer may proceed with submission of a formal application to Scottish Water. This will lead to the provision of Technical Approval for the designs and for the overall connection application.

Under current funding rules, which Scottish Water are governed by, the cost and programming of any work associated with the Part 4 (strategic) infrastructure, such as water treatment works, are the responsibility of Scottish Water. Parts 1, 2 and 3 infrastructure are the responsibility of the developer, where financial contributions for Parts 2 and 3 infrastructure can be claimed back from Scottish Water if applicable. This again is done as part of the Reasonable Cost Contributions (Reasonable Cost) (Scotland) Regulations 2006.

Scottish Water, the City Council and other relevant partners are exploring the opportunity to address the cumulative impact of development across the region. Therefore, the information contained in the infrastructure requirements tables may be subject to change.

Infrastructure requirements relating to the Masterplan Zones are identified under Policy I1 of the LDP and the LDP Action Programme. The precise need for infrastructure and the process for delivery will need to be discussed and agreed with Scottish Water at the earliest opportunity in the preparation of proposals for development. Masterplans and planning applications for development will need to reflect the infrastructure requirements identified as well as the means of delivering such infrastructure. Any financial contributions will need to be agreed with the Council, in consultation with Scottish Water, before an application can be determined.

The Council is currently exploring a mechanism for securing developer contributions towards the cumulative impact of development on water infrastructure and further details will be published when they are available.

Health

Infrastructure requirements have been calculated with NHS Grampian on the basis of national health standards and by estimating the likely number of new patients generated by each proposed development. Contributions will be calculated using nationally recognised space standards and build costs, based upon the population requirements for GP surgeries, dental chairs and community pharmacies.

Infrastructure requirements relating to the Masterplan Zones are identified under Policy I1 of the LDP and the LDP Action Programme. The precise need for infrastructure and the process for delivery will need to be discussed and agreed with NHS Grampian at the earliest opportunity in the preparation of proposals for development. Masterplans and planning applications for development will need to reflect the infrastructure requirements identified as well as the means of delivering such infrastructure. Any financial contributions will need to be agreed with the Council, in consultation with NHS Grampian, before an application can be determined.

Open Space and Green Space Network

In 2010, the City Council published its first Open Space Audit which has provided a detailed assessment of open spaces across the City, and has informed preparation of the Aberdeen Open Space Strategy, which provides a strategic framework for protecting, creating, connecting, and improving open spaces. This work has

provided the basis for determining open space standards and green infrastructure across the City.

Policy NE4 of the Local Development Plan sets out the open space requirements for residential development which states that 2.8 hectares of open space must be provided per 1000 people. This figure will be calculated using the number of bed spaces in the development. The categories of open space include Play space, Outdoor Sports Areas, Natural Greenspace and Green Corridors and Allotments.

The Open Space Supplementary Guidance and Open Space Audit will be used to identify the level of provision in the local area, using minimum quantity, quality and accessibility standards. Where the quantity standards have already been met within the surrounding area, it may be appropriate to make improvements to the quality of, and accessibility to, the existing provision.

It is important that open spaces are high quality and functional. If the size or viability of the development, such as brownfield development, means that the open space provided would be of limited function and usefulness, and where there is adequate quantity of provision in the surrounding area, contributions towards the improvement of quality and accessibility of surrounding open spaces may be appropriate instead. The level of contributions required will be based on cost estimates to be included in the emerging Supplementary Guidance on Open Space.

In addition, indoor and outdoor sports facilities which require works other than ground works for playing fields, for example, lighting, changing facilities, spectator areas, car and coach parking, attract contributions based on a cost per square metre basis for building works. The requirements are assessed and determined so as not to degrade the level of amenity already enjoyed by existing residents. Existing facilities are taken into account and adjustments to the overall rate will be made on a site by site basis.

All other types of development, including commercial, business and employment developments, will need to provide an appropriate level of open space in the context of the landscape setting and the nature and scale of development. Such provision will need to be agreed with the Council at the earliest opportunity in developing proposals for a site.

Green Space Network

The Open Space Strategy and Open Space Supplementary Guidance also include information on the rationale and delivery mechanisms for Green Space Network. Green Space Network is shown in the LDP Proposals Map as Policy NE1. Green Space Network is a recognised form of infrastructure; a strategic network of woodland and other habitats, active travel and recreation routes, open space and waterbodies, providing an enhanced setting for development, opportunities for adapting to climate change and a facility for the people living or working in the developments.

As stated in Policy NE1 of the Local Development Plan, development which has an impact on existing wildlife habitats, or connections between them, or other features of value to natural heritage, open space, landscape and recreation, must be mitigated either within or outside of the development. As Green Space Network defines the

strategic and priority green infrastructure, mitigation measures should be targeted towards the protection and enhancement of Green Space Network. Such mitigation measures could include the following:

- woodland expansion;
- the creation of a high quality landscape structure which supports the development plan spatial strategy;
- strategic routes for active travel and recreation;
- strategic habitat networks and habitat development projects;
- enhancement or creation of open space;
- the restoration of vacant and derelict land for green network purposes; and
- the integration of woodland, habitat, greenspace and access development with water catchment and coastal zone management.

Such provision will need to be agreed with the Council at the earliest opportunity in developing proposals for a site. The Open Space Strategy and Supplementary Guidance give more information on the rationale, priorities and delivery mechanisms of the Green Space Network.

Library Provision

Where there is deemed to be an impact on current provision from new development, the Council will seek contributions towards the creation of libraries. The guideline requirement for floor area in relation to population indicates that a building of at least 42 m² per 1,000 residents is required. As the population criteria are exceeded only by continued expansion of the settlement by developers the need for a contribution towards such facilities is therefore reasonable and justifiable. The Authority will plan for any contributions towards improvements to the service provision in the most effective manner.

Contributions are calculated assuming 2.3 persons per household, at a build cost of £1,175 per m², required by the population arising from the development.

Community Facilities

Where there is deemed to be an impact on current provision from new development, the Council will seek contributions towards the creation of community facilities. This is calculated on the requirement of 0.69 m² of community facility per dwelling unit as determined for small to medium size developments. This is to ensure that existing residents are not disadvantaged by an increase of usage from additional residents the proposed development would generate. The level of contribution is determined by the size of the existing facilities and current population served by the facilities together with the anticipated increase in the population served by the new development. In some instances there will be no contribution required if the facilities are of adequate size to cope with both existing and anticipated usage.

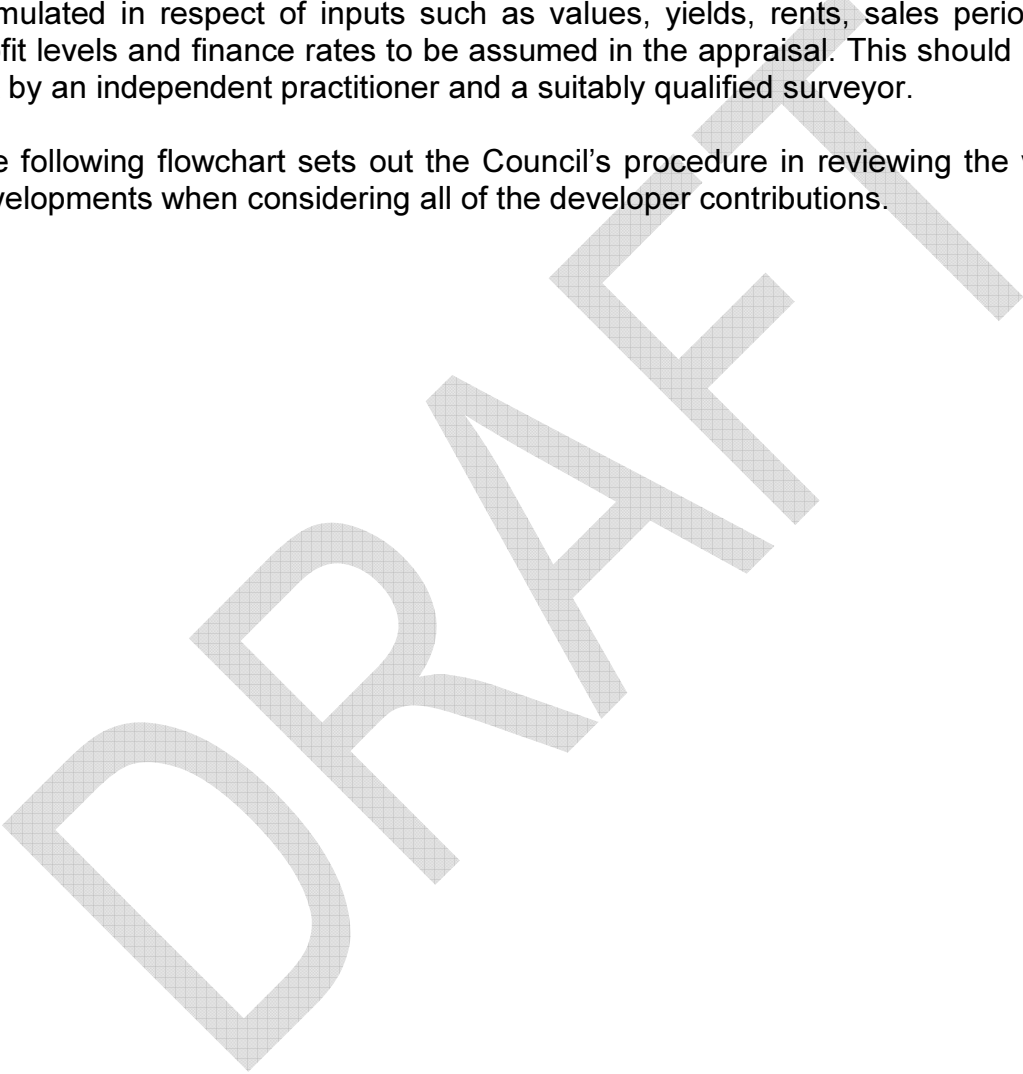
Contributions are calculated on the basis of the requirement for 0.69 m² of community facilities per household, using a build cost of £1,175 per m².

Development Viability

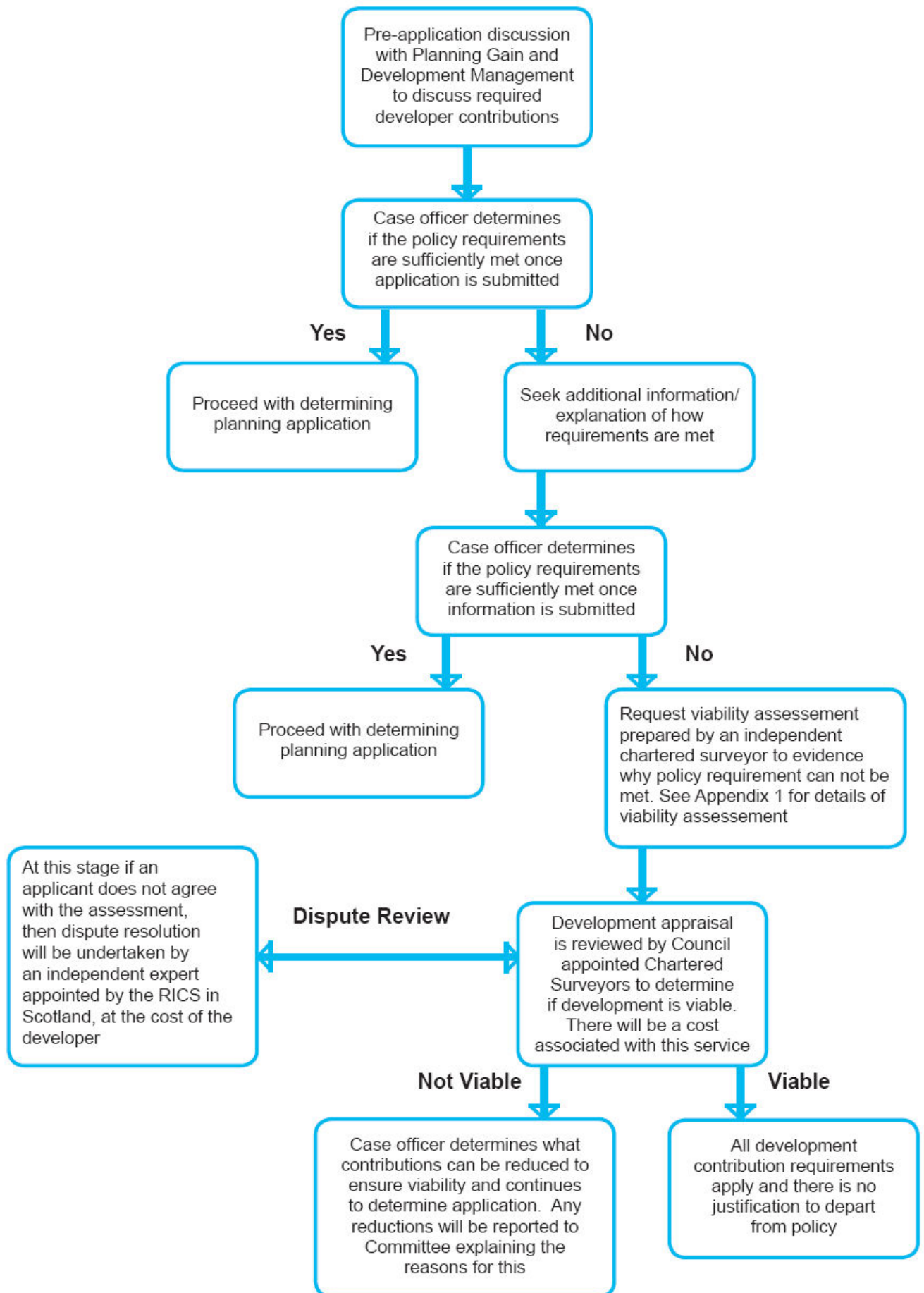
The consideration of financial viability is, in the application of planning policy and the negotiation of developer contributions, an essential element in the determination of planning applications. A certain degree of knowledge and understanding is needed by planners and decision-makers as to the viability implications of all of the requirements placed on development, and where difficulties arise in delivering all the requirements, independent expert viability input is required.

In these instances applications must be supported by viability assessments and It is important that these are supported by adequate comparable evidence. For this reason it is important that the appraisal is undertaken by a suitably qualified practitioner. This ensures that appropriate assumptions are adopted and judgment formulated in respect of inputs such as values, yields, rents, sales periods, costs, profit levels and finance rates to be assumed in the appraisal. This should be carried out by an independent practitioner and a suitably qualified surveyor.

The following flowchart sets out the Council's procedure in reviewing the viability of developments when considering all of the developer contributions.



Development Viability Flowchart



Appendix 1

Indicative Outline of what to include in a Viability Assessment

Proposed scheme details

- Floor areas:
 - commercial: gross internal area (GIA) and net internal area (NIA)
 - residential: GIA and NSA
- Residential unit numbers and habitable rooms including the split between private and affordable tenures

Gross development value (GDV)

- Any existing income that will continue to be received over the development period
- Anticipated residential sales values and ground rents (and supporting evidence including deductions for incentives)
- Anticipated rental values and supporting evidence
- Yields for the commercial elements of the scheme and supporting evidence
- Details of likely incentives, rent-free periods, voids
- Anticipated sales rates (per month)
- Anticipated grant funding for affordable housing
- Anticipated value of affordable units (with supporting evidence/explanation of how these have been valued and assumptions)
- Deductions from commercial GDV to reach NDC (Stamp Duty Land Tax (SDLT), agents, legal + VAT).

Costs

- Expected build cost (if required, a full QS cost report also showing how costs have been estimated)
- Demolition costs
- Historic costs (as reasonable and appropriate)
- Site preparation costs
- Vacant possession costs
- Planning costs
- Construction timescales, programme and phasing
- Any anticipated abnormal costs
- Rights of light payments / party walls / over sailing rights
- Details of expected funding and finance rates
- Professional fees, including:
 - architect
 - quantity surveyor
 - structural engineer
 - mechanical/electrical engineer
 - project manager
 - letting agent fee
 - letting legal fee
 - Site Value (see Section 3 of the guidance)
- Other costs

Additional details for projection based viability assessments

- Expected sales growth

- Expected rental growth
- Expected cost inflation
- Credit rate

Development programme

- Pre-build
- Construction period
- Marketing period

Viability cashflow

- Income/value/capital receipt
- Costs
- Phasing (where appropriate)

Benchmark viability proxies

- Profit on cost
- Profit on value
- Development yield
- Internal rate of return (IRR)

Planning application details

- Plans/sections/elevations (as relevant)
- Design and access statement

Sensitivity Analysis

- Two way sensitivity analysis
- Scenario analysis
- Simulation analysis

Accompanying Report (basic outline)

- Executive summary
- Contents outline
- Introduction and background
- Description of site location
- Planning policy context
- Description of scheme
- Market information summary
- Build cost and programme
- Methodology and approach
- Outputs and results
- Sensitivity analysis
- Concluding Statement

Appendix 2

Supplementary Guidance

Topic: Children's Nurseries and Sports Facilities

May 2012

PRIVATE CHILDREN'S NURSERIES

Children's nurseries fall within Class 10 of the Town and Country Planning (Use Classes) (Scotland) Order and planning permission is required for the change of use of any building falling outside this use class to form a children's nursery. The term includes day nurseries, crèches, and pre-school playgroups and can be defined as a place where a number of children under 5 years of age are brought together for part or all of a working day on a regular basis and where provision is made for their care, recreation and in some cases meals

Considerations

The main issues to consider:

- The likely effect on the character of the area, especially where the building would be completely removed from residential use and where it relates to conservation areas and listed buildings.
- Car parking and traffic congestion caused by both staff and parents dropping off and collecting children.
- Noise from children, both internally and externally.

Residential Areas

In assessing whether or not a nursery would cause any conflict with, or nuisance to the character of an area and the existing level of residential amenity, the following would normally be considered:

- Planning permission for a day nursery will not normally be granted where the number of children would create an unreasonable noise nuisance either from within the premises or outside in any play space, particularly within a residential area. For the foregoing reason the type / location of property concerned will be important. Flats and terraced houses, in particular, are generally not considered suitable.
- The proposed opening hours and days of operation of the nursery would be expected to be Monday to Friday. Opening hours outwith the period 8 am to 6 pm are unlikely to be acceptable.
- In terms of the number of children the applicant proposes to supervise. A figure of 20 children will normally be considered the maximum for day nurseries in residential areas. Large buildings with extensive gardens on roads characterised by hotels, educational establishments and other commercial uses rather than houses, may be considered more appropriate locations for day nurseries accommodating more than 20 children.
- There should be adequate provision of suitable outdoor play space. Normally a minimum of 100 sq metres for 12 children or less would be sought with this rising to 150 sq metres for premises with more than 12 children attending. In terms of protecting the residential amenity of adjoining properties, playspace should be located away from the habitable rooms of adjoining properties and be adequately buffered through for

instance the use of landscaping or screened fencing to protect from noise and overlooking.

- The effect of any external / internal alterations proposed in relation to the change of use such as car parking, new signage. Car parking will not normally be acceptable in front gardens unless well screened by landscaping from the road. Window displays can create a cumulative visual effect and can conflict with the character of the area.
- Access and Car Parking. Vehicular and pedestrian access should be safe and direct. The access and nursery should normally be at ground floor level. There should be appropriate car parking on site in accordance with the Council's adopted standards and the parking layout should enable visiting cars to enter and leave the site in a forward gear. Establishments must provide adequate facilities on site to enable the safe dropping off/picking of vehicle passengers. Where no such facility can be provided, use of the public road will only be acceptable following an assessment of road/ parking capacity and road safety, which concludes the effects of the proposal are not significant.
- A concentration of day nurseries is likely to be resisted where it results in an unacceptable loss of amenity, change the character of the area and creates parking or traffic problems particularly within residential areas

Non-Residential Areas

Industrial Areas

- On-site work place nurseries within existing buildings, which are solely for use by the employees of that firm or establishment, are regarded as ancillary to the main use and do not require planning permission.
- New nurseries should not normally be considered favourably within Policy BI1 or BI2 areas designated within the Local Development Plan unless the applicants can satisfactorily demonstrate that:
 - (a) an adequate level of amenity could be created for the children
 - (b) The nursery would not be on a prime industrial site and would not threaten the Council's industrial land supply and or the aims of the BI1 and BI2 policies.

Conditions

It may be considered appropriate to attach certain conditions. These may relate to:

- Opening hours
- Car Parking and / or pick up / drop off facilities
- Restricting the number of children
- The requirement for a noise impact assessment and noise mitigation
- Restricting the use to a nursery only

Planning applications should be accompanied by the following information:

- detailed plans showing the proposed use of floor space within the building together with the amount of amenity space to be made available, on-site parking provision and treatment of all boundaries;
- an indication of the anticipated hours and days of use together with a justification for late or early hours and opening on weekends, where the premises are in a residential area;
- details of the number of employees (both part time and full time), and;
- the number of children of different age groups to be cared for.

Childminding

Childminding is the most common form of day care facility and is normally carried out in existing dwellings. Registered childminders working from home are unlikely to require planning approval. However, it is best to check with the planning service as there may be a requirement for planning permission depending on the type, size, location, scale and nature of the use, particularly the number of children.

SPORTS FACILITIES

Regard should be taken of environmental and amenity considerations in dealing with proposals for sports facilities. Where green space is being developed, an alternative area should be made available in or immediately adjacent to the same community and should provide similar or improved benefits to the replaced area in line with the Local Development Plan policy on Urban Green Space.

SPORTS FACILITIES

Sport and recreational facilities will be acceptable where it can be demonstrated that:-

- 1.** They are not detrimental to the natural or built environment or to residential amenity;
- 2.** Locations are accessible to the catchment population, giving priority to walking, cycling and public transport;
- 3.** Public access arrangements can be maintained, enhanced or, where appropriate, provided in a convenient location in the vicinity of the development;
- 4.** The impact of floodlighting and appearance of any associated structures would not adversely affect the amenity of nearby residential properties or the character of the area; and
- 5.** They meet the objectives of Aberdeen's Playing Pitch Strategy.

Existing sport and recreation facilities shall be retained unless they are being replaced by an improved facility.

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Appendix 3

Supplementary Guidance

Topic: Bats and Development

May 2012



Contents

1.	Introduction	1
2.	General Information about Bats	1
3.	Bats and Legislation	1
4.	Bats and Licensing	3
5.	Bats and Land Use Planning	4
6.	Identifying Bat Roosts	6
7.	Establishing if a Survey is Required	8
8.	Survey Standards	10
9.	Minimum Survey Details	12
10.	Once a Survey is Complete	14
11.	Conditions or Obligations	14
12.	What You Can Do To Help Bats	14
13.	Further Information	15
14.	Other Useful Contacts	15
15.	Acronyms	15

1. Introduction

This Supplementary Guidance provides information that will be needed for the consideration of the effects of planning and development on bats.

The detail outlined includes information about bats, how to identify bat roosts, the types of developments which usually require bat surveys, what information is required in the survey, and what happens once the survey is complete.

If a bat survey is required, the best time for carrying out surveys is between mid-May and mid-August. Planning this in advance and including the survey with the application can prevent delays in the planning process.

2. General Information about Bats

As the main predator of night-flying insects, bats are of major ecological importance. Bat populations are considered to be a good indicator of the broad state of wildlife and environmental quality due to their sensitivity to pressures experienced by other species.

They are small animals that roost in a variety of places and can be found in colonies, small groups or singly.

They do not usually damage property.

Bats require many different roosts depending on different conditions at different times of the year.

There are 17 resident bat species found in the UK, where 9 are found in Scotland.

The most common species which can be found in Aberdeen include the Common and Soprano Pipistrelle bats, and less frequently are the Brown Long-eared and Daubentons bats. Other species may be present and changing weather patterns may result in more species being discovered in north-east Scotland.

Bats have declined significantly. This is mainly due to the loss of suitable roost and feeding sites, reduced insect prey as a result of pesticide use, and mortality due to the use of highly toxic timber treatments in house roosts. Many species of bats are either threatened or endangered.

3. Bats and Legislation

Bats are European Protected Species (EPS) and are protected by European, UK and Scottish Law. The main piece of the legislation in the UK for the protection of bats is the **Conservation (Natural Habitats, &c.) Regulations 1994 (as amended)**. These Regulations transpose the requirements of the EU Habitats Directive (92/43/EEC) and aim to protect and maintain the conservation status of EPS in Scotland. The following is a brief summary of the legislation.

Under these Regulations it is an offence to either deliberately or recklessly: -

- Capture, injure or kill a wild bat;
- Harass a wild bat or group of bats;
- Disturb a wild bat in a structure or place (roost) it uses for shelter or protection;
- Disturb a wild bat while it is rearing or otherwise caring for its young (maternity roost);
- Obstruct access to a bat roost or otherwise deny the animal use of the roost;
- Disturb a wild bat in a manner that is, or in circumstances which are, likely to significantly affect the local distribution or abundance of that species; and
- Disturb a bat in a manner that is, or in circumstances which are, likely to impair its ability to survive, breed or reproduce, or rear or otherwise care for its young.

The following is also an offence: -

- Damage or destroy a bat breeding site or resting place (roost).

A roost is any structure or place used for shelter or protection. As bats tend to return to the same roost every year, roosts are protected regardless if bats are present or not.

4. Bats and Licensing

Scottish Natural Heritage (SNH) - In some circumstances, actions that are otherwise an offence can be carried out under a licence. SNH has a power to grant licences for scientific, educational or conservation purposes including surveys.

Further information on licensing from SNH can be found at: -
<http://www.snh.gov.uk/protecting-scotlands-nature/species-licensing/mammal-licensing/bats-and-licensing/>

Where an impact on bats cannot be avoided, SNH will only grant a licence if the proposal satisfies all 3 of the following tests:-

1. That there is a licensable purpose for which licenses can be granted. A licence may be granted 'to preserve public health or public safety or for other imperative reasons of overriding public interest including those of a social or economic nature and beneficial consequences of primary importance for the environment'.

If this is satisfied 2 further tests must be satisfied: -

2. That there is no satisfactory alternative to the granting of a licence; and
3. That the action authorised will not be detrimental to the maintenance of the population of the EPS concerned at a favourable conservation status in their natural range.

SNH will not issue a licence unless the 3 tests, which ensure that the bat population is not unduly threatened, have been met.

Please note that licences will not be granted until any planning position has been resolved.

5. Bats and Land Use Planning

The presence of a bat roost, even when bats are apparently absent, is a material consideration when a planning authority is considering any development proposal.

Developers should consider the presence of bats and roosts at the beginning of the planning stage. Contact your Local Planning Office for more guidance on bats at the start of planning your development (see section 13 'Further Information' on page 15).

Scottish Planning Policy states that in relation to the presence or potential presence of EPS, this presence rarely imposes an absolute block on development, however, mitigation measures are often required which may affect the layout, design and timing of works.

Planning permission must not be granted for development that would likely have an adverse effect on an EPS unless the Local Planning Authority is satisfied that 3 tests have been met (see section 4 'Bats and Licensing' on page 3).

Applicants should submit supporting evidence for any development that meets these 3 tests, demonstrating both the need for the development and that a full range of possible alternative courses of action have been properly examined and none found to acceptably meet the need identified.

The **Local Development Plan** and **Structure Plan** (or **Strategic Development Plan**) must be referred to as they contain policies to protect priority habitats and species.

The **UK Biodiversity Action Plan** notes all bat species as 'Species of Conservation Concern', while seven are 'Priority Species'. The Priority Species are the Barbastelle; Bechstein's; Noctule; Soprano Pipistrelle; Brown Long-eared; Greater Horseshoe; and Lesser Horseshoe.

There is a **North East Scotland Local Biodiversity Action Plan** for the Daubentons bat.

Table 1 shows the list of bats species currently found in Aberdeen and their protection status.

Table 1: Bat Species in Aberdeen and their Protection Status.

BAT SPECIES	EPS	SCC	PS	NELBAP
Brown Long-eared	●	●	●	
Common Pipistrelle	●	●		
Daubentons	●	●		●
Soprano Pipistrelle	●	●	●	
EPS: European Protected Species SCC: Species of Conservation Concern PS: Priority Species NELBAP: North East Scotland Local Biodiversity Action Plan				

When an application for development is received without a bat survey and it is suspected that a bat roost is present, **the Local Planning Authority should request a bat survey to establish the impacts on bats before the planning application is determined.**

A bat survey cannot be included as a condition of Planning Approval but must be completed prior to granting planning permission. This is a requirement of the EPS legislation.

The Local Planning Authority can refuse planning permission under its duty to protect EPS.

6. Identifying Bat Roosts

The types of places that bats roost include buildings such as houses, churches and schools. Other structures include bridges; caves; mines; cellars; tunnels; plus, holes and crevices in mature trees.

The main feature to look for is the presence of droppings. They are dark brown or black and are between 4 and 8mm in length; they resemble the size and shape of a grain of rice. Unlike mouse droppings, bat droppings crumble into a fine powder when pressed between the fingers.

Bat droppings may be found on the outside walls of buildings below holes where bats enter and leave their roosts. Piles of droppings can also be found below ridge boards, hips and around chimneys or gable ends.

The quantity of droppings should not be relied upon to establish the number of roosting bats. Bats can be tucked away in crevices, therefore, only a small amount of droppings may be seen or none visible at all.

As well as droppings, a specific odour and discarded moth wings can also indicate the presence of bats. A polished or clean surface where light enters along with the absence of cobwebs can also indicate an area regularly used by bats.

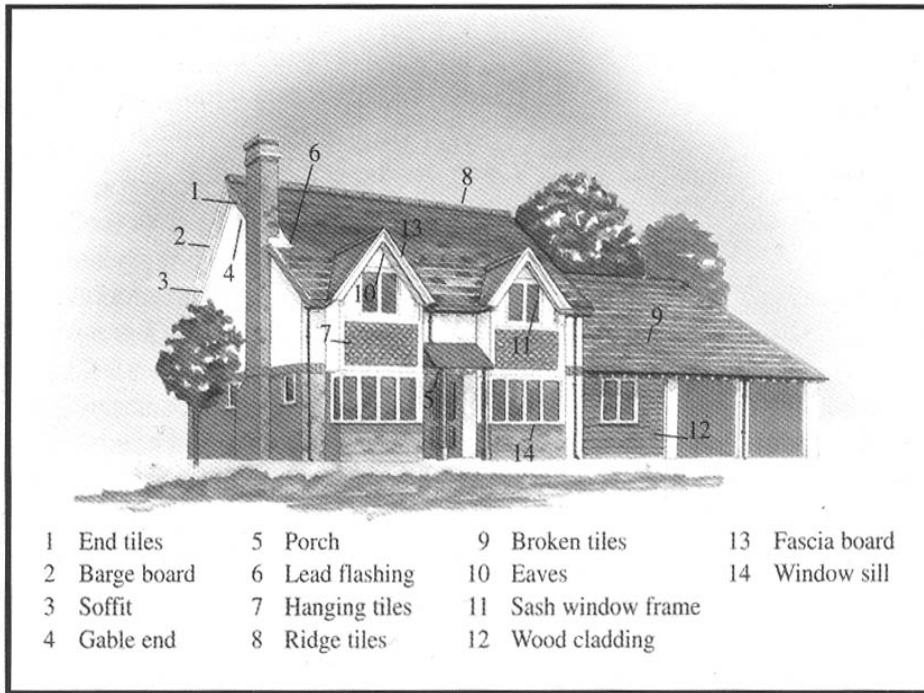
Summer roosts, frequently in buildings, are generally close to good feeding habitat and rich in insects. Good habitats include grassland, wetland, rivers and woodland.

With few insects around in winter, bats hibernate but wake occasionally to feed on milder evenings. Winter hibernation sites include caves, cellars, ice-houses, tunnels, bridges and other places which provide cool, stable conditions. Hibernating bats are very vulnerable to disturbance.

Tree holes can be used in both summer and winter.

The following Diagram 1 indicates areas in a house where bats may be found.

Diagram 1: Areas in Houses Where Bats May Be Found



Photograph Source: Scottish Natural Heritage (2003)

Wall cavities and flat roofs are also popular with Pipistrelle bats in Aberdeen.

Bats can be found in any kind of building old or new, and it is important to note that almost any roof or building, such as flat or pitched roofs, wall cavities and window frames, is a potential bat roost.

7. Establishing if a Survey is Required

As previously indicated, bats use a wide range of buildings, structures and trees as roosts and have different requirements at various times of the year (please refer back to section 6 'Identifying Bat Roosts' including Diagram 1 on page 7). **It is therefore difficult to predict accurately where bats are likely to be found.**

Where planning permission is sought, a survey **will always be required** for any structural work or demolition of any building with a known bat roost or where bats are known to be present in the building. (This may be highlighted by a North East Scotland Biological Records Centre (NESBReC) data search or as notified by any competent authority (SNH) or other).

Where it is not known if bats or a bat roost is present, development or demolition including alterations or extensions that would affect the types of buildings listed below may require a bat survey: -

- Any constantly heated building such as residential homes, hospitals, schools and swimming pools.
- Traditional buildings including churches and castles, with complex roof spaces.
- Stone and slate buildings including farmhouses; steadings; estate lodges; gatehouses; mill buildings; and old school buildings with an intact or almost intact roof structure.
- Any building or structure close to freshwater and wetland habitats (such as rivers, burns, streams, ponds or wet grassland), woodland, hedgerows and/or lines of trees. This includes bridges and other structures over water features or wet ground.
- Underground and other structures such as tunnels, kilns, cellars, ice houses, or fortifications which provide stable winter temperatures can provide appropriate hibernation sites.

Buildings which are in a very exposed location by the sea with no suitable foraging habitat within 1km are unlikely to be used by bats and will not require a survey.

Other activities that may require a survey include proposed tree work (felling or lopping) and/or development affecting: -

- a. Old and veteran trees older than 100 years; and/or
- b. Trees with obvious holes, cracks or cavities; and/or
- c. Trees with a girth greater than 1m at chest height.

Young plantations of conifers are not likely to have bat roosts. Trees are more likely to be used by bats if they are linked by other trees or hedgerows to woodland or water bodies.

As a guide, the Council's Geographical Information System (GIS) has an overlay which shows where bats have previously been noted in the City. Any proposed development within or adjacent to these areas should be surveyed.

Please note, it should not be presumed that bats will not be present outside these areas. Where development is proposed outside these areas, it will be up to the Local Planning Authority to determine if a survey is required or not.

If there are reports that bats have been seen flying over a site where development is proposed, this may mean that they are foraging in the area and will have a roost nearby. It does not necessarily mean that there will be a roost on the development site.

It is recommended that for outside known sites uses by bats, sightings can be used together with information on the type of building to be altered or demolished along with suitable habitat in the vicinity to determine whether a bat survey should be carried out.

Any sightings received from a member of the public should be passed to NESBReC to enable them to keep records up to date.

Remember, bats can be found in any structure and/or building both old and new if it is in the correct environment!

8. Survey Standards

Providing an adequate survey will save time in processing a planning application where bats may be affected.

Surveys must be carried out by a surveyor that is suitably experienced.

Surveys must be detailed, complete and the correct methodology must be used.

The minimum standard for bat surveys must be met, and any surveys which do not meet them, will not be accepted (full survey details are included in section 9 'Minimum Survey Details' page 12).

The following literature can also provide relevant survey standards: -

- a. Bat mitigation Guidelines – English Nature; and
- b. Bat Surveys Good Practice Guidelines – Bat Conservation Trust.

Surveys must be undertaken at the correct time of year. (See Diagram 1: Bat Activity Calendar on page 11.)

For householder applications and other small scale developments such as steading conversions, winter surveys may be acceptable to rule out the presence of bats but these will only be accepted where all relevant parts of the building can be adequately inspected.

If winter surveys (outside mid-May to mid-August) find evidence of use by bats, or where they are inconclusive, a further survey during the summer months will be required to establish the extent of use and identify appropriate mitigation.

The survey and assessment should cover all phases of a phased development.

The Local Planning Authority will also liaise with NESBReC to obtain records of bat sighting and roost sites.

The following Diagram 1 presents the different activities by bats and at what time of year these activities occur. **Please note that unusual weather patterns can shift normal timings.**

Diagram 1: Bat Activity Calendar.

MARCH	APRIL	MAY	JUNE
Signs of limited activity: small numbers feeding on warmer nights.	Active and hungry. Become torpid ¹ again when cold.	Fully active. Females search for suitable nursery sites.	Young are born.
JULY	AUGUST	SEPTEMBER	OCTOBER
Mothers suckle young. Some young almost full-size; others still very small.	Females desert nursery sites and seek males. Juveniles begin catching insects.	Mating takes places. Fat begins to build up ready for winter.	More mating. Seeking suitable hibernation sites. Periods of torpor.
NOVEMBER	DECEMBER	JANUARY	FEBRUARY
Bats begin hibernation, becoming torpid for longer periods.	Hibernating.	Hibernating. Using stored fat as fuel.	Hibernating. Little fat left.

Mid May through to Mid August is the best time to carry out surveys.

¹ Torpid is when the body temperature lowers and the heart rate slows.

9. Minimum Survey Details

All surveys submitted for development proposals should include the following: -

1. Objectives of the survey.
2. Time and date of the survey, and who carried the survey out.
3. Brief descriptions of weather conditions at the time of the survey.
4. Description of the proposed works.
5. Sources of pre-existing information such as records from NESBReC or the National Biodiversity Network (NBN) together with local sightings of bats.
6. Description of the buildings (including type of structure and materials) and/or trees being surveyed and their suitability as a bat roost for all locally recorded species of bat.
7. Habitat description of the site and surrounding area for context. This should include information on exposure of the site, proximity to water courses and water features, trees/hedgerows/woodland or other semi-natural habitat.
8. Methods of survey i.e. dawn and dusk emergence survey or daytime inspection of building. Justification should be provided for the method of survey used and details of any equipment used.
9. Results of survey including sufficient evidence to justify conclusions in point 8 above. Results should include: -
 - Species present and approximate numbers;
 - Details found of signs of usage by bats; and
 - How habitats or features present are used by bats and an indication of level of use.
10. Interpretation and evaluation. These details should include: -
 - Presence or absence;
 - Constraints and limitations on survey. This should include factors influencing the survey results such as temperature and weather, and any limitations on accessibility to areas of the building. Are any areas of the survey inconclusive, and if so, what is the worst case scenario?
 - Assessment of usage by bats including sex of bats present, type of roosts i.e. winter site or maternity roost, and approximate size of roost; and
 - Site status assessment – importance of roost to the local bat species population.
11. Impact assessment either at the time of development or long term. In order to assess this accurately, adequate information on the proposed development will have to be made available to the surveyor. If bats are present, a summary of impacts should be provided including details of type, magnitude and duration of long term and short term impact. This should consider impact at site level in a wider context.

12. Mitigation and compensation – essential where bats are present and will be affected by the development. These details should include: -

- Mitigation strategy – overview of how the impacts will be addressed with justification for timings of works if this is to be used to avoid disturbance to bats;
- Roost creation or restoration and/or enhancement;
- Exclusion – timing and methods;
- Post development site safeguard and monitoring;
- Work schedule with phasing; and
- Relevant maps or plans or diagrams.

13. References.

14. Photographs and maps of key features of structure and surrounding habitat.

15. Qualifications and experience of surveyor including relevant licences.

10. Once a Survey is Complete

When a detailed report is submitted and includes evidence of bats, the Local Planning Authority is likely to consult with SNH for specialist advice on the significance of impacts on the species and the likely effectiveness of any mitigation proposed.

The Council will then take the following action: -

- If no evidence of bats is discovered **AND** the survey has been completed in accordance with the guidelines above, no further action will be required.
- If a survey has been conducted during the winter (outside mid-May to mid-August) and has reported suitable habitat for evidence of bats and is inconclusive, a further summer survey will probably be necessary **before** planning approval can be granted.
- If bats are present, appropriate action/mitigation will need to be discussed and agreed with the Local Planning Authority prior to the granting of planning permission.
- Where a direct impact on bats cannot be avoided, the developer will be advised to apply for a licence from SNH (see section 4 'Bats and Licensing' page 3).

11. Conditions or Obligations

Conditions or obligations can be imposed to highlight the applicant's legal responsibilities and give clear guidance on how to give protection to bats. Examples, which will depend on the individual situation, can include: -

- Restrictions on the timings when work can take place if a bat roost is present;
- Use of building materials such as bat bricks or special tiles which provide access points for bats;
- Management prescriptions to be agreed for habitats adjoining the development, e.g. grassland, scrub, woodland, hedgerows; and
- The creation of feeding habitats adjoining the development, e.g. grassland, meadows, large ponds.

12. What You Can Do To Help Bats

You can make your development bat friendly by incorporating provision for roosting and feeding. Bat boxes, placed on trees or buildings can attract bats. Bat bricks and bat slates into your house will allow access into the roof space. Landscaped gardens can be more bat friendly by including hedges, trees, ponds and night-scented flowers to attract insects for bats to feed on. Demonstrating that this has been considered could form an important element of your planning application. More information can be obtained from the Bat Conservation Trust (see section 14 'Other Useful Contacts' page 15).

Remember, even if you do not require planning permission for any works on a building, or a survey has not been conducted, if you at least suspect the presence of bats, you must stop works immediately and seek the advice of SNH.

13. Further Information

Local Planning Office

Aberdeen City Council
Business Hub 4
Ground Floor North
Marischal College
Broad Street
ABERDEEN, AB10 1AB

Tel: 01224 523470

Email: pi@aberdeencity.gov.uk

Web: www.aberdeencity.gov.uk/planning

14. Other Useful Contacts

Scottish Natural Heritage

Tel: 01224 266500

Email: licensing@snh.gov.uk

Web: www.snh.gov.uk/protecting-scotlands-nature/species-licensing/

Bat Conservation Trust

Tel: 0845 1300 228

Email: enquiries@bats.org.uk

Web: www.bats.org.uk

North East Scotland Biological Records Centre (NESBReC)

Tel: 01224 273633

Email: nesbrec@aberdeenshire.gov.uk

Web: www.nesbrec.org.uk

National Biodiversity Network

Tel: 0115 959 6433

Email: support@nbn.org.uk

Web: www.nbn.org.uk

15. Acronyms

EEC European Economic Community

EPS European Protected Species

EU European Union

GIS Geographical Information System

NBN National Biodiversity Network

NELBAP North East Scotland Local Biodiversity Action Plan

NESBReC North East Scotland Biological Records Centre

PS Priority Species

SCC Species of Conservation Concern

SNH Scottish Natural Heritage

[Back cover]

Appendix 4
Respondents, Representations and Officers Responses

Respondent	Summary of comment	Response to comment
Infrastructure & Developer Contributions Manual		
Scottish Water (141)	<p>Supportive of the general approach that the Supplementary Guidance has taken to Water and Drainage. However clarity is required on whether the financial contributions associated with water and drainage that are referred to relate to strategic pieces of infrastructure that the Council would be looking to co-ordinate across several developers. Scottish Water require all upgrades to Part 3 Assets be paid for and delivered up-front by the developer, although claim some of these costs can be claimed as part of their Reasonable Cost Contributions (under the Provision of Water and Sewerage Services (Reasonable Cost) (Scotland) Regulations 2006).</p>	<p>The Council recognises that there needs to be clear arrangements in place for the planning and delivery of infrastructure for water supply and waste water disposal. Scottish Water played a crucial role in identifying infrastructure requirements for water through the plan preparation process. The Council intends to continue this by working with Scottish Water and other stakeholders to refine the evidence base for infrastructure and developer contributions.</p> <p>The Manual has been updated since the first publication in order to make the position clearer that the cost and programming of work associated with Part 4 strategic infrastructure is the responsibility of Scottish Water. All other Part infrastructure (1, 2 and 3) are the responsibility of the developer, where financial contributions for Parts 2 and 3 infrastructure can be claimed back from Scottish Water if applicable. This again is done as part of the Reasonable Cost Contributions (Reasonable Cost) (Scotland) Regulations 2006.</p>
Kincorth/Leggart Community Council (329)	<p>How long will it take to gain sufficient funds to build a primary and a secondary school for site OP77? Given that it is unlikely that all 1650 houses will be built at one time. Previous experience has shown that schools and other facilities are not built for quite some time after development has begun.</p> <p>The Supplementary Guidance mentions health, however, the only mention in the LDP is for an extension to the Cove Health Practice. A</p>	<p>In light of the comments raised by Scottish Water we would propose to add text to the Supplementary Guidance which explains the process as described above.</p> <p>The total number of houses will not be built all at one time. Long term phasing of sites is set out in the Local Development Plan and discussions will be held between the Council and developers regarding the planned year to year development rates.</p> <p>As mentioned previously, whilst the Council recognises there are challenges to delivering infrastructure in the current financial climate, it will be important for the Council to work closely with the development industry to find solutions to delivering development and the necessary infrastructure, such as education provision, to mitigate any impacts. The Council can utilise mechanisms to spread costs through staged payments</p>

Respondent	Summary of comment	Response to comment
	<p>timetable must be set for the construction of the schools. A health centre would also be required within a development of this size. The section on Cumulative Transport Infrastructure makes no reference to the roads that would be required to prevent future problems at the Bridge of Dee that are currently under discussion at Nestrans.</p>	<p>where appropriate, and we are exploring opportunities for up-front funding. In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes. Any infrastructure or financial contributions sought will be proportionate to the impact of development and also take account of the capacity of existing services and infrastructure.</p> <p>Masterplans will need to demonstrate how supporting infrastructure will be delivered, along with the phases of development proposed, and provide appropriate evidence to support any request by a developer to diverge from the infrastructure identified. For example, this could be as a result of a change in the scale and/or impact of development proposed from the allocation in the LDP. Subsequent planning applications will need to reflect the agreed masterplan and mitigation measures, and developer contributions will be finalised through a Planning Agreement. This will ensure that infrastructure requirements and developer contributions are necessary and proportionate to mitigate the impact of development. It should also ensure that infrastructure provision and developer contributions do not unduly restrict the implementation of development proposals or affect the viability of development, whilst ensuring that new development will be accompanied by an appropriate level of services to support new communities.</p>
<p>Scottish Environment Protection Agency (408)</p>	<p>Supportive of the recognition in the Local Development Plan of the need to consider delivery of infrastructure at an early stage.</p> <p>Supportive of the approach taken by the Supplementary Guidance of seeking contributions for the Air Quality Action Plan. This approach has been successful in other local authority areas and it will undoubtedly</p>	<p>The comments supporting the approach to contributions towards Air Quality Action Plan are noted and welcomed.</p>

Respondent	Summary of comment	Response to comment
<p>Emac on behalf of Scotia Homes (1189)</p>	<p>help to raise the profile of poor air quality.</p> <p>Supportive of the inclusion of Green Space Network (GSN) in Developer contributions. The SG states that the Open Space Strategy and Supplementary Guidance sets out priorities, rationale and delivery mechanism for Green Space Networks, however there is no such Supplementary Guidance on Open Space in the consultation documents.</p> <p>The Supplementary Guidance states that 'Masterplans will need to reflect the infrastructure requirements identified and should include a Delivery Statement setting out details of how the proposed development, and the accompanying infrastructure, will be delivered.</p> <p>Subsequently, actions relating to the delivery of infrastructure will need to be defined once the Masterplan and Delivery Statement have been agreed with the Council and any key agencies.'</p> <p>The delegation of responsibility to Masterplans, which have not yet been prepared, ignores the above advice that exact levels of contributions should be included in this Infrastructure and Developer Contributions Manual.</p>	<p>In preparing the LDP, the table of infrastructure requirements set out in Appendix 4 was presented in the main body of the text alongside Policy I1 in earlier drafts of the document. However, due to the size of the table and to keep the document as concise as possible it was relocated to an appendix with a cross-reference from Policy I1 to make this link clear. The inclusion of infrastructure requirements in the LDP complies with paragraph 23 of Circular 1 /2010 Planning Agreements which requires infrastructure requirements be set out within the LDP.</p> <p>Policy I1 also includes a cross-reference to the Infrastructure and Developer Contributions Manual, which is published as Supplementary Guidance to the LDP. In doing that the Council is seeking to provide as much information as possible on the likely contributions to be sought through Planning Agreements along with clear guidance on the methodology used to identify infrastructure requirements in the Proposed Plan.</p> <p>The reference to the Action Programme is included to provide details on infrastructure provision as required by paragraph 23 of Circular 1/2010 Planning Agreements. The Action Programme will be updated on an ongoing basis to reflect progress and highlight further actions required to implement each policy, project and development in the LDP.</p>

Respondent	Summary of comment	Response to comment
		<p>Appendix 2 does not relate directly to infrastructure requirements and is intended to provide a simple list of all opportunity sites in the LDP. In order to be as concise as possible and avoid repetition we do not consider there is a need for a linkage to Appendix 2.</p> <p>In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes.</p> <p>The masterplanning and planning application processes, and particularly technical assessments presented to the Council during these stages, will provide an opportunity for verification of impacts of development and mitigation measures proposed and will inform the negotiation of Planning Agreements. This approach will ensure that any infrastructure requirements and Planning Agreements are in scale and kind with the proposed development.</p> <p>Comments are welcomed and the Council will strive to continue with collaborative working approach.</p>
Sportscotland(1244)	<p>Welcomes early definition of the likely requirement for contributions which may be required and would be happy to work with the Council in further defining these.</p>	
Homes for Scotland (1442)	<p>This entire policy should be included within the main LDP. Paragraph 97 of Circular 1/2009 states: 'Matters that should be included in the LDP or SDP, and not in supplementary guidance include: items for which financial or other contributions, including affordable housing, will be sought, and the circumstances (locations, types of development) where they will be sought.'</p> <p>Homes for Scotland are concerned that the levels of developer contributions that are</p>	<p>The Infrastructure & Developer Contributions Manual defines the criteria and methodology for seeking contributions to mitigate the impact of development.</p> <p>In preparing the LDP, the table of infrastructure requirements set out in Appendix 4 was presented in the main body of the text alongside Policy I1 in earlier drafts of the document. However, due to the size of the table and to keep the document as concise as possible it was relocated to an appendix with a cross-reference from Policy I1 to make this link clear.</p> <p>Paragraphs 23 and 29 of Circular 1/2010 Planning Agreements state that infrastructure requirements should be set out within the LDP and</p>

Respondent	Summary of comment	Response to comment
	<p>emerging through the Supplementary Guidance (SG) are much higher than anticipated and fear that this will result in many schemes becoming unviable. The lists of contributions set out in the SG appear excessive, and do not relate in scale and kind to the proposed developments.</p> <p>PROPOSED MODIFICATION</p> <p>The following areas of greatest concern require justification by Aberdeen City Council that can clearly demonstrate that 'the requirements are directly related to the development proposal and the need for them arises from its implementation,' (Circular 1/2010, para 22). Unless this evidence is robust, Homes for Scotland requests that they be removed from the SG, until such time as it can be demonstrated that they are 'fair and proportionate'.</p> <p>Circular 1/2010, para 23 goes on to say: 'The Scottish Government wishes to improve the speed, efficiency and transparency of preparing and agreeing planning agreements. This means that:</p> <ul style="list-style-type: none"> - Supplementary guidance should specifically identify expected contributions' <p>1. Cumulative Transport Infrastructure The fact that Aberdeen City Council is 'currently exploring a mechanism for securing developer contributions towards cumulative</p>	<p>associated Action Programme. Policy I1 also states that "the level of provision will be commensurate to the scale and impact of development". Policy I1 includes a cross-reference to the Infrastructure and Developer Contributions Manual, which is published as Supplementary Guidance to the LDP. In compliance with Circular 1/2010 the LDP (at Appendix 4) and Action Programme list items for which contributions are likely to be sought and, alongside, the Supplementary Guidance is intended to provide as much information as possible on the contributions likely to be sought through conditions and/or Planning Agreements.</p> <p>The identification of infrastructure requirements related to new development in the Local Development Plan (LDP) has been based on the following principles:</p> <ul style="list-style-type: none"> - assessing the spare capacity of existing services/facilities/infrastructure and their ability to accommodate new development; - measuring the likely impact of new development on these services in different areas of the City; and - identifying where new infrastructure, or improvements to existing infrastructure, would be required to mitigate the impact of new development. <p>This work was carried out in partnership with colleagues from across Aberdeen City Council and key agencies involved in the planning and delivery of infrastructure, through the Aberdeen Future Infrastructure Requirements for Services Group. This process led to the identification of infrastructure specified in the LDP as being required to support particular developments across the City. The information gathered was published as a list of infrastructure requirements in the Proposed Plan, which served to share information with the development industry and other consultees as soon as possible in the plan preparation process.</p> <p>In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory</p>

Respondent	Summary of comment	Response to comment
	<p>transport infrastructure and further details will follow shortly' is contrary to para 23, above. Without this additional information, developers cannot know the full extent of their liabilities which may arise from their proposals, and cannot determine development viability. We therefore reserve the right to comment when further information is available.</p> <p>2. Schools The SG states that 'on larger sites, the applicant may be required to provide a school site. For primary provision this should be 3.25 hectares or thereby and for secondary schools this should be 10 hectares or thereby of serviced and where possible reasonably flat land'. We would expect the ultimate size to be concluded through the masterplan process and to be a size meeting the standards prevailing at that time. It is possible that with wide spread cost cutting throughout local authorities, the councils will not want the management responsibilities of 10 hectare sites for schools.</p> <p>3. Water The SG again states that 'The Council is currently exploring a mechanism for securing developer contributions towards the cumulative impact of development on water infrastructure and further details will when they are available'. We are understandably concerned about the lack of clarity provided by this statement and reserve the right to comment when further</p>	<p>agencies through the masterplanning and planning application processes. Any infrastructure or financial contributions sought will be proportionate to the impact of development and also take account of the capacity of existing services and infrastructure.</p> <p>Masterplans will need to demonstrate how supporting infrastructure will be delivered, along with the phases of development proposed, and provide appropriate evidence to support any request by a developer to diverge from the infrastructure identified. For example, this could be as a result of a change in the scale and/or impact of development proposed from the allocation in the LDP. Subsequent planning applications will need to reflect the agreed masterplan and mitigation measures, and developer contributions will be finalised through a Planning Agreement. This will ensure that infrastructure requirements and developer contributions are necessary and proportionate to mitigate the impact of development. It should also ensure that infrastructure provision and developer contributions do not unduly restrict the implementation of development proposals or affect the viability of development, whilst ensuring that new development will be accompanied by an appropriate level of services to support new communities.</p> <p>The principles outlined above are reflected in the formulae for developer contributions identified in the Infrastructure and Developer Contributions Manual.</p> <p>The Council will not be seeking contributions to resolve existing inadequacies.</p> <p>We have sought to find the most appropriate method for calculating costs for infrastructure provision. It has not been possible to set per-unit costs for infrastructure provision as this will vary depending on the circumstances of each site.</p>

Respondent	Summary of comment	Response to comment
	<p>information is available.</p> <p>4. Health The level of contributions required, as listed in Appendix 4, to supply NHS Grampian with additional GP surgeries, dental chairs and community pharmacies is totally unreasonable. Unless a transparent and independent audit of all existing health facilities clearly demonstrates a deficiency of facilities, and which can be directly related to proposed new development, our member companies will not be contributing to new ones. No other Local Authority in Scotland requires the housebuilding industry to contribute to the NHS.</p> <p>5. Open Space and Green Network The SG states that 'The level of contributions required will be based on cost estimates to be included in the emerging Supplementary Guidance on Open Space.' It goes on to say that 'The Open Space Strategy and Open Space Supplementary Guidance also include information on the rationale and delivery mechanisms for Green Space Network.' Unfortunately neither document is available, as they are yet to be finalised, and will not go to committee until January 2011 (after the consultation period for the LDP is passed). This is contrary to Circular 1/2010, para 23 and should be removed. Our members reserve the right to comment on them when they are available.</p>	<p>The Council's approach recognises that infrastructure is necessary to mitigate the impact of development and that without the necessary infrastructure development would not function effectively. It is therefore necessary to identify infrastructure requirements. By identifying these early in the plan preparation process, and applying those to specific sites and/or groups of sites, costs for infrastructure provision can be factored into land costs and shared more equitably, thereby avoiding any disproportionate cost to, or affecting the viability of, specific developments. The requirement to prepare Masterplans will provide developers with the opportunity to demonstrate how development and accompanying infrastructure will be delivered.</p> <p>The Council will continue working with the FIRS Group to help deliver supporting infrastructure according to the timing and phasing of development.</p> <p><u>Cumulative Transport Infrastructure</u> Provision of new transport infrastructure including services and facilities will always need to be negotiated between the Council and the Developer and informed by evidence presented in technical documents such as Transport Assessments as well as mitigation measures proposed. Where it is considered that development will generate the need for new transport infrastructure, contributions will be sought.</p> <p>The Council has carried out a significant level of work with transport colleagues and partners, as well as public transport operators, to establish the likely level of new infrastructure and services that will be required to support new developments. The transport schemes associated with new developments fall into two categories, Local Infrastructure and Strategic Infrastructure. Local transport projects are expected to be provided as part of new development and should be reflected in the associated Masterplan and subsequent planning applications. Strategic transport projects have been identified by a partnership group consisting of officers</p>

Respondent	Summary of comment	Response to comment
	<p>6. Library Provision and Community Facilities The SG states that 'the Council will seek contributions towards the creation of libraries', and 'new development will generate a requirement for the provision of community facilities'. We would require evidence that the existing provision would not be able to cope with increased demand created by the development of more housing. Are the existing facilities at capacity, or are there any threatened closures of existing libraries and community facilities? Before any member of Homes for Scotland agrees to any justifiable contribution, they would require evidence that they are 'not be(ing) used to resolve existing deficiencies in infrastructure provision'. It may well be the case that existing facilities require additional funding to stay open, and the creation of new libraries and community centres is not a cost effective way of ensuring this. Further consideration should be given to enhancement of existing services.</p>	<p>from Aberdeen City Council, Aberdeenshire Council, the Strategic Development Planning Authority, Transport Scotland and NESTRANS. Funds realised from this initiative will be managed and delivered by Nestrans.</p> <p>For the provision of local transport infrastructure, each site will be dealt with on a case by case basis and will need to be negotiated between the Council, Commercial operators and the Developer. Discussions should take place as early as possible in the Masterplanning and/or planning application process.</p> <p><u>Schools</u> Following an assessment of the capacity of existing schools the Council has identified where increased capacity is required in primary and secondary schools to support new development. The work that was carried out to determine if increased capacity is needed is based on the latest information available at the time of preparing the Proposed Plan and will be updated on an annual basis. Where developments are being brought forward for implementation through masterplan and planning applications, the Council will discuss with developers the exact level of contribution and scale of intervention required to mitigate any impacts. Such discussion will be informed by more detailed information submitted by the developer, to ensure that any contributions sought are directly related to the impact of the development. Indicative sizes for school sites are included as a guide, and this will also be determined through this process. The costs identified are based on build costs for previous school developments. The Council will seek the provision of facilities that can be shared between school and community use.</p> <p><u>Water</u></p> <p><u>Health</u> NHS Grampian has assessed the capacity and catchment areas of existing services and facilities, and have recommended where new or</p>

Respondent	Summary of comment	Response to comment
		<p>improved facilities are required. The Council considers that it is reasonable to highlight where new development would trigger the need for new services such as health facilities. The delivery of health facilities will need to be discussed and agreed with NHS Grampian at the masterplanning and planning application stages.</p> <p><u>Open Space</u> The Open Space Supplementary Guidance replaces the Open Space Development Guidelines for Greenfield Sites 2001 and was developed in parallel with the draft Open Space Strategy 2011. The two draft documents were consulted on for eight weeks from 20th May to 15th July 2011 and after taking into consideration the comments received the documents were adopted as Interim Supplementary Guidance on September 13th 2011.</p> <p><u>Library Provision and Community Facilities</u> Regarding Libraries and Community Facilities, amendments have been made to the revised Supplementary Guidance. Contributions will only be sought where there is deemed to be an impact on current provision from new development.</p>
Stewart Milne Homes (1464)	<p>The development industry cannot be accountable for delivering these facilities and improvements in their entirety, with fair and proportionate investment also needed by the Local Authority. The Development Plan must not be based on the assumption that developers can pay for the majority of new public infrastructure. Full assessment of existing capacities are required and contributions should only be sought where they relate in scale and kind to the development proposal.</p>	<p>We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning Agreements.</p> <p>In identifying infrastructure requirements and formulae for developer contributions we have taken account of the tests contained in Circular 1/2010.</p> <p>The need for new or improved infrastructure has been identified following detailed analysis of the capacity of existing services to cope with additional development, as well as the forecasted impact of developments</p>

Respondent	Summary of comment	Response to comment
	<p>The school sites should be developed sustainably as part of wider masterplanning process and will not in every instance need the same area of land take dependant on building design and overall pupil requirement and public authority requirements prevailing at the time.</p> <p>Object to the requirement to provide both facilities and the land on which to locate the facilities. This policy approach seeks to cover all eventualities leaving the Council the ability to alter the required developer contributions through SG at any time. The development industry requires clear structured policy and guidance at the outset in order that essential development contributions can be factored into the development finance of the site at the very early stage. Any significant changes to contributions later in the planning process has the ability to render development unviable leaving the Council unable to meet Structure Plan requirements.</p> <p>Remove specific reference to areas for schools within the SG (3.25ha for primary schools and 10ha for secondary schools).</p> <p>Remove developer requirement to provide NHS facilities (GP surgeries, dental chairs and community pharmacies).</p>	<p>based on the information available at that time. As such, these requirements are necessary in planning terms to make development acceptable, and to avoid any detrimental impact on services and infrastructure.</p> <p>Policy 11 also states that “the level of provision will be commensurate to the scale and impact of development”. The negotiation of infrastructure provision and developer contributions through the Masterplan and planning application process will ensure infrastructure provision does not unduly restrict the implementation of development proposals or affect the viability of development, whilst ensuring that new development will be accompanied by an appropriate level of services to support new communities.</p> <p>Following an assessment of the capacity of existing schools the Council has identified where increased capacity is required in primary and secondary schools to support new development. The work that was carried out to determine if increased capacity is needed is based on the latest information available at the time of preparing the Proposed Plan and will be updated on an annual basis. Where developments are being brought forward for implementation through masterplan and planning applications, the Council will discuss with developers the exact level of contribution and scale of intervention required to mitigate any impacts. Such discussion will be informed by more detailed information submitted by the developer, to ensure that any contributions sought are directly related to the impact of the development. Indicative sizes for school sites are included as a guide, and this will also be determined through this process. The costs identified are based on build costs for previous school developments. The Council will seek the provision of facilities that can be shared between school and community use.</p> <p>NHS Grampian has assessed the capacity and catchment areas of existing services and facilities, and have recommended where new or</p>

Respondent	Summary of comment	Response to comment
<p>GVA Grimley on behalf of Heron Property Group Ltd (1547)</p>	<p>Supportive of the principle of the Infrastructure and Developer Contributions Manual and ACC's approach to make the planning and delivery of infrastructure open, transparent and supported by a robust evidence base. The document sets out a range of infrastructure requirements relating to transport, core paths, schools, water, health, open space/green space network, library provision, community facilities and air quality. The method for working out the required contributions under each category however is inconsistent and is in places difficult to calculate with complex formulas being used. A unit cost would be a more consistent method for setting out the requirements and would assist developers in understanding the requirements upfront.</p> <p>The manual also makes no mention of phasing and funding arrangements which are critical to the viability and deliverability of development. Accordingly, the manual should make reference to the acceptability of staged payments in accordance with paragraph 20 of Circular 1/2010. This states: It is essential that planning authorities understand the implications of a planning agreement on the viability of development. Entering into an</p>	<p>improved facilities are required. The Council considers that it is reasonable to highlight where new development would trigger the need for new services such as health facilities. The delivery of health facilities will need to be discussed and agreed with NHS Grampian at the masterplanning and planning application stages.</p> <p>We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning Agreements.</p> <p>In identifying infrastructure requirements and formulae for developer contributions we have taken account of the tests contained in Circular 1/2010.</p> <p>Paragraph 3.3 of the Proposed Plan points towards the relevant references to infrastructure provision and developer contributions – Appendix 4 of the Proposed Plan, the Action Programme and the Infrastructure and Developer Contributions Manual.</p> <p>Whilst the Council recognises there are challenges to delivering infrastructure in the current financial climate, it will be important for the Council to work closely with the development industry to find solutions to delivering development and the necessary infrastructure to mitigate any impacts. The Council can utilise mechanisms to spread costs through staged payments where appropriate, and we are exploring opportunities for up-front funding.</p> <p>Policy 11 also states that “the level of provision will be commensurate to the scale and impact of development”. The negotiation of infrastructure provision and developer contributions through the Masterplan and planning application process will ensure infrastructure provision does not unduly restrict the implementation of development proposals or affect the viability of development, whilst ensuring that new development will be accompanied by an appropriate level of services to support new</p>

Respondent	Summary of comment	Response to comment
<p>Dundas and Wilson CS LLP on behalf of Asda Stores Ltd (1558)</p>	<p>agreement is likely to have financial consequences. Cash flow can be affected where substantial sums of money have to be paid before a development proceeds or at an early stage in the construction. Where a planning agreement requires financial contributions, staged payments in line with the construction programme can help avoid prejudicing the overall viability of a project.</p>	<p>communities.</p> <p>The need for new or improved infrastructure has been identified following detailed analysis of the capacity of existing services to cope with additional development, as well as the forecasted impact of developments based on the information available at that time. As such, these requirements are necessary in planning terms to make development acceptable, and to avoid any detrimental impact on services and infrastructure.</p> <p>The Council is reviewing its evidence base for services and infrastructure on an ongoing basis to ensure that there is sufficient infrastructure for seeking the provision of infrastructure or developer contributions through new development.</p>
<p>Dundas and Wilson CS LLP on behalf of Asda Stores Ltd (1558)</p>	<p>Supportive of the general principle of developer contributions as set out in the Supplementary Guidance provided that they are necessary and relevant to the proposed development. The principal means for securing developer contributions, identified in the Proposed Plan, is to be through planning conditions and/or Planning Agreements. It is, therefore, necessary that the basis for securing developer contributions is consistent set out in Scottish Government Circulars 4/1998 and 1/2010.</p> <p>Clarity is required as to whether the Manual provides criteria for infrastructure requirements and developer contributions for (i) all development; (ii) only development within Masterplan Zones; or (iii) all development except Masterplan Zones. This is currently</p>	<p>The draft Infrastructure and Developer Contributions Supplementary Guidance contained as much information as was available at the time it was prepared. The Council aims to provide greater clarity by re-consulting on a revised draft of the Infrastructure and Developer Contributions Manual in early 2012.</p> <p>In response to the representation querying which developments the SG is applicable to, All development sites will have to provide the infrastructure and developer contributions necessary to mitigate the impact of development. The Masterplan Zones correspond to the list of infrastructure requirements identified in Appendix 4 of the LDP and the Action Programme. The guidance set out in the SG provides. The Council has clarified this position on page 4 of the draft Supplementary Guidance.</p> <p>We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning</p>

Respondent	Summary of comment	Response to comment
	<p>unclear and should be clarified.</p> <p>The Manual is inadequate in other respects:</p> <ol style="list-style-type: none"> 1. Requirements for certain contributions are excessive and are not necessarily consequent upon or related to developments proposed. For example, contributions to core paths are not necessarily relevant to new retail development. This would not meet the requirements for either Circular 4/1998 or 1/2010. 2. Either insufficient or no information is provided for contributions for important infrastructure e.g. for transport or water supply. Without this information the Manual is of negligible use and should not be included as formal Supplementary Guidance. Consultation and subsequent inclusion within the Proposed Plan should only be made when full details on all areas of potential contribution are available. 3. The Manual should expressly note that any developer contributions or requests for infrastructure provision will only be made where they are clearly in accord with the requirements of either Circular 4/1998 and 1/2010 (depending upon whether the requirement is made through conditions or planning agreements respectively). These tests are set out in full in the above Circulars but in short, conditions must be necessary; relevant to planning; relevant to the development to be permitted; enforceable; precise; and reasonable in all other respects. Agreements must meet the tests of necessity; serving a 	<p>Agreements.</p> <p>In identifying infrastructure requirements and formulae for developer contributions we have taken account of the tests contained in Circular 1/2010.</p> <p>Paragraph 3.3 of the Proposed Plan points towards the relevant references to infrastructure provision and developer contributions – Appendix 4 of the Proposed Plan, the Action Programme and the Infrastructure and Developer Contributions Manual.</p> <p>The need for new or improved infrastructure has been identified following detailed analysis of the capacity of existing services to cope with additional development, as well as the forecasted impact of developments based on the information available at that time. As such, these requirements are necessary in planning terms to make development acceptable, and to avoid any detrimental impact on services and infrastructure.</p> <p>The Council is reviewing its evidence base for services and infrastructure on an ongoing basis to ensure that there is sufficient infrastructure for seeking the provision of infrastructure or developer contributions through new development.</p> <p>In preparing the LDP, the table of infrastructure requirements set out in Appendix 4 was presented in the main body of the text alongside Policy I1 in earlier drafts of the document. However, due to the size of the table and to keep the document as concise as possible it was relocated to an appendix with a cross-reference from Policy I1 to make this link clear. The inclusion of infrastructure requirements in the LDP complies with paragraph 23 of Circular 1 /2010 Planning Agreements which requires infrastructure requirements be set out within the LDP.</p> <p>Policy I1 also includes a cross-reference to the Infrastructure and Developer Contributions Manual, which is published as Supplementary</p>

Respondent	Summary of comment	Response to comment
	<p>planning purpose; relate to the proposed development; be fair in terms of scale and kind; and be reasonable in all other aspects. The wording of Policy I1 should be amended to reflect these tests.</p> <p>Changes:</p> <ol style="list-style-type: none"> 1. Publication of the Manual is premature. The Manual should only be published for consultation as Supplementary Guidance when all sections are complete regarding the proposed levels of contribution. In the meantime the Supplementary Guidance should be withdrawn. 2. When published, the Manual should clearly state in the Introduction section which new development it is applicable to. 3. The Manual should state in the section entitled "Our approach to Infrastructure delivery" on page : "Notwithstanding the guidance contained in this Manual, all requirements for infrastructure provision and/or developer contributions shall be made only where they fully satisfy the appropriate planning tests set out in Scottish Government Circular 4/1998 (namely: need, relevance to planning; relevance to development permitted; ability to enforce; precision and reasonableness – as set out in Annex A to this Circular) and/or Scottish Government Circular 1/2010 (paras 11-22) or any subsequent replacement of these Circulars. 	<p>Guidance to the LDP. In doing that the Council is seeking to provide as much information as possible on the likely contributions to be sought through Planning Agreements along with clear guidance on the methodology used to identify infrastructure requirements in the Proposed Plan.</p> <p>The reference to the Action Programme is included to provide details on infrastructure provision as required by paragraph 23 of Circular 1/2010 Planning Agreements. The Action Programme will be updated on an ongoing basis to reflect progress and highlight further actions required to implement each policy, project and development in the LDP.</p> <p>Appendix 2 does not relate directly to infrastructure requirements and is intended to provide a simple list of all opportunity sites in the LDP. In order to be as concise as possible and avoid repetition we do not consider there is a need for a linkage to Appendix 2.</p> <p>In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes.</p> <p>Policy I1 is currently being scrutinised by an independent Reporter as part of the Examination in Public of the Aberdeen Local Development Plan Proposed Plan. Therefore Policy I1 can not be amended to reflect the tests at this time.</p> <p>Provision of new transport infrastructure including services and facilities will always need to be negotiated between the Council and the Developer and informed by evidence presented in technical documents such as Transport Assessments as well as mitigation measures proposed. Where it is considered that development will generate the need for new transport infrastructure, contributions will be sought.</p> <p>The Council has carried out a significant level of work with transport colleagues and partners, as well as public transport operators, to establish</p>

Respondent	Summary of comment	Response to comment
<p>Bancon Developments Ltd (1561)</p>	<p>Objects to the SG and submit that the Infrastructure and Developer Contributions Manual be deleted in its current form, and be re-written on the basis of a guide, rather than a specific set of criteria and requirements, and be provided as a complete document for public comment prior to adoption, rather than with information omitted 'to follow shortly'.</p>	<p>the likely level of new infrastructure and services that will be required to support new developments. The transport schemes associated with new developments fall into two categories, Local Infrastructure and Strategic Infrastructure. Local transport projects are expected to be provided as part of new development and should be reflected in the associated Masterplan and subsequent planning applications. Strategic transport projects have been identified by a partnership group consisting of officers from Aberdeen City Council, Aberdeenshire Council, the Strategic Development Planning Authority, Transport Scotland and NESTRANS. Funds realised from this initiative will be managed and delivered by Nestrans.</p> <p>For the provision of local transport infrastructure, each site will be dealt with on a case by case basis and will need to be negotiated between the Council, Commercial operators and the Developer. Discussions should take place as early as possible in the Masterplanning and/or planning application process.</p> <p>The Infrastructure & Developer Contributions Manual defines the criteria and methodology for seeking contributions to mitigate the impact of development.</p> <p>We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning Agreements.</p> <p>The draft Infrastructure and Developer Contributions Supplementary Guidance contained as much information as was available at the time it was prepared. The Supplementary Guidance document was subject to consultation alongside the Aberdeen Local Development Plan Proposed Plan from September 2010 until January 2011. Following the consideration of the representations received and modifications made, the Supplementary Guidance will be published again for consultation in due</p>

Respondent	Summary of comment	Response to comment
<p>Paul and Williamsons LLP (1571)</p>	<p>Concerned with the approach set out in this SG. It is contrary to the terms of Circular 1/2010 as it fails to regard the 'scale and kind test' in Circular 1/2010. The approach in the SG appears to be simply a formula to each development to conclude that a contribution of a fixed amount will be required. The SG is incomplete therefore the Council seem to be seeking to obtain LDP status without giving landowners/developers and other interested parties an opportunity to properly comment. Seeking road improvement costs completely from landowners/ developers is unfair. We fail to understand how development anywhere in the city should contribute towards improvements. This objection is made for Core Paths contributions as well. Object to the formula for calculating per house equivalents for commercial premises. Same density (30 dwellings) should be used in the formula. Council should consider the individual impacts of each development. There should be a division of cost for educational provision between developers/landowners and the Council (same objection with regards to health facilities). Figures regarding the cost to develop a school must be justified. Concerned there may be an element of duplication in the costs as this may cover provision of community facilities as well. Object to the proposal to base the open space and green space network requirement on the number of bed spaces in a</p>	<p>course.</p> <p>We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning Agreements.</p> <p>In identifying infrastructure requirements and formulae for developer contributions we have taken account of the tests contained in Circular 1/2010.</p> <p>Paragraph 3.3 of the Proposed Plan points towards the relevant references to infrastructure provision and developer contributions – Appendix 4 of the Proposed Plan, the Action Programme and the Infrastructure and Developer Contributions Manual.</p> <p>Whilst the Council recognises there are challenges to delivering infrastructure in the current financial climate, it will important for the Council to work closely with the development industry to find solutions to delivering development and the necessary infrastructure to mitigate any impacts. The Council can utilise mechanisms to spread costs through staged payments where appropriate, and we are exploring opportunities for up-front funding.</p> <p>Policy 11 also states that “the level of provision will be commensurate to the scale and impact of development”. The negotiation of infrastructure provision and developer contributions through the Masterplan and planning application process will ensure infrastructure provision does not unduly restrict the implementation of development proposals or affect the viability of development, whilst ensuring that new development will be accompanied by an appropriate level of services to support new communities.</p> <p>The open space and green space network requirement is based on the Average Household Occupancy in Aberdeen as detailed in the Open</p>

Respondent	Summary of comment	Response to comment
	<p>development. It should be done on the average household figure. Inappropriate to charge per square metre based on building works. Also object to the application of a build cost unless the Council are actually going to go on and build indoor and outdoor facilities etc for that particular development. Object to same approach used for library and community facilities.</p>	<p>Space Supplementary Guidance.</p> <p>Regarding Libraries and Community Facilities, amendments have been made to the revised Supplementary Guidance. Contributions will only be sought where there is deemed to be an impact on current provision from new development.</p> <p>The Supplementary Guidance document was subject to consultation alongside the Aberdeen Local Development Plan Proposed Plan from September 2010 until January 2011. Following the consideration of the representations received and modifications made, the Supplementary Guidance will be published again for consultation in due course.</p>

Children's Nurseries and Sports Facilities

**Sportscotland
(1244)**

Combining these 2 issues in 1 SG is illogical as there is no connection between the two.

The SG guidance in relation to sports facilities is confusing as it deals with redevelopment of these (which is covered already by policy NE3 within the LDP itself, in respect of which we have made separate representation) and the development of new sports facilities. It is considered that this is an unduly negative policy and is not required.

Delete this SG in relation to sports facilities. The SPP requirements in relation to the policy presumption against the redevelopment of playing fields can be covered by an appropriately revised Policy NE3 and the assessment of new sports proposals can be considered in relation to other policies in the

Although the two subjects appear unrelated. Both are concerned with protecting the local environment and residential amenity. Because of this we would wish to retain the Supplementary Guidance as it is.

Respondent	Summary of comment	Response to comment
	LDP, principally policy CF2.	
Bats and Development		
N/A	N/A	N/A

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	31 st May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	River Don Corridor Framework Supplementary Guidance, in support of the Aberdeen Local Development Plan.
REPORT NUMBER:	EPI/12/110

1. PURPOSE OF REPORT

- 1.1 The Planning etc. (Scotland) Act 2006 paragraph 22 (1) states that a planning authority may adopt and issue guidance in connection with a local development plan. Aberdeen City Council has already prepared a number of draft Supplementary Guidance documents and has consulted on many of these alongside the Proposed Aberdeen Local Development Plan for a period of 16 weeks (between 24th September 2010 and 17th January 2011). Following examination the Aberdeen Local Development Plan was adopted on 29 February 2012 with 24 associated Supplementary Guidance documents adopted just over a month later. It is intended that further tranches of approved Supplementary Guidance will be forwarded to Scottish Ministers in due course for adoption.
- 1.2 The purpose of this report is to obtain Committee approval for a new item of Supplementary Guidance, the River Don Corridor Framework, to be forwarded to Scottish Ministers for adoption. This document was prepared in support of the Aberdeen Local Development Plan (LDP).
- 1.3 The River Don Corridor Framework was subject to public consultation for six weeks from 8 February 2012 until 21 March 2012. This document provides means to guide and assess development proposals affecting the River Don Corridor.
- 1.4 Due to the size of these Supplementary Guidance documents, hard copies have not been attached to this report. Copies are available in the Members' Lounge and by request from the Local Development Plan team.

2. RECOMMENDATION(S)

2.1 It is recommended that the Committee:

- (a) Approve the River Don Corridor Framework Supplementary Guidance document;
- (b) Note the representations received on the draft Supplementary Guidance document during the consultation undertaken; and
- (c) Approve officers' responses to these representations received on the draft Supplementary Guidance document.

3. FINANCIAL IMPLICATIONS

3.1 This Supplementary Guidance has been developed through the Sustainable Urban Fringes (SURF) Aberdeen project, a demonstration element of the wider European Union SURF Project, for which Aberdeen City Council is the Lead Partner (www.sustainablefringes.eu). The SURF Project is 50% funded by the European Regional Development Funds IVB North Sea Region programme. While there are no direct financial implications arising from this report, costs incurred through consultation and publicity related to the proposed guidance will be met through the existing SURF Aberdeen project budget (C27122).

4. OTHER IMPLICATIONS

4.1 There are no known legal or equipment implications arising from this report.

4.2 As a major landowner in the city, proposals for the development of land and assets owned by Aberdeen City Council will be subject to assessment in line with the principles and standards set out in the Supplementary Guidance, where applicable.

4.3 The progression of this Supplementary Guidance document will provide a clear area based framework for decision making, allowing comprehensive guidance for both applicants and officers, thereby making a significant contribution towards the Council's aim of promoting and achieving sustainable development. Detailed Supplementary Guidance also has value in reducing officer time spent on pre-application discussions.

4.4 The proposed River Don Corridor Framework Supplementary Guidance brings clear environmental, social and economic benefits to the River Don Corridor, and Aberdeen. It is intended to guide and inform officers in their assessment of the access, recreational, landscape, cultural and historical, environmental, wildlife and community engagement implications of any given development proposal within the Corridor, thereby effectively driving improvements in these aspects of the Corridor and the quality of the Corridor overall.

4.5 This item of Supplementary Guidance has been pre-screened under the Strategic Environmental Assessment (SEA) process. Council officers, including the SEA Co-ordinator, maintain the opinion that a full SEA is not required for this Guidance.

5. BACKGROUND/MAIN ISSUES

5.1 The Council agreed the content of the Proposed Plan on 18 August 2010. The Proposed Plan was a critical stage in the plan preparation process and was the result of a significant amount of assessment and public consultation.

5.2 Those items of Supplementary Guidance to be carried over from the Aberdeen Local Plan 2008 have been subject to public consultation and are the subject of separate reports, depending on whether further consultation is required on changes made. The River Don Corridor Framework Supplementary Guidance document has been newly produced through the Sustainable Urban Fringes Aberdeen project which has included extensive local community consultation, in part, helping to guide the Framework's direction and content.

5.3 The purpose of this Supplementary Guidance document is to provide further information and detail in respect of policies set out in the Local Development Plan, in accordance with the Scottish Government's intention that the Local Development Plan itself focuses on vision, the spatial strategy, overarching and other key policies, and proposals.

5.4 The River Don Corridor Framework Supplementary Guidance is intended to support the Green Space Network Policy NE1, as contained in the Proposed Plan. As this Policy makes clear, 'The natural environment and open spaces are important for landscape, natural heritage and wider social, health, economic and environmental reasons, such as adapting to climate change and helping to improve air and water quality.' This Framework is area specific and primarily integrates aspects of the Core Paths Plan, Open Space Strategy, Nature Conservation Strategy as well as other strategies, policies and plans that help deliver a high quality natural environment and access to good quality open spaces, making the River Don Corridor a place where people want to live, work and invest. It sets out the Vision, Aims, Objectives and Spatial Plan in relation to the access, recreational, landscape, cultural and historical, environmental, wildlife and community engagement aspects of the River Don Corridor.

5.5 A six week public consultation was undertaken from 8 February 2012 until 21 March 2012 to gain the views of all stakeholders on the document. All summarised representations and officer responses can be found in *Appendix A - River Don Corridor Framework: Public Consultation Comments, Responses and subsequent Framework amendments*. Five representations were received in total, one from the

Scottish Environment Protection Agency (SEPA), one from The River Don Trust, one from The Grandhome Trust and two from members of the public. The majority of representations expressed general support for the aims and objectives of the Framework, with requests for additional references and some amendments. Some of the comments received have been accepted and minor alterations to the document have subsequently been made. However, there are occasions where comments have been submitted and officers do not think a change to the document is suitable.

6. IMPACT

- 6.1 The Local Development Plan continues to support the vision of Aberdeen becoming an even more attractive place to live and in which to do business and will ensure that high quality employment opportunities exist. This process aspires to improve the access that the people of Aberdeen have to high quality services that meet their needs. The development and refinement of fit for purpose Supplementary Guidance to assist the Aberdeen Local Development Plan is paramount to supporting this vision and achieving the goals that Aberdeen aspires to.
- 6.2 The vision for Aberdeen is to be a city which is vibrant, dynamic and forward looking – an even better place to live and work, where people can expect high-quality services that meet their needs. This means making a visible difference to the quality of the city's urban and natural environment by promoting high quality development and providing an effective infrastructure to make us a world class strategic location.
- 6.3 To do this we must think strategically, facilitate development, engage positively with communities and the business sector and be open and transparent in our decision making. We also have a key role in delivering the vision for the City and Shire as expressed through regional plans and strategies. Planning and Sustainable Development is tasked with seeing that Aberdeen stays at the forefront of planning for the future.
- 6.4 The Supplementary Guidance represented in this report relates to the following Single Outcome Agreement objectives: 1- We live in a Scotland that is the most attractive place for doing business in Europe; 2- We realise our full economic potential with more and better employment opportunities for our people; 10- We live in well-designed, sustainable places where we are able to access the amenities and services we need; 12- We value and enjoy our built and natural environment and protect it and enhance it for future generations; 13- We take pride in a strong, fair and inclusive national identity; and 15- Our public services are high quality, continually improving, efficient and responsive to local people's needs.

- 6.5 The Supplementary Guidance represented in this report meets the vision of the Community Plan in promoting a strong image of the City and a sense of civic pride.
- 6.6 The Supplementary Guidance represented in this report supports the Council's 5 year Business Plan in terms of protecting and enhancing our high quality natural and built environment, attracting visitors, workers and investment to protect the economic future of the city, and, to facilitate new development projects to improve Aberdeen's living and working environment.
- 6.7 An equalities and human rights impact assessment (EHRIA) has been carried out in relation to the proposed Supplementary Guidance for Committee Report EPI/12/025 which was approved at EP&I Committee on 31 January 2012.

7. BACKGROUND PAPERS

- Aberdeen Local Development Plan – Proposed Plan
http://www.aberdeencity.gov.uk/Planning/ldp/pla_aldp_document_map.asp
- Aberdeen Local Development Plan – Proposed Action Programme
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31716&slD=14342>
- Aberdeen City and Shire Strategic Development Planning Authority: Aberdeen City and Shire Structure Plan
<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=423&slD=149>
- Planning etc. (Scotland) Act 2006
http://www.opsi.gov.uk/legislation/scotland/acts2006/asp_20060017_en_1
- Scottish Planning Series: Planning Circular 1/2009: Development Planning
<http://www.scotland.gov.uk/Resource/Doc/261030/0077887.pdf>
- The Town and Country Planning (Development Planning) (Scotland) Regulations 2008
http://www.opsi.gov.uk/legislation/scotland/ssi2008/ssi_20080426_en_1

8. REPORT AUTHOR DETAILS

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EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
SEPA1	We would like to see additional reference in the guidance to the Water Framework Directive and the responsibilities this places on various bodies, including local authorities, through the Water Environment and Water Services (Scotland) Act 2003. Further guidance on this is given in our planning guidance – LUPS-GU17 Guidance on the Water Framework Directive including river basin planning available on our website.	Accepted	The Framework has been amended to include additional reference to this.
SEPA2	Reference should be made to the fact that River Basin Management Plans are a material planning consideration and therefore the planning system has a key role in implementing these measures through the location and design of developments.	There is existing reference to River Basin Management Plans in Aberdeen's Local Development Plan (LDP) – section 1.13. This Draft Framework will be Supplementary Guidance to the LDP and as such seeks to complement it, not repeat what is in it.	The Framework has not been amended on the basis of this response.
SEPA3	Reference should also be made to the Water Environment (Controlled Activities) (Scotland) Regulations 2005 as amended (CAR) as important measures to address key pressures on the water environment of the River Don.	Accepted	The Framework has been amended to include additional reference to this.
SEPA4	In section 2.7 it is stated that "Below Dyce the channel is heavily modified". Given the Water Framework Directive connotations of this description (a Highly Modified Waterbody is a designation under the WFD), it would probably be best not to describe this section of the river as heavily modified to avoid confusion.	Accepted	This terminology in the Framework has been amended to avoid confusion with the Water Framework Directive terminology.
SEPA5	We would like to see a stronger emphasis in the document on the role that greenspace can play in the restoration of morphology (the condition of beds and banks) by creating space for channel restoration and improving riparian vegetation. This would link in to the Water Framework designation above.	Accepted	The Framework has been amended to include stronger emphasis on this.
SEPA6	We would also like to see reference to the role that greenspace can play in reducing diffuse pollution entering the water environment by functioning as 'green infrastructure' under sustainable urban drainage. Additional guidance on sustainable	Accepted	The Framework has been amended in the sustainable urban drainage section to

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
	urban drainage is given in our planning guidance on our website (LUPS-GU2 Planning advice on Sustainable Drainage Systems (SUDS)).		include reference to the role of greenspace in reducing diffuse pollution.
SEPA7	We would also like to see reference to <u>aquatic</u> habitats in the biodiversity section.	There is already reference to aquatic habitats in the biodiversity section: “Estuarine & Intertidal Habitats, Wet and Riparian Woodland, River and Burns and Wetlands”	The Framework has not been amended on the basis of this response.
SEPA8	We would like to see a clearer summary of the guidance, including any proposals, including identification of opportunities for improvement of bed and banks, and clearer recommendations as to how the guidance and various proposals will be taken forward. For example, all planning applications within the River Don Corridor should be required to demonstrate how they contribute to the implementation of the Guidance.	Accepted, in part. Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved.	The Framework has been amended by widening the introductory paragraphs to include a clearer outline of the document, including a statement of intent. The document now also includes clearer details regarding the mechanisms for 'Implementation, monitoring & review'.
TurnGT1	On behalf of the Grandhome Trust who will be bringing forward LDP site 0P12 for development, we welcome and support the broad aspirations of the document.	Noted, with thanks	The Framework has not been amended on the basis of this response.
TurnGT2	Having undertaken a recent review of the Masterplan, we are developing a clear vision for our site based on known environmental constraints and assets. There are several points of detail in the document which relate to Grandhome, but pre-date our analysis. The document therefore clearly needs	This Framework sets some of the higher level Strategic Objectives (SO) that must be considered as part of any development along the River Don Corridor. These SO are supported with a series of graphics that	The Framework has not been amended on the basis of this response.

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
TurnGT3	<p>updated to align with our Vision and I would suggest a meeting is arranged with the Council and SURF in order to align both processes.</p> <p>Greenspace Analysis, p23 Although this Section suggests large parts of the City are distant from a Major City Park, this is typical for suburban locations. The analysis should refer to access to open countryside around Aberdeen. Although a different open space resource, it is important nonetheless. It is not possible to achieve equality of open space provision across the City, but different locations will have a different blend of open space provision. This point is quite separate from localised provision within housing areas and the Grandhome masterplan has sought to address this through the provision of large parks, wildlife corridors and incidental open space.</p>	<p>present these in visual terms. These graphics are in themselves not prescriptive plans that are expected to be implemented in complete conformity. This point is especially pertinent within masterplan zones and sites. It is the role of Masterplans to align to and help deliver these SO, by setting out further details of the development principles at specific locations. It is considered unlikely that the SO in this Framework will conflict with the principles set out in a Masterplan for the Grandhome site, or any other masterplan along this Corridor.</p> <p>It should be noted that at this time no Masterplan has either been submitted or approved for the Grandhome site.</p> <p>Matters related to open space provision associated with new developments are subject to the approved Open Space Audit Report and Open Space Supplementary Guidance documents. It is not the role of this Framework to divert from these approved Policies and Guidance. This Framework seeks to consider and present these in an integrated format.</p>	<p>The Framework has not been amended on the basis of this response.</p>
TurnGT4	<p>Open Space Typologies, p28 The definitive network on this plan should be treated with caution as our own evidence base suggests the position and extent of the Green Space Network is largely arbitrary. Nonetheless, the masterplan has conformed with the intent, and has also sought to achieve a greater and more pervasive green network within the site.</p>	<p>Matters related to the Green Space Network (GSN) are subject to the Green Space Network Policy (NE1) as detailed in the approved Local Development Plan. The GSN Policy is not arbitrary but based on a very clear rationale. It is not the role of this Framework to divert from this Policy. This Framework seeks to consider and present</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
TurnGT5	<p>Strategic Objective 3, p42</p> <p>Whilst supportive of the desire to increase native woodland cover, regrettably, we must object to the plan shown in support. The plan shows aspirational woodland within the western edge of site OP12. Taken with the Green Space Network, the pylon corridor, contamination zones and other constraints, these aspirational designations do not account for the need to deliver housing within the allocated sites.</p> <p>We are seeking the removal of this designation and in its place a commitment to ensure that the landscape strategy for Grandhome will complement or reinforce existing tree cover in visual and biodiversity terms.</p>	<p>Accepted, in part – please refer to the amendment opposite.</p> <p>Please also refer to the Response to item Ref TurnGT2 above.</p>	<p>The Framework has been amended to slightly vary the graphic supporting Strategic Objective 3 (and all subsequent graphics presenting this principle). The aspirational woodland within the boundary of OP12 (Grandhome) has been moved outwith the Opportunity Site boundary. This amendment should be considered in light of the Response to item Ref TurnGT2, above.</p>
TurnGT6	<p>Strategic Objective 7, p51</p> <p>Strategic Objective 7 suggests the designation is essential to screen new development. We consider that the EIA and masterplanning processes are best place to assess and respond to this impact and that the document is making a presumption that visibility equates to harm.</p> <p>We would ask that the designation be removed and replaced with a statement that supports the principle of having regard to the relationship between new development and its impact on the Don Corridor.</p>	<p>Please refer to the Response to item Ref TurnGT2 above.</p> <p>In addition – Strategic Objective 7 neither states, nor intends to suggest, that screening is <i>the</i> solution to development along the River Don Corridor. SO7 seeks to reinforce the principle that appropriate development should occur within an appropriate landscape setting. Screening may or may not be one solution to address this issue at one time or location.</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
TurnGT7	<p>Strategic Objective 6, p49</p> <p>At this stage we are not proposing any pedestrian crossings of the Don in support of the masterplan. A Don Crossing will raise significant technical and environmental issues and should not be undertaken lightly, particularly if there is a desire to 'future-proof' these structures. The matter requires special consideration beyond the remit of the current document.</p> <p>Moreover, whilst the ability for pedestrians to cross the Don would be advantageous, at this stage we see no significant economic or social benefits that would justify the cost and diversion of resources away from other objectives in the document, or outweigh potential environmental harm. We respectfully ask that these aspirational links be removed from the plan.</p>	<p>Please refer to the Response to item Ref TurnGT2 above.</p> <p>In addition – It is not the role of this Framework to divert from existing Policies, Plans or Guidance. This Framework seeks to consider and present existing Policies and Guidance in an integrated format.</p> <p>Crossings over the River Don, including within the vicinity of the Grandhome masterplan site, are explicitly stated requirements within the following approved documents supporting the approved Local Development Plan:</p> <p>Infrastructure Requirements for Masterplan Zones 2010</p> <p>Development Framework, Former Davidson's Mill, Bucksburn 2011</p> <p>Stoneywood Estate Framework and Masterplan 2011</p> <p>Proposed Action Programme & Draft Action Programme 2012</p>	<p>The Framework has not been amended on the basis of this response.</p>
TurnGT8	<p>Item 5.2 - 5.4</p> <p>Regrettfully, the Trust must object to the document allocating community nodes, strategic connections, paths and bridges on the Grandhome OPI2 site without regard for wider considerations. Whilst all of these concepts are welcome, the masterplan consultation process is the most appropriate mechanism for addressing these issues. We would ask that all such references are omitted and that the document focus on the Don Corridor only.</p>	<p>Please refer to the Response to item Ref TurnGT2 above.</p> <p>In addition – areas of the Grandhome opportunity site (OP12) fall very much within the indicative boundary for the River Don Corridor. As such, this River Don Corridor Framework would fail in its Objectives if it did not take into consideration the full extent of the Corridor, including this site.</p> <p>It is also a consistent with good planning principles to consider connectivity within a site and of that site with surrounding areas – one of which is Grandhome (OP12).</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
RDT	<p>The River Don Trust wishes to concentrate its comments on the lack of focus upon Invasive Non Native Species throughout the document.</p> <p><i>The River Don Trust is a charitable organisation established: To advance for public benefit environmental protection and improvement by conserving and enhancing the natural biodiversity associated with the freshwater environments of the River Don.</i></p> <p>The River Don Trust (RDT-Hereafter) is at the forefront of INNS surveying, monitoring, and control within the River Don catchment. The RDT prepared and produced a Bio security document consulted upon by various stakeholders including Aberdeen City Council and SURF Aberdeen. The document sets out key species for monitoring and control within the catchment, it outlines aims for monitoring, recording and controlling INNS and also established a timeline of actions over the period of 2011-2016.</p> <p>As a result of this document the RDT has developed and secured funding for several control programmes to control INN Plants such as Giant Hogweed, Japanese Knotweed and Himalayan Balsam all of which are known to be present within the River Don Corridor area.</p> <p>Therefore the RDT was surprised at the lack of suggested INNS monitoring, recording and control within the draft supplementary guidance.</p> <p>The RDT would wish to see that changes are made to SURF Aim B: Enhance the biodiversity value of the area, reinforcing and expanding habitat networks - to include a specific strategic objective to monitor record and control INNS plants within the corridor.</p> <p>At present the objective, Improve biodiversity loosely covers this topic but the RDT feels that this topic merits its own strategic objective.</p> <p>Ideally this objective would take into consideration any potential INNS control projects which could span, include or be focused within the River Don Corridor Project area, not</p>	<p>This Framework sets some of the higher level Strategic Objectives (SO) that must be considered as part of any development along the River Don Corridor.</p> <p>Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved.</p> <p>These comments have been recorded and will be considered as part of the above process.</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
	<p>withstanding ongoing projects.</p> <p>The RDT wishes that the objective also stipulates that control where possible is undertaken in a coordinated manner to allow for efficient use of resources and that existing stakeholders such as the RDT and City Council be a point of contact for future developments to illustrate current best practice.</p>		
RDT2	<p>The RDT would wish to see that documents such as the River Don Bio security Plan 2011-2016 also be identified as a reference source, (available at www.riverdon.org.uk)</p>	Accepted	<p>The Framework has been amended to include reference to the River Don Trust's Bio security Plan.</p>
PublicID1	<p>First of all, in general terms I am very happy at the idea of considering the Don corridor as a reserved green area and of facilitating access to it, particularly for non-motorised movement.</p>	Noted, with thanks	<p>The Framework has not been amended on the basis of this response.</p>
PublicID2	<p>My first concern is that the proposed 3rd River Don crossing receives barely a mention; this is a grave omission given that it now looks fairly certain that the bridge will be built. I was under the impression that the design of the bridge and of the approaches to it would include provision for foot and cycle traffic, in which case failure to consider it properly (i.e. by including the approaches to the bridge) it in the Supplementary Guidance document strikes me as odd, Even if it ends up as purely a vehicular bridge, there is bound to be an impact on pedestrian and cycle traffic, particularly in Tillydrone but also in Danestone.</p>	Noted	<p>The Framework has been amended to strengthen reference to the 3rd Don Crossing and to re-align the strategic connections in the locality of the Crossing.</p>
PublicID3	<p>I hope that it will have pedestrian and cycle access, preferably separate as there is considerable public hostility to shared cycle/pedestrian paths and at best it slows down cycle traffic which reduces the advantage of cycling over travelling by bus (I qualify for a bus pass so there is no cost advantage, the main advantages being speed and health benefits). I also hope that it will have entry and exit points for such access to meet existing</p>	<p>This Framework sets some of the higher level Strategic Objectives (SO) that must be considered as part of any development along the River Don Corridor.</p> <p>Detailed plans for river crossings, including the 3rd Don Crossing, are not the subject of</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
	paths and roads.	<p>this Framework.</p> <p>Detailed plans for the 3rd Don Crossing have been completed and approved. They can be viewed here: http://www.aberdeencity.gov.uk/transport_streets/roads_pavements/transport_projects/lap_accessnorth_home.asp</p>	
PublicID4	<p>I am also concerned that the main emphasis appears to be on movement along the banks of the river - much needed - but that other than providing additional crossings there seems to be no plan to facilitate movement across the river, particularly between where I live (Laurel View) and the south side of the river, a route on which I travel regularly on foot and by cycle to reach other parts of Aberdeen. The south side of Grandholm Bridge, which oddly is shown as a proposed access point, has no space for car parking and can be reached on foot or by cycle from the East and South only by the road from the foot of Don Street. The surface of this cobbled road was very badly remade when services were put in for Grandholm Village and it is virtually impossible to cycle downhill on the correct side of the road. Cars regularly drive on the other side of the road to avoid the extremely rough surface, causing a further safety hazard to pedestrians and cyclists travelling uphill. There is no pavement on this road, and the historic Jacob's Ladder stairs (not mentioned in the document) which used to provide safe pedestrian access to the bridge from above have been closed off for a number of years, no doubt on health and safety grounds.</p>	<p>This Framework sets some of the higher level Strategic Objectives (SO) that must be considered as part of any development along the River Don Corridor.</p> <p>It considers access to, along and across the River, as well as the strategic connections between the communities and facilities around the River Don Corridor.</p> <p>Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved.</p> <p>These comments have been recorded and will be considered as part of the above process.</p>	<p>The Framework has not been amended on the basis of this response.</p>
PublicID5	<p>It seems to me that the plan pays insufficient attention to who may cross the river, why they might do so and the locations of their start points and destinations. For example, I notice that many people travel to Tesco Extra from the south side of the river by the road which runs parallel to the mill lade on the north of the river. Just before the east end of the Tesco complex there is an underpass, which used to give convenient access to Tesco</p>	<p>Please refer to the Response to item PublicID4, above.</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
PublicID6	<p>but which is now virtually useless because the direct route to up a stairway which once led to it from the mill lade road has been blocked by the developers of the Mill Lade Wynd houses and the alternative path which users trod out has been effectively blocked by deposits of waste from the older houses in the area.</p> <p>I am puzzled by your designation of the area around Tesco Extra and Bannatyne's as a 'Community Node'. Granted it has the only shops in Danestone and a private health club but that hardly qualifies it as a centre for the community. Access to the Tesco store from the West and from Bannatyne's is particularly difficult for non-motorised disabled people, cyclists and pedestrians because the main entrance is vehicular only with no pavement and the ramp and stairs entrance is at the other end of the very large building.</p>	<p>The use of the term Community Nodes in this Framework is a means to identify those areas that contain facilities of interest for communities: not necessarily those places that are the heart of communities. As places of interest, they are likely to generate a movement of people that is important to consider as part of the planning process.</p> <p>While the locality referred to only contains two individual facilities, these are significant enough to have an impact on the movement of people to / from the surrounding communities, and beyond.</p> <p>Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved.</p> <p>These comments have been recorded and will be considered as part of the above process.</p>	<p>The Framework has not been amended on the basis of this response.</p>
PublicID7	<p>There are several, existing paths which are not shown in your plans, in particular along the North side of the river running East from the North end of Grandholm bridge, and diagonally to the West of Grandholm Village to the present bridge (currently either being demolished or refurbished) over the Grandholm Mill Lade. This path is very muddy in wet weather but provides convenient pedestrian access to the no. 19 bus terminus across the pedestrian bridge downstream of Grandholm Bridge. I</p>	<p>This Framework does not seek to identify and mark all existing paths / networks in the Corridor – this would be unfeasible and would overcomplicate the plans.</p> <p>This Framework sets some of the higher level Strategic Objectives (SO) that must be considered as part of any development along</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
PublicID8	<p>I applaud your desire to celebrate the industrial history of the area and I am particularly concerned that we should not lose the best preserved mill lade, on the North side of the river leading to Grandholm Village and the remaining Grandholm Mill buildings. This waterway is suffering from neglect but is still intact and is well worthy of being maintained properly. There are also vestiges of the mill lade on the opposite side of the river which are in a poorer state but which I feel should be refurbished before it is too late, as well as remains of another lade near Bannatynes. The water wheel from Woodside Mills is currently in the National Museum of Scotland and it would be really good for it to be reinstated and made to work again, perhaps for a local electricity generation plant; I am sure that a modern water turbine or two could be incorporated sympathetically into schemes to refurbish one or both mill lades, or even into schemes to make use of the weirs on the river.</p>	<p>the River Don Corridor. The focus of this aspect of the Framework is on identifying supporting delivery and management of key Core Paths -- as designated by the Core Paths Plan. Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved. These comments have been recorded and will be considered as part of the above process.</p>	<p>The Framework has not been amended on the basis of this response.</p>
PublicID9	<p>I also agree that interpretation is an important aspect as we have a superb resource for pupils in our local schools and visitors to experience our local history and ecology at first hand. Marked trails and waypoints, improved access and renewal of tired out woodland all form a major part of what needs to be</p>	<p>Noted, with thanks. Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved. These comments have been recorded and will be considered as part of the above process.</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
PublicID10	<p>done. I'm just an individual but would be very glad to help a community effort to move this on.</p> <p>A community is what we are lacking (see my comment on the 'Community Node' marked on your plans as the immediate area where I live was never planned as a community, like the rest of the Bridge of Don postal area other than the area on the North of the river between the Brig of Balgownie and the Bridge of Don, which grew organically before the times when giant housing estates suddenly appeared without a thought to integration into the community or how the occupants would travel to work elsewhere.</p> <p>It appears to me that the community aspect needs to be tackled in more than one way: by the provision of real community hubs, where people meet for other purposes than weekly shopping or strolling on a treadmill; by creating and developing community organisations to bring people together; and by involving individuals in the area in shared projects which bring them together. I don't think that it is enough to rely on organisations which already exist though part of the policy ought to be to boost their membership and provide them with support to take the burden off a few hard pressed volunteers. I believe that consideration should be given to forming new organisations or task groups specifically aimed at involving people from the community, and providing council finance and support at least in the short term to ensure that they take off and become sustainable (if their purpose is not short term).</p>	<p>The planning of community nodes, including their specific locations, is outside of the scope of this Framework.</p> <p>This Framework does support strengthening of the physical and organisational connections that seek to link communities.</p> <p>Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved.</p> <p>These comments have been recorded and will be considered as part of the above process.</p>	<p>The Framework has not been amended on the basis of this response.</p>
PublicID11	<p>I commend you for your efforts in producing this guidance and hope that my comments are of assistance in developing plans for improving the River Don Corridor.</p>	<p>Noted, with thanks</p>	<p>The Framework has not been amended on the basis of this response.</p>
PublicCL1	<p>The work around River Don corridor is excellent - such a wealth of history and wildlife easily accessible. The rubbish dumping is horrible. Suggest involve staff from local industry and shops to help protect this area.</p>	<p>Noted, with thanks.</p> <p>This Framework sets some of the higher level Strategic Objectives (SO) that must be considered as part of any development along</p>	<p>The Framework has not been amended on the basis of this response.</p>

EPI/12/110 - Appendix A – River Don Corridor Framework: Public Consultation Comments, Responses and resulting Framework amendments

Ref	Comment summary	Response	Amendments to Framework
		<p>the River Don Corridor.</p> <p>Specific opportunities for improvement and associated proposals and recommendations will be identified and considered as part of the development of the Implementation Plan for the Framework. Development of this Plan will commence once the Framework has been approved.</p> <p>These comments have been recorded and will be considered as part of the above process.</p>	

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Supplementary Guidance: Householder Development Guide
REPORT NUMBER:	EPI/12/109

1. PURPOSE OF REPORT

- 1.1 The Planning etc. (Scotland) Act 2006 paragraph 22 (1) states that a planning authority may adopt and issue guidance in connection with a local development plan. The purpose of this report is to inform members of the outcome of a consultation exercise undertaken in relation to a specific draft Supplementary Guidance document, entitled the Householder Development Guide, and to seek approval of responses to the comments received and adoption of the Supplementary Guidance document.
- 1.2 This report concerns a single draft Supplementary Guidance document, relating to domestic or 'householder' development. The draft Householder Development Guide was subject to an 8-week period of public consultation, which ran from 25th November 2011 until 20th January 2012. That public consultation was agreed by members at the Enterprise, Planning & Infrastructure Committee meeting of 15th November 2011.
- 1.3 A total of 4 responses were received in response to the consultation. Following analysis of those representations received, a number of minor alterations to the document are proposed. These are set out in Appendix 1. Taking into account the minor nature of the changes made, and the direct link between those changes and the comments received through consultation, it is concluded that a further period of public consultation will not be necessary. As a result, it is proposed that the Householder Development Guide be adopted as interim planning advice, pending its submission to Scottish Ministers for formal ratification. Unless directed otherwise by the Scottish Government within 28 days of its submission, the document will be adopted as formal Supplementary Guidance in support of the Aberdeen Local Development Plan without further referral to this committee.

1.4 A summary of the representations received during the consultation period, officers' recommended responses to those representations and a list of all other minor amendments made are attached as Appendix 1 to this report.

2. RECOMMENDATION(S)

2.1 It is recommended that the Committee:

(a) Note the representations received in relation to the draft Householder Development Guide;

(b) Approve officers' responses to representations received on the draft Householder Development Guide; and

(c) Agree the adoption of the Householder Development Guide Supplementary Guidance document pending ratification by the Scottish Government following which it will be formally adopted by the Council.

3. FINANCIAL IMPLICATIONS

3.1 There are no direct financial implications arising from this report. Any future publication costs can be met through existing budgets. Adoption of the Householder Development Guide as Supplementary Guidance is expected to make savings associated with more efficient processing of domestic planning applications and a reduction in staff time spent on routine pre-application enquiries.

4. OTHER IMPLICATIONS

4.1 There are no known legal or equipment implications arising from this report.

4.2 As an owner of substantial residential property in the city, proposals for the development of residential property owned by Aberdeen City Council will be subject to assessment in line with the principles and standards set out in the Supplementary Guidance, where applicable.

4.3 The progression of this Supplementary Guidance will provide a clear framework for decision making, allowing comprehensive guidance for both applicants and officers, thereby making a significant contribution towards the Council's aim of promoting and achieving sustainable development. The publication of a single document specifically related to householder development represents a more straightforward and user-friendly approach, which should result in a reduction in the

number of pre-application enquiries of a simple nature. This has value in reducing officer time spent on pre-application discussions.

- 4.4 The proposed Householder Development Guide brings environmental benefits, as it informs applicants and agents of the Council's duties as regards trees, protected species and the natural environment generally. The document explains where additional supporting information may be required in the presence of such factors, and links to other Supplementary Guidance documents which provide more specific guidance on these issues. By making applicants aware of environmental issues at an early stage, design proposals should be tailored to mitigate any likely impact upon trees, protected species etc.

5. BACKGROUND/MAIN ISSUES

- 5.1 At a Special Council Meeting on 25th January 2012, members resolved to adopt the plan from 29th February, unless directed otherwise by the Scottish Government. The plan was duly adopted on 29th February 2012.
- 5.2 The purpose of this Supplementary Guidance is to provide further information and detail in respect of policies set out in the Local Development Plan, in accordance with the Scottish Government's intention that the Local Development Plan itself focuses on vision, the spatial strategy, overarching and other key policies, and proposals.
- 5.3 The draft Householder Development Guide is an entirely new document, albeit one which incorporates elements of previous topic-based guidance documents. Prior to the adoption of the Aberdeen Local Development Plan, planning guidance on domestic development types was contained in a diverse range of separate supplementary guidance documents on various different topics. The Householder Development Guide is intended to provide clear and concise guidance to householders and agents within a single document, and will be the principal tool for planning officers to use when assessing proposals for domestic development. It is noted that previous planning guidance in relation to domestic development types was not progressed alongside the Aberdeen Local Development Plan, and so the progression of this new document is a matter of some priority.
- 5.4 The preparation of this Householder Development Guide has involved extensive discussions with officers in order to ascertain which elements of existing guidance were of particular value and identify areas where existing guidance was insufficient or where no appropriate guidance exists. The document has also taken account of the Scottish Government's planned changes to domestic Permitted Development rights.

5.5 A copy of the Householder Development Guide is attached as Appendix 2 to this report.

6. IMPACT

6.1 The Local Development Plan continues to support the vision of Aberdeen becoming an even more attractive place to live and in which to do business and will ensure that high quality employment opportunities exist. This process aspires to improve the access that the people of Aberdeen have to high quality services that meet their needs. The development and refinement of fit for purpose Supplementary Guidance to assist the Aberdeen Local Development Plan is paramount to supporting this vision and achieving the goals that Aberdeen aspires to.

6.2 The vision for Aberdeen is to be a city which is vibrant, dynamic and forward looking – an even better place to live and work, where people can expect high-quality services that meet their needs. This means making a visible difference to the quality of the city’s urban and natural environment by promoting high quality development and providing an effective infrastructure to make us a world class strategic location.

6.3 To do this we must think strategically, facilitate development, engage positively with communities and the business sector and be open and transparent in our decision making. We also have a key role in delivering the vision for the City and Shire as expressed through regional plans and strategies. Planning and Sustainable Development is tasked with seeing that Aberdeen stays at the forefront of planning for the future.

6.4 The Supplementary Guidance presented in this report. relates to the following Single Outcome Agreement objectives: 10- We live in well-designed, sustainable places where we are able to access the amenities and services we need; 12- We value and enjoy our built and natural environment and protect it and enhance it for future generations; 13- We take pride in a strong, fair and inclusive national identity; and 15- Our public services are high quality, continually improving, efficient and responsive to local people’s needs.

6.5 The draft Supplementary Guidance presented in this report meets the vision of the Community Plan in promoting a strong image of the City and a sense of civic pride.

6.6 The draft Supplementary Guidance presented in this report supports the Council’s 5 year Business Plan in terms of protecting and enhancing our high quality natural and built environment, attracting visitors, workers and investment to protect the economic future of the city, and, to facilitate new development projects to improve Aberdeen’s living and working environment.

6.7 An equalities and human rights impact assessment (EHRIA) was previously carried out in relation to this draft Supplementary Guidance document. That EHRIA is included as Appendix 3 to this report.

7. BACKGROUND PAPERS

- Aberdeen Local Development Plan – Proposed Plan
http://www.aberdeencity.gov.uk/Planning/ldp/pla_aldp_document_map.asp
- Aberdeen Local Development Plan – Proposed Action Programme
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31716&slD=14342>
- Aberdeen City and Shire Strategic Development Planning Authority: Aberdeen City and Shire Structure Plan
<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=423&slD=149>
- Planning etc. (Scotland) Act 2006
http://www.opsi.gov.uk/legislation/scotland/acts2006/asp_20060017_en_1
- Scottish Planning Series: Planning Circular 1/2009: Development Planning
<http://www.scotland.gov.uk/Resource/Doc/261030/0077887.pdf>
- The Town and Country Planning (Development Planning) (Scotland) Regulations 2008
http://www.opsi.gov.uk/legislation/scotland/ssi2008/ssi_20080426_en_1

8. REPORT AUTHOR DETAILS

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Consultation responses for Supplementary Guidance on Householder Development

Representor	Representation	Officers Response
Historic Scotland	Introductory section could include information related to listed buildings, including when listed building consent is required and include links to the relevant national and local policy and guidance.	Agreed – further background information explaining the listed building consent process and directing to further reading would be beneficial, and has now been added to the introductory section of the document.
Ally Thain (ACC-Private Sector Housing Manger)	There are concentrations of HMO's near the University, i.e. Garthdee and Sunnybank. These areas lie out with the designated 'City Centre' area and are accessed by a common stairway.	Acknowledged – guidance in relation to HMOs has been reconfigured in consultation with the Council's HMO licensing unit. Restriction on common stairways has been removed.
Ally Thain (ACC-Private Sector Housing Manger)	What is the reasoning behind not supporting a planning application for a flat with a common stair located out with the city centre, but supporting one in the city centre?	Acknowledged – guidance in relation to HMOs has been reconfigured in consultation with the Council's HMO licensing unit. Restriction on common stairways has been removed.
Ally Thain (ACC-Private Sector Housing Manger)	What is the reasoning behind concerns about car parking and rubbish disposal in HMO's? A family house could produce the same amount of rubbish and have the same number of vehicles?	This is potentially true, though a family is less likely to be comprised exclusively of adults. In assessing the impact, in planning terms, of a number of adults living independently in HMOs, regard should be had for the increased pressure

		on shared facilities relative to normal family use. This position reflects the Scottish Government's Circular 8/2009, which sets out guidance on Planning Control and Licensing for HMOs.
Ally Thain (ACC-Private Sector Housing Manger	Not all HMO's are rented to students however HMO accommodation in Aberdeen is very important, especially as the Universities policy is to accommodate first years only.	Acknowledged – guidance in relation to HMOs has been reconfigured in consultation with the Council's HMO licensing unit. Thresholds for requiring planning permission for HMO have been revised to ensure proportionality and ensure that available HMO rental stock is not unduly affected.
Ally Thain (ACC-Private Sector Housing Manger	HMO Unit's policy is to encourage HMO accommodation. Your guidance would appear to have the opposite effect	Acknowledged – guidance in relation to HMOs has been reconfigured in consultation with the Council's HMO licensing unit. Thresholds for requiring planning permission for HMO have been revised to ensure proportionality and ensure that available HMO rental stock is not unduly affected.
Ally Thain (ACC-Private Sector Housing Manger	Most student halls of residence accommodation is access via a common stairway and some premises lie outwith the City Centre. The guidance would put an end to this	Purpose-built student accommodation often requires HMO licence, but would not normally require planning permission for HMO, as purpose-built student accommodation would be considered a

		separate use.
Ally Thain (ACC-Private Sector Housing Manger)	There needs to be consultation with my service, HMO Landlords, Private student accommodation providers etc	Agreed – consultation has been undertaken with the Council's HMO unit.
Amy Tigg (Transport Scotland)	Pages 29-30 make reference to the Council as roads authority. For trunk roads in Aberdeen City, Transport Scotland is the relevant roads authority	Acknowledged – Reference in p30 changed accordingly. P29 to remain unchanged, as any discussion with local roads authority would flag up if any proposal related to trunk roads, and it would complicate guidance unduly to refer residential driveway applicants to Transport Scotland.
Nicola Abrams (SEAP)	No comments to make	Noted

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Supplementary Guidance

Topic: Householder Development Guide



▪ INTRODUCTION

Good quality design, careful siting and due consideration of scale are key to ensuring that domestic development does not erode the character and appearance of our residential areas. Poorly designed extensions and alterations to residential properties can have a significant impact on the character and appearance of a building which, when repeated over time, can have a significant cumulative impact upon the wider area. By ensuring that careful consideration is given to such works, and consistent standards applied, we can seek to retain the characteristics of the built environment which contribute towards the character and identity of an area, while also protecting the amenity enjoyed by residents.

▪ OVERALL OBJECTIVE

All extensions and alterations to residential properties should be well designed, with due regard for both their context and the design of the parent building. Such extensions and alterations should make a positive contribution to the design and appearance of a building, maintain the quality and character of the surrounding area, and respect the amenity of adjacent neighbours. This document seeks to facilitate good design and provide a sound basis for restricting inappropriate development, bringing together a number of existing pieces of supplementary guidance into a single document in the process.

▪ SCOPE OF GUIDANCE

The guidelines set out in this document shall apply, on a city-wide basis unless otherwise stated, to all domestic properties. In the case of dormer windows and roof extensions, the guidelines will also extend to originally residential properties now in non-domestic use. It should be noted that the guidance contained within this document will be applicable only to those development proposals which require an express grant of planning permission, and shall not apply where any proposal is exempted from the application process by virtue of relevant permitted development rights. Permitted Development is a term used for certain types of development which, by satisfying specified conditions, is automatically granted planning permission without the submission of an application to the planning authority.

This document supersedes existing supplementary guidance relating to *'Dormer Windows and Roof Extensions'*, *'Dwelling Extensions in Aberdeen City'*, *'Dwelling Extensions in Cove'* and *'Extensions forward of the Building Line'*. The guidelines set out in this supplementary guidance should, where relevant to the development proposal, be read in conjunction with the City Council's other published Supplementary Guidance and Technical Advice Notes.

▪ THE ROLE OF THE PLANNING SYSTEM

In coming to a decision on any planning application, the planning authority must determine that application in accordance with the development plan, unless '*material considerations*' indicate otherwise. At time of writing, the development plan comprises the Aberdeen Local Plan 2008 and the Aberdeen City and Shire Structure Plan 2009.

There are two main tests in deciding whether a consideration is material and relevant:

- It should serve or be related to the purpose of planning – it should therefore relate to the development and use of land; and
- It should fairly and reasonably relate to the particular application.

It is for the decision-maker to assess both the weight to be attached to each material consideration and whether individually or together they are sufficient to outweigh the provisions of the development plan. As a result of changes to the planning system, made through the 2006 Planning etc. (Scotland) Act and associated regulations, Supplementary Guidance prepared and adopted in connection with a Local Development Plan will form part of the development plan.

It should be noted that the planning system does not exist to protect the interests of one person against the activities of another, although in some cases private interests may well coincide with the public interest. In distinguishing between public and private interests, the basic question is whether the proposal would unacceptably affect the amenity and existing use of land and buildings which ought to be protected in the public interest, not whether owners or occupiers of neighbouring or other existing properties would experience financial or other loss from a particular development.

▪ STATUTORY REQUIREMENTS

The definition of "development" is set out in the Town and Country Planning (Scotland) Act 1997, as amended by the Planning etc (Scotland) Act 2006, and is termed as the carrying out of building, engineering, mining or other operations in, on, over or under land, or the making of any material change in the use of any buildings or other land. There are various exemptions to this, details of which can be provided by the planning authority.

Permitted Development rights set out in the Town and Country Planning (General Permitted Development) (Scotland) Order 1992, as amended. This document, commonly termed the 'Permitted Development Order' or 'PD Order', sets out various works which will not require an express grant of planning permission, provided those works are carried out in accordance with certain criteria. Where it is intended to utilise these rights, we encourage householders to seek confirmation from the planning authority before any works are carried out. The permitted development rights available to any

particular property can vary depending on factors such as location within a conservation area, removal of such rights by condition placed on a past approval, or removal of such rights by a virtue of an Article 4 direction. The effect of such a Direction is to remove permitted development rights, thereby necessitating submission of a formal application for planning permission. All of Aberdeen's Conservation Areas are covered by Article 4 Directions, with the exception of Rosemount and Westburn (Conservation Area 11). Article 4 directions also apply to areas of areas of Kingswells and Burnbanks, which lie outwith any Conservation Area. Please contact the planning authority for further details.

Taking into account the above, householders considering any works to land or property, should ask the following questions;

1. Do these works constitute 'development' as set out in planning legislation?
2. If the works constitute 'development', can they be carried out as 'Permitted Development'?

The answers to these questions will determine whether a planning application is necessary for any works, though it is recommended that the Council be consulted in order to ensure that any interpretation of legislation is correct.

In assessing planning applications, there are a number of duties incumbent upon Aberdeen City Council as the planning authority. These are duties set out in relevant planning legislation, and include the following;

Listed Buildings – The authority shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

Conservation Areas – With respect to buildings or land in a conservation area, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area.

Trees – The planning authority shall, in granting planning permission for any development, ensure adequate provision is made for the preservation or planting of trees. Furthermore the authority shall make tree preservation orders (TPOs) as it considers to be necessary in connection with the grant of any such permission.

Protected Species – Where there is reason to believe that protected species may be located within or adjacent to a development site, the Planning Authority may deem it necessary for an application to be accompanied by additional supporting information in order to allow proper assessment of any likely impact as a result of development. For further guidance in relation to protected species, applicants should consult the City Council's published Supplementary Guidance on **Natural Heritage**; and **Bats and Development**.

Where works would affect a listed building, it may be necessary to apply for a separate consent for those works, called Listed Building Consent. This consent is independent from ordinary planning permission, and may be required in addition to planning permission. Where both consents are necessary, the applicant must obtain both consents before work can begin.

In assessing any application for Listed Building Consent, the emphasis is placed on preserving the historic character of the building(s) in question. Applications can be made online via the Scottish Government's e-planning website (www.eplanning.scotland.gov.uk) or direct to Aberdeen City Council using the application forms available on our own website. For advice on whether Listed Building Consent will be necessary for your proposal, please contact Aberdeen City Council's Development Management section on 01224 523 470 or by email via pi@aberdeencity.gov.uk. In considering proposals for Listed Building Consent, Conservation Area Consent or planning permission for development which may affect the historic environment, the planning authority will take into account Scottish Planning Policy (SPP), the Scottish Historic Environment Policy (SHEP) and the Managing Change in the Historic Environment guidance note series published by Historic Scotland.

Planning legislation requires that certain applications are advertised in the local press. Applications for Listed Building Consent or planning applications that affect the setting of a listed building will be advertised, while those located within a Conservation Area may be advertised depending on the potential impact of the proposal. There is no charge to the applicant in such instances. Advertisement is also required where it has not been possible to issue notification because there are no properties on adjacent land, and for this the cost will be borne by the applicant.

▪ GENERAL PRINCIPLES

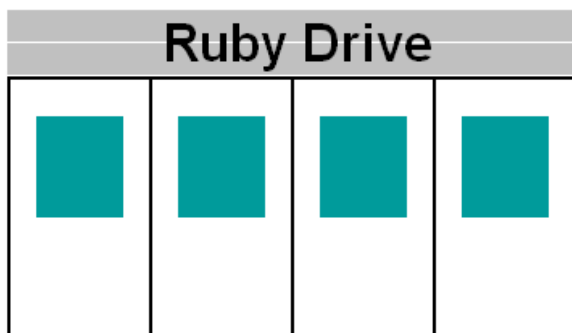
Elsewhere in this document, guidelines are set out in relation to specific types of development, such as house extensions, porches etc. In addition to those specific criteria, the following principles will be applied to **all** applications for householder development:

1. Proposals for extensions, dormers and other alterations should be architecturally compatible in design and scale with the original house and its surrounding area. Materials used should be complementary to the original building. Any extension or alteration proposed should not serve to overwhelm or dominate the original form or appearance of the dwelling.
2. Any extension or alteration should not result in a situation where amenity is 'borrowed' from an adjacent property. Significant adverse impact on privacy, daylight and general residential amenity will count against a development proposal.

3. Any existing extensions, dormers or other alterations which were approved prior to the introduction of this supplementary guidance will not be considered by the planning authority to provide justification for a development proposal which would otherwise fail to comply with the guidance set out in this document. This guidance is intended to improve the quality of design and effectively raise the design standards and ground rules against which proposals will be measured.
4. The built footprint of a dwelling house as extended should not exceed twice that of the original dwelling.
5. No more than 50% of the front or rear curtilage shall be covered by development.

▪ **REAR & SIDE EXTENSIONS**

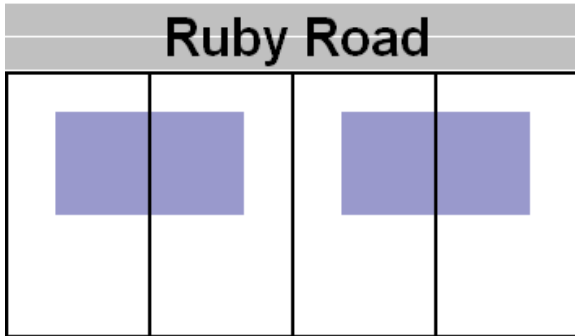
In addition to the design considerations noted above, the planning authority shall continue to apply guidelines relating to specific types of dwellings, as follows. Where dimensions are stated for projection of extensions, these should be measured from the rearmost original part of the main building, and should not include any store or outhouse which did not originally contain any internal living accommodation. Where an extension is proposed as part of a steading conversion, the proposal will be assessed primarily against the Council’s published Supplementary Guidance on *‘The Conversion of Steadings and other Non-residential Vernacular Buildings in the Countryside’*.



1. Detached Dwellings

- a) The maximum dimensions of any single-storey extension will be determined on a site-specific basis.
- b) On detached properties of 2 or more storeys, two storey extensions will generally be possible, subject to the considerations set out in the ‘General Principles’ section, above.

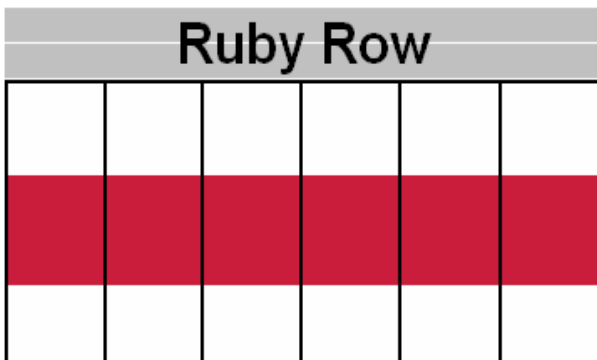
2. Semi-detached Dwellings



- a) Single storey extensions will be restricted to 4m in projection along the boundary shared with the other half of the semi-detached property. In all other cases, the maximum size of single storey extension will be determined on a site-specific basis, with due regard for the topography of the site and the relationship between buildings.

- b) On properties of 2 or more storeys, two storey extensions may be possible, subject to the design considerations set out in the 'General Principles' section, above. The projection of two-storey extensions will be restricted to 3m along the boundary shared with the other half of the semi-detached property.

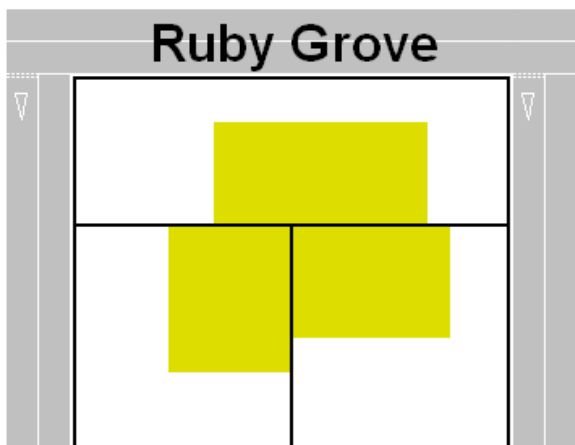
3. Conventional Terraced Dwellings



- (a) Single storey extensions to terraced dwellings will be restricted to 3m in projection along a mutual boundary.
- (b) Two storey extensions will normally be refused where the proposal runs along a mutual boundary. There will generally be limited scope for the addition of two-storey extensions to terraced properties.

- (c) Proposals for extensions to end-terrace properties will be subject to these standards unless it can be demonstrated that the specific circumstances of the site and the proposal justify a departure from the above.

4. Grouped Terraces



- (a) Extensions should not project forward of any established building line
- (b) Single-storey extensions to group terrace properties will be restricted to 3m in projection from the rear wall of the original dwelling
- (c) Two-storey extensions to grouped terrace properties will not normally be acceptable

▪ FRONT EXTENSIONS / PORCHES

The Council has developed the practice, when considering proposals for porch extensions in front of a formal building line, of limiting such structures to the minimum size necessary for protection from storms.

The practice which has become established is intended to preserve the consistent architectural form of a terrace, maintain an uncluttered street scene and to ensure that light and prospect are not lost to neighbouring properties. Recent changes to permitted development legislation allow the construction of porches in certain prescribed instances. In assessing applications of this nature, the following will apply;

a) Front extensions of any type should be of a scale and design which is complementary to, and consistent with, the original dwelling. Modest porches will generally be acceptable, but these should not incorporate additional rooms (e.g. toilet, shower room), and should not detract from the design of the original building or the character of the street.

b) In all cases, careful consideration will be given to (i) impact on adjacent property; (ii) visual impact; and (iii) the extent of any building line and the position of the adjacent buildings generally.

c) Within a Conservation Area, it will not be permitted to add a front extension to any property which forms part of an established building line.

d) Given the wide variety of house types across the city and the existence of 'dual-frontage' dwellings, it will be for the planning authority to determine which elevation forms the principal elevation of a dwelling for the purposes of this guidance.

e) It may be permissible to incorporate bay windows on front elevations, subject to an appropriate restriction in depth and an acceptable design outcome which will complement the original property. The design and scale of such extensions should reflect that of the original dwelling, and should not be utilised as a means to secure significant internal floorspace.

f) Any front extension should incorporate a substantial proportion of glazing, in order to minimise its massing and effect on the streetscape.

▪ DORMER WINDOWS AND ROOF EXTENSIONS

Recent changes to the Permitted Development rights available to householders allow for the addition of dormer windows (subject to criteria regarding position in relation to a road, distance from site boundaries etc) to properties outwith Conservation Areas. Nevertheless, such alterations can have a significant impact upon the character of a property and the wider streetscape, and so careful consideration of proposals remains important.

As a basic principle, new dormer windows or roof extensions should respect the scale of the building and they should not dominate or tend to overwhelm or unbalance the original roof. The purpose of this design guide is to assist those intending to form, alter or extend dormer windows in their property, in formulating proposals which are likely to be considered favourably by the planning authority. Situations may arise where the extent of new dormers or roof extensions will be considered excessive. There may also be situations where any form of roof extension or dormer will be considered inappropriate e.g. on a very shallow pitched roof with restricted internal headroom. It is recommended therefore that advice from the planning authority is obtained before submitting a formal application for any consent.

A series of general guidelines are outlined below, and are followed by further guidelines which will be applied to older properties of a traditional character and modern properties respectively.



Above: Examples of the variety of dormer types to be seen around Aberdeen

Below: Situation where roof pitch is too shallow to comfortably accept any type of dormer or roof extension



Example of a poorly designed roof extension – Dormers are too large, dominating the roof slope, and use of substantial infill panels and slated aprons contributes to bulky appearance

Dormer Windows: General Principles

The following principles will normally apply in all cases:

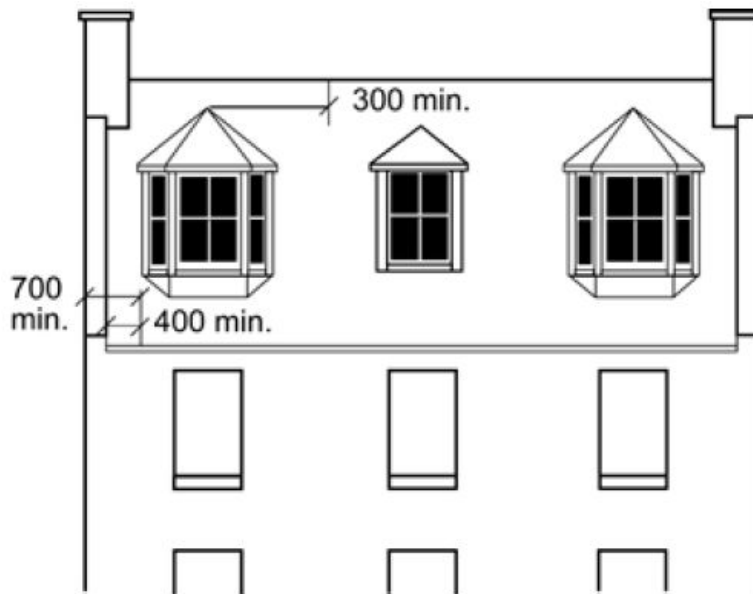
- a) On traditional properties, original dormers must be retained and repaired, and their removal and/or replacement with larger or modern dormers will not be permitted;
- b) The removal of inappropriate earlier dormers and roof extensions, and their replacement by architecturally and historically accurate dormers will be actively encouraged;
- c) In terraces or blocks of properties of uniform design where there are no existing dormers, the construction of new dormers will not be supported on the front or other prominent elevations (e.g. fronting onto a road);
- d) On individual properties or in terraces where there are existing well-designed dormers and where there is adequate roof space, the construction of new dormers which match those existing may be acceptable. Additional dormers will not be permitted however, if this results in the roof appearing overcrowded. These dormers should be closely modelled in all their detail and in their position on the roof, on the existing good examples. They will normally be aligned with windows below;
- e) Box dormers will not be permitted anywhere on listed buildings, nor will the practice of linking existing dormers with vertical or inclined panels; and
- f) In the case of non-listed buildings in conservation areas, consideration may be given to the provision of linked panels between windows on the private side of the building, where the extension is not seen from any public area or is otherwise only visible from distant view. In such cases any linked panel should slope at a maximum of 75° to the horizontal.

Non-traditional style dormers may be accepted on the rear of non-listed buildings in conservation areas, but generally not on the rear or any other elevations of listed buildings.

Dormer Windows: Older properties of a traditional character

1. Front Elevations

- a) On the public elevations of older properties the Council will seek a traditional, historically accurate style of dormer window. In addition, all new dormers will have to be of an appropriate scale, i.e. a substantial area of the original roof must remain untouched and clearly visible around and between dormers. The main principles to be followed are:
- b) Existing original dormers should be retained or replaced on a "*like for like*" basis. Box dormer extensions will not normally be acceptable on the front elevations;
- c) The aggregate area of all dormers and/or dormer extensions should not dominate the original roof slope. New dormers should align with existing dormers and lower windows and doors;
- d) The front face of dormers will normally be fully glazed and aprons below the window will not be permitted unless below a traditional three faceted piended dormer;
- e) Dormers should not normally rise directly off the wallhead. In the case of stone buildings, dormers which rise off the inner edge of the wallhead will generally be acceptable. The position of the dormer on the roof is very important. Dormers which are positioned too high on the roof give the roof an unbalanced appearance
- f) The outer cheek of an end dormer should be positioned at least 700mm in from the face of the gable wall or 1000mm from the verge. Where there is tabling on top of the gable, the cheek should be at least 400mm in from the inside face of the tabling. It is never acceptable for a dormer haffit to be built off the gable or party wall; and
- g) The ridge of any new dormer should be at least 300mm below the ridge of the roof of the original building. If it is considered acceptable for the dormer ridge to be higher than this, it should not nevertheless, breach the ridge or disturb the ridge tile or flashing.

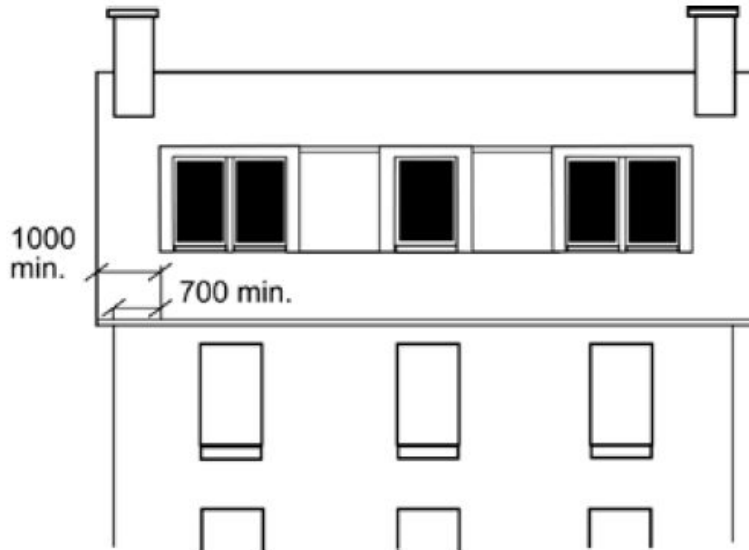


Piended dormers on typical Aberdeen tenement (linking dormers not acceptable on front elevations)

2. Rear Elevations and Exceptions

The guidelines for older properties may be relaxed where a property is situated between two properties which have existing box dormer extensions, or in a street where many such extensions have already been constructed. They may also be relaxed on the non-public (rear) side of a property. In such cases, and notwithstanding the design and finish of neighbouring development, the following minimum requirements will apply:

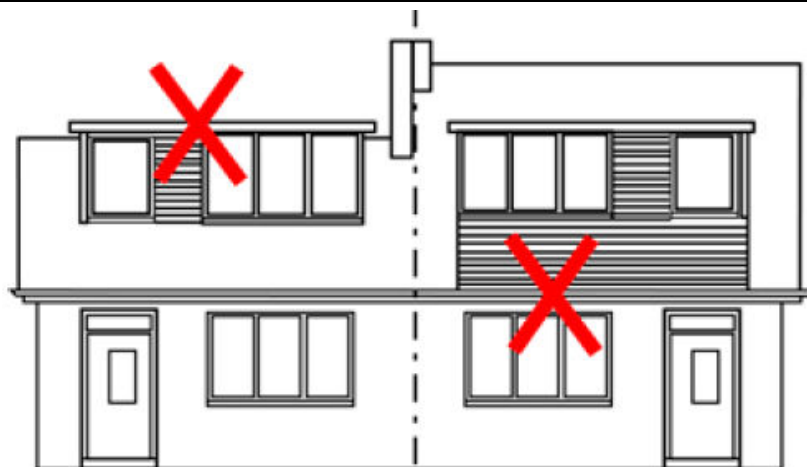
- a) The aggregate area of all dormer and/or dormer extensions should not dominate the original roof slope;
- b) Dormer haffits should be a minimum of 400mm in from the inside face of the gable tabling;
- c) The front face of dormer extensions should be a minimum of 400mm back from the front edge of the roof, but not so far back that the dormer appears to be pushed unnaturally up the roof slope.
- d) Flat roofs on box dormers should be a reasonable distance below the ridge;
- e) Windows should be located at both ends of box dormers;
- f) A small apron may be permitted below a rear window; and
- g) Solid panels between windows in box dormers may be permitted but should not dominate the dormer elevation.



Flat roofed box dormer (normally only acceptable on rear elevations)

Dormer Windows: Modern Properties

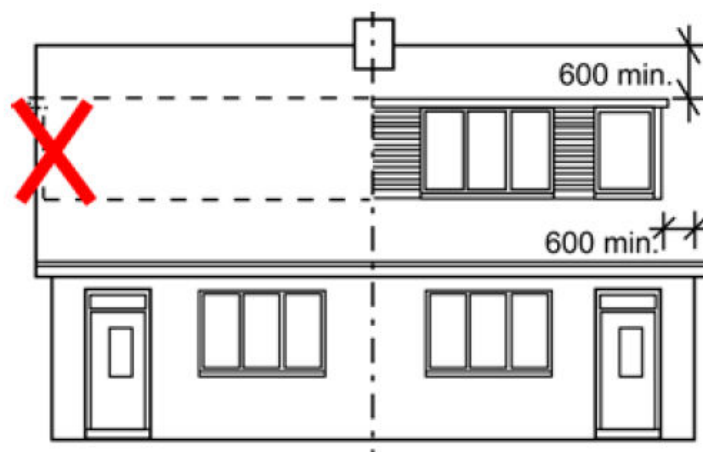
- a) Dormers and box dormer extensions have become common features in many modern housing areas, and the wide variety of designs of modern dwellings necessitates a greater flexibility in terms of design guidance. The amenity of other properties and the residential neighbourhood must however, still be protected, with the integrity of the building being retained after alteration. The following basic principles may be used to guide the design and scale of any new dormer extension:
- b) The dormer extension should not appear to dominate the original roofspace.
- c) The dormer extension should not be built directly off the front of the wallhead as the roof will then have the appearance of a full storey. On public elevations there should be no apron below the window, although a small apron may be acceptable on the rear or non-public elevations. Such an apron would be no more than three slates high or 300mm, whichever is the lesser;



Dormer extension should not extend to or breach ridge (roof too shallow)

Dormer extensions should not be built off front of wall head or include apron

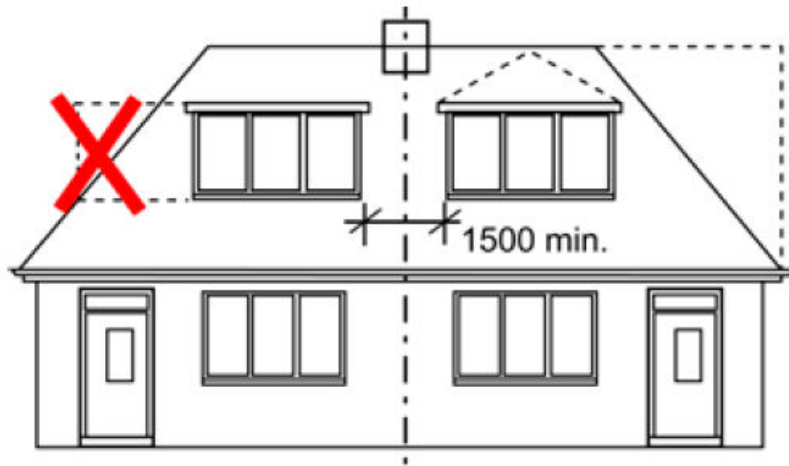
- d) The roof of the proposed extension should not extend to, or beyond the ridge of the existing roof, nor should it breach any hip. Dormer extensions cannot easily be formed in hipped roofs. Flat roofed extensions should generally be a minimum of 600mm below the existing ridge;
- e) The dormer extension should be a minimum of 600mm in from the gable. The dormer haffit should never be built off the gable or party walls, except perhaps in the situation of a small semi-detached house where the dormer extension may sometimes be built off the common boundary. In terrace situations, or where a detached or semi-detached bungalow is very long, dormer extensions should be kept about 1500mm apart (i.e. dormer haffits should be 750mm back from the mutual boundary) so as not to make the dormer appear continuous or near continuous;



Box dormer extension on small semi-detached house (in this case it is permissible to build up to the party wall). Dormers should not extend out to verge / roof edge.

- f) The outermost windows in dormer extensions should be positioned at the extremities of the dormer. Slated or other forms of solid panel will not normally be acceptable in these locations. In the exception to this situation, a dormer on a semi-detached house may have a solid panel adjacent to the common boundary when there is the possibility that the other half of the house may eventually be similarly extended in the foreseeable future. In this case the first part of the extension should be so designed as to ensure that the completed extension will eventually read as a single entity;
- g) There should be more glazing than solid on the face of any dormer extension.
- h) Box dormer extensions should generally have a horizontal proportion. This need not apply however, to flat roofed individual dormer windows which are fully glazed on the front;

- i) Finishes should match those of the original building and wherever possible the window proportion and arrangement should echo those on the floor below:
- j) The design of any new dormer extension should take account of the design of any adjoining dormer extension.

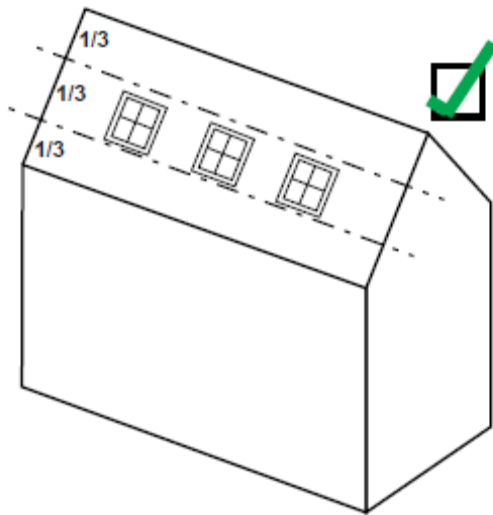


Flat roofed dormers on more traditional hipped roof house (Dormers should not breach hips. A pitched roof on this kind of dormer greatly increases its bulk). Extending roof to the gable on one side only is best avoided.

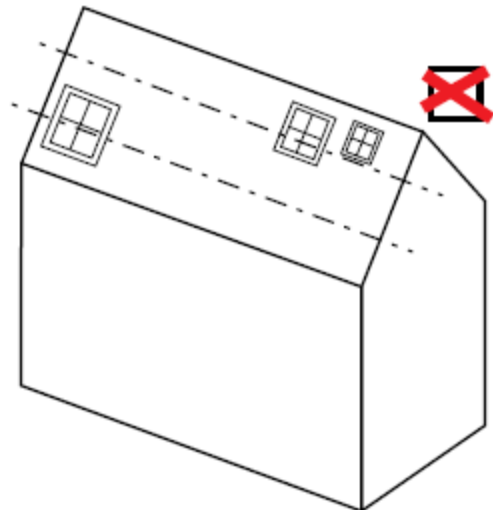
▪ **ROOFLIGHTS**

The installation of rooflights is a simple and cost effective method of allowing additional natural light and ventilation into an attic or roofspace. An excessive use of these rooflights can however, create visual clutter on a roof. Planning Permission is required for the installation of such rooflights on buildings in conservation areas and Listed Building Consent is required for proposals involving alteration of a listed building. When considering the installation of a rooflight, account should be taken of the following:-

a) A rooflight provides considerably more light than a normal vertical window of the same dimension. Many rooflights installed are consequently, larger and more numerous than is really necessary. In a roofspace used only for storage, the smallest rooflight will generally be adequate;



Small rooflights in the middle third of roof space and evenly spaced.



Rooflights too large and too close to eaves and verge. A variety of sizes, spacing and levels.

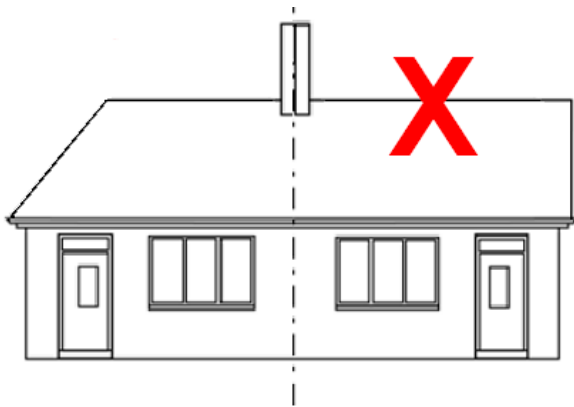
b) Rooflights should have a conspicuously vertical proportion. Seen from ground level, the foreshortening effect will tend to reduce the apparent height of the window, giving it a more squat appearance;

c) On older buildings, and particularly on listed buildings and buildings in conservation areas, a 'heritage' type of rooflight will be expected. This is of particular importance on public elevations. Even the addition of a central glazing bar to a rooflight can provide a more authentic appearance in such instances;

d) Large timber or cast iron rooflights divided into several sections were frequently provided above stairwells. It is not ideal to replace these with a single-pane modern rooflight. If the original rooflight cannot be repaired, aluminum or steel patent glazing is a more satisfactory option; and

- e) For rooflights fitted into slated roofs, manufacturers can provide a special flashing with their rooflights to keep the projection of the rooflight above the plane of the slates to a minimum.
- f) There are available metal roof windows which have an authentic traditional appearance whilst meeting current standards for insulation and draught exclusion.

▪ **OTHER FORMS OF DORMER WINDOW AND ROOF EXTENSION**



Hipped roof extensions

Modifying only one half of a hipped roof is likely to result in the roof having an unbalanced appearance. The practice of extending a hipped roof on one half of a pair of semi-detached houses to terminate at a raised gable will not generally be accepted unless;

- The other half of the building has already been altered in this way; **or**
- Such a proposal would not, as a

result of the existing streetscape and character of the buildings therein, result in any adverse impact on the character or visual amenity of the wider area.

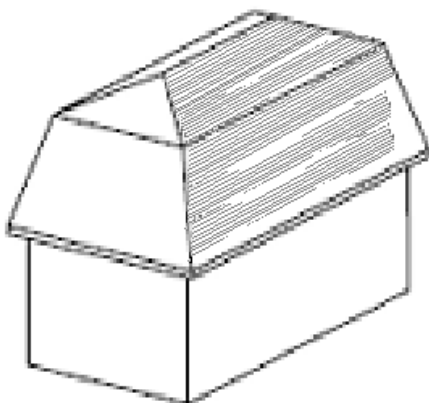
Half dormer windows

Half dormer windows have the lower part of the window within the masonry wall, with the part in the roof space surrounded by masonry or timberwork. This type of window is usually quite narrow, vertical in proportion, and is appropriate when the floor is below the wall-head level.



Wall-head gables

A wall-head gable commonly has a centre window, with flues passing each side within the masonry to a common central chimney. It would be essential for any such feature to be constructed in the same material as the wall below. (Both half dormer window and wall-head gables have a strong visual impact which could substantially alter the character of a building. They are therefore, unlikely to be acceptable on listed buildings, but might be accepted in conservation areas or on other older buildings of a traditional character.)



Mansard Roofs

Mansard roofs are a common, even a somewhat overused method of obtaining additional attic floorspace having standard headroom overall. Mansard roofs tend to have a top heavy appearance on buildings which have only a single storey of masonry, and should be restricted to buildings of two or more masonry storeys. They will not normally be acceptable in semi-detached or terraced situations unless all the other properties in the

group are to be similarly altered at the same time. In effect, few situations will arise where an existing roof can readily be converted to a mansard roof.

On the occasions when a mansard roof solution is acceptable, considerable attention to detail is required to ensure that the altered roof is visually authentic. The following points should be observed:

- a) There should be no fascia at the eaves, nor should the mansard project forward of the masonry line;
- b) The mansard should be taken down to either a concealed lead gutter behind a masonry parapet, or to an "ogee" or half round cast iron gutter in line with the face of the masonry;
- c) The gables of the building should be extended up in the same material as the original gables, and should terminate at a masonry skew in the same profile as the mansard roof. It will not normally be acceptable to return the mansard roof across the gable with hipped corners;
- d) The lower slope of the roof should be inclined at no greater than 75° to the horizontal.

▪ OTHER DOMESTIC ALTERATIONS

Replacement Windows and Doors

Windows and doors are important features of a building that contribute greatly to the character of the building and of the street in which the building stands. They are also increasingly subject to alteration or replacement. Householders are referred to the council's Supplementary Guidance entitled '*Guidance on the Repair and Replacement of Windows and Doors*'.

Satellite Dishes

In all cases, microwave antennas should, as far as is practicable, be sited so as to minimise their visual impact and effect on the external appearance of a building. The cumulative effects of such seemingly minor additions can be significant, particularly within conservation areas and where installed on listed buildings. Permitted development rights exist for the installation of satellite dishes on dwellinghouses outwith Conservation Areas, provided any dish installed would not project more than 1m from the outer surface of an external wall, roof plane, roof ridge or chimney of the dwellinghouse.

For buildings containing flats, satellite dishes may only be installed without planning permission where the site;

1. Lies outwith any Conservation Area
2. Is not within the curtilage of a Listed Building
3. Would not protrude more than 1m from the outer surface of any wall, roof place, roof ridge or chimney.

Where planning permission is required for such works, the Council's duties in relation to listed buildings and conservation areas will be of relevance. Householders should also be aware that, irrespective of the Permitted Development rights set out above, a separate application for Listed Building Consent is likely to be required where installation is proposed within the curtilage of a listed building.

Decking

Homeowners are often unaware that the formation of decking may require planning permission. It is therefore important to discuss any such proposals with the planning authority at an early stage to determine what consents may be necessary and to identify any potential issues with a proposal. The formation of decking will require planning permission in the following instances;

- Any part of the deck would be forward of a wall forming part of the principal elevation, or side elevation where that elevation fronts a road;
- The floor level of any deck or platform would exceed 0.5m in height;
- The combined height of the deck and any wall, fence, handrail or other structure attached to it, would exceed 2.5m;
- If located within a Conservation Area or within the curtilage of a Listed Building, the deck or platform would have a footprint exceeding 4 square metres

Raised decking can in many cases provide a desirable outdoor amenity space, but the impact upon adjacent properties should be given careful consideration. The raised surface of a deck may result in overlooking into neighbouring gardens and a consequent loss of privacy. Equally, enclosing raised decks with additional fencing can result in neighbours being faced with excessively tall boundary enclosures which can affect light in neighbouring gardens.

The following guidelines will be relevant to the assessment of proposals involving raised decking areas;

- a) Proposals should not result in an unacceptable loss of privacy for neighbouring residents.
- b) Proposals should not result in an adverse impact upon the amenity of adjacent dwellings, including both internal accommodation and external private amenity space.
- c) There will be a presumption against the formation of decking to the front of any property, or on any other prominent elevation where such works would adversely affect the visual amenity of the street scene.

Fences, Walls and Other Boundary Enclosures

Boundary enclosures such as fences, gates and walls may not require planning permission, due to the permitted development rights which exist.

- a) Planning permission will always be required for such works to a listed building, or within the curtilage of a listed building.
- b) Planning permission will always be required for such works within a Conservation Area.
- c) Conservation Area Consent may be necessary for the demolition of boundary walls with conservation areas.
- d) In all instances, the scale and form of boundary enclosures should be appropriate to their context and should not detract from the street scene as a result of inappropriate visual impact.
- e) In all instances, proposals for boundary enclosures should not result in an unacceptable impact upon the amenity of neighbouring dwellings.

Driveways

The Council's Supplementary Guidance on *'Transport and Accessibility'* provides guidance on situations where planning permission will be required for such works. This guidance also sets out criteria by which applications for parking areas in Conservation Areas and within the curtilage of Listed Buildings will be assessed.

Planning permission will be required in the following circumstances;

- The property is a flat;
- Construction work involves over 0.5 metres of earthworks (excavation or raising of ground level);
- The verge to the footway has grass over 2.5 metres wide;
- The driveway accesses on to a classified road;
- The property is a listed building or is situated in a conservation area.

Permission will not be granted for a driveway across an amenity area or roadside verge unless it would have no detrimental impact in road safety and would have no adverse effect on the amenity of the area (e.g. involves the loss of mature or semi-mature trees).

For more detailed guidance on proposals involving the formation of a driveway, please consult sections 8 and 9 of the Council's *'Transport and Accessibility'* Supplementary Guidance.

Microrenewables

The term 'micro-renewables' refers to all forms of domestic micro-generation utilising a renewable form of energy. These come in a number of forms, and are increasingly common as the relevant technology evolves and becomes more widely available, efficient, and reliable.

The planning authority aims to encourage the use of micro-renewable technologies within the curtilage of domestic dwellinghouses. Careful consideration is required in relation to their positioning, however, in order to avoid undue prominence within the street scene, particularly within conservation areas and where proposals may affect the setting of a listed building. Installation of such equipment can in many cases be carried out by virtue of Permitted Development rights, which allow for improvements and alterations to dwellinghouses and other works within the curtilage of a dwellinghouse, provided the site is located outwith any designated Conservation Area and does not involve works within the curtilage of a Listed Building. At present there are no permitted development rights available for domestic microgeneration via the installation of wind turbines on a dwellinghouse. In most circumstances, planning permission will be required for the installation of wind turbines elsewhere within the curtilage of a domestic property.

▪ **CHANGE OF USE FROM AMENITY SPACE TO GARDEN GROUND**

Amenity space and landscaping are valued assets within residential areas. They are common features in most housing developments and are provided for a number of reasons including –

- to improve the appearance of the area;
- to provide wildlife habitats, enhance ecology and often form part of sustainable urban drainage systems;
- to act as pedestrian routes through developments;
- to provide informal recreation areas;
- to provide good safety standards for drivers, cyclists and pedestrians in terms of road verges or visibility splays.

Many homeowners seek to purchase areas of such land from either the Council or a housing developer to enlarge their own gardens. In all circumstances this requires planning permission for a change of use from amenity ground to garden ground.

Prior to submitting a planning application it is advisable to contact the landowner to see if they would be willing to sell the particular piece of land. In the case of the Council land you should contact –

Asset Management
Enterprise, Planning and Infrastructure

Business Hub 10
Second Floor South
Marischal College
Broad Street
AB10 1AB

It is also advisable to contact Planning and Sustainable Development prior to submitting your application for planning advice on acceptability of your proposal.

Planning applications will be assessed in the context of Policy H1 (Residential Areas) of the Aberdeen Local Development Plan which states that proposals for householder development will only be approved if they do not result in the loss of valuable open space. Each planning application for change of use is dealt with on its own individual merits, however in considering whether an application is acceptable the Council will assess the proposal against the following criteria –

- The proposal should not adversely affect amenity space which makes a worthwhile contribution to the character and amenity of the area or contains mature trees that make a significant contribution to the visual amenity of the wider neighbourhood. In most circumstances the amenity ground will make a contribution, however sometimes small incidental areas of ground make little contribution to the appearance of the neighbourhood. For instance it may be acceptable to include within garden ground secluded areas that are not visible from footpaths or roads and that do not make a contribution to the wider visual amenity of the area. Similarly it may be acceptable to include small corners of space that can be logically incorporated into garden ground by continuing existing fence lines.
- The proposal should not fragment or, if replicated, be likely to incrementally erode larger areas of public open space or landscaping.
- The proposal should not worsen or create a deficiency in recreational public open space in the area. The less amenity space there is in an area the more value is likely to be placed on the existing amenity space. The Open Space Audit identifies areas of the city where there is a deficiency and should this be the case there will be a presumption against the granting of planning permission.
- The proposal should not result in any loss of visual amenity including incorporating established landscaping features such as mature trees or trees that make a significant contribution to the area. It is unlikely the Council would support the incorporation and likely loss of such features, however in circumstances where it is acceptable replacement planting to compensate will normally be required.
- The proposal should not result in an irregular boundary layout that would be out of keeping with the otherwise uniform character of the area.

- The proposal should not result in the narrowing of footpath corridors or lead to a loss of important views along such footpaths, making them less inviting or safe to use.
 - The proposal should not prejudice road or pedestrian safety. Areas of amenity space often function as visibility splays for roads and junctions.
 - The proposal should not give rise to the setting of a precedent that would make it difficult to resist similar proposals in the future. Over time the cumulative impact of the loss of separate areas of ground can lead to the gradual erosion of amenity space, which is not in the public interest and can affect the overall amenity and appearance of the area.
- **HOUSES IN MULTIPLE OCCUPATION (HMOs)**

Presently the term House in Multiple Occupation (HMO) is not one commonly associated with the planning system in Scotland. The term is not defined in planning legislation, though Scottish Government Circular 8/2009 does provide some advice on HMOs, suggesting that there may be a role for the planning system in managing HMOs where a material change in the use of a house or flat has taken place. Multiple occupancy can intensify pressure on amenity, particularly with regards to shared/mutual areas and car parking. It is therefore appropriate to ensure that appropriate provision is made prior to granting planning permission for an HMO.

A useful starting point is to clearly identify what constitutes an HMO for the purposes of this Supplementary Guidance. The planning system defines 'dwellinghouse' and 'flat' as detailed below;

Flat "means a separate and self contained set of premises whether or not on the same floor and forming part of a building from some other part which it is divided horizontally." Part 1 (2) Town and Country Planning (General Permitted Development) (Scotland) Order 1992)

A house is defined within class 9 (houses) under the Town and Country Planning (Use Classes) (Scotland) Order 1997. This allows for use as;

- a) *A house, other than a flat, whether or not as a sole or main residence, by-*
 - (i) *A single person or by people living together as a family; or*
 - (ii) *Not more than 5 residents living together including a household where care is provided for residents*
- b) *as a bed and breakfast establishment or guesthouse, where at any one time not more than 2 bedrooms are, or in the case of premises having less than 4 bedrooms, 1 bedroom is, used for that purpose.*

This means that, where more than 5 persons are living together, other than as a family, the premises would not fall within the definition of a 'dwellinghouse' for planning purposes. It is reasonable to use this same threshold as the point at which a material change in the use of premises has occurred, and an application for change of use to form an HMO would be necessary.

Where flats are concerned, planning legislation does not specify any number of residents above which premises will no longer be considered a 'flat' for planning purposes. Given the potential for increased pressure on amenity, particularly in shared/mutual areas and car parking, it is necessary for this guidance to set a threshold above which use will no longer be considered as a 'flat'. HMOs account for a significant proportion of the available rental accommodation in Aberdeen, and are particularly important in supporting the City's sizeable student population. In setting a threshold above which planning permission will be necessary, it is noted that any number of people may live together in a single property, provided they are part of the same family unit. Taking this into account, it is considered that 6 or more unrelated people living together in a flat would be materially different from family use. This will be the threshold used for the purposes of this guidance.

Planning permission will be required for change of use to a House in Multiple Occupation in the following instances;

- 1. The occupation of a house by 6 or more unrelated persons**
- 2. The occupation of a flat by 6 or more unrelated persons**

It is important to note that separate licensing requirements exist for the establishment of an HMO, irrespective of the planning-specific guidance set out in this document. The granting of planning permission does not remove any requirement to obtain the appropriate licence and vice versa. Furthermore, success in obtaining planning permission for use of premises as an HMO does not guarantee a successful license application. It should be noted that, while the term 'HMO' is common to both systems, it has a different meaning depending on the context in which it is used. For licensing purposes, an HMO is defined as any house or flat which is the principal residence of three or more people who are members of three or more families.

This guidance is intended to set the thresholds at which a house or flat will no longer be considered to be in domestic use and will be treated as a House in Multiple Occupation for planning purposes. Having identified where such changes of use take place, it is then necessary to set out the factors which will be considered in assessing any such application.

Proposals involving formation of an HMO as defined in this guidance will be assessed with regard to matters including, but not limited to, the following;

1. Any adverse impact upon pedestrian or road traffic safety as a result of increased pressure on car parking;
2. Significantly adverse impact upon residential amenity for any reason. This may include, but not be limited to, adequate provision of refuse storage space, appropriate provision of garden ground/amenity space, and an appropriate level of car parking.
3. An excessive concentration of HMOs in a given locality, cumulatively resulting in a material change in the character of that area. This will be assessed in consultation with the Council's HMO Unit within the Housing & Environment service, who hold relevant information on the location of existing licensed HMO properties.

Where it is not practicable for dedicated car parking to be provided alongside the development, a proposal must not exacerbate existing parking problems in the local area.

APPENDIX A: GLOSSARY OF TERMS

Amenity - The attributes which create and influence the quality of life of individuals or communities.

Amenity space - Areas of open space such as gardens, balconies and roof terraces.

Article 4 direction – Some types of development do not need planning permission by virtue of permitted development rights. An Article 4 Direction is an order made by Scottish Ministers which suspends (for specified types of development) the general permission granted under the Town and Country (General Permitted Development) (Scotland) Order 1992 (as amended), thereby removing permitted development rights.

Bay window - a window or series of windows forming a bay in a room and projecting outward from the wall externally

Boundary enclosure – Boundary treatment such as a fence, wall, hedge, ditch or other physical feature which demonstrates the edges of a site or otherwise encloses parts of that site

Building line - The line formed by the frontages of buildings along a street. For the purposes of this guidance, this shall not generally include elements such as the front of any porches, canopies, garages or bay windows.

Common boundary – A boundary which is shared by residential properties on either side

Conditions – Planning conditions are applied to the grant of planning permission and limit and control the way in which a planning consent may be implemented. Such conditions can require works to be carried out in a certain way (e.g. restriction on opening hours or adherence to an approved tree management plan) or can require submission of further information in order to demonstrate the suitability of technical details (e.g. drainage or landscaping schemes for a new development)

Conservation Area – Conservation Areas are areas of special architectural or historical interest, the character or appearance of which it is desirable to preserve or enhance. Such areas are designated by the local planning authority. Details of the Conservation Areas in Aberdeen can be found on the Council's website, www.aberdeencity.gov.uk.

Conservation Area Consent – Conservation Area Consent is required for proposals which involve the whole or substantial demolition of any unlisted building or structure in a Conservation Area. Conservation Area Consent is not required for the demolition of a building which has a volume of less than 115 cubic metres, or for the partial demolition of a building, or for minor alterations to gates, walls and fences within a Conservation Area. Demolition

works may, however, require planning permission, and so confirmation should be sought from the planning authority.

Curtilage - The land around, and belonging to, a house.

Daylight – Diffuse level of background light, distinct from direct sunlight

Development Plan – The “Development Plan” is a term used to incorporate both the current Local Plan/Local Development Plan and the current Structure Plan/Strategic Development Plan.

Dormer Window – Dormer windows are a means of creating useable space in the roof of a building by providing additional headroom.

Dwellinghouse – For the purposes of this guidance, the term “dwellinghouse” does not include a building containing one or more flats, or a flat contained within such a building

Fenestration - The arrangement of the windows in a building.

Gable - The part of a wall that encloses the end of a pitched roof.

Habitable rooms - Includes bedrooms and living rooms, but does not include bathrooms, utility rooms, WCs or kitchens when not accompanied by dining facilities.

Haffit – The sides or ‘cheeks’ of a dormer window.

Hipped Roof – A four-sided roof having sloping ends as well as sloping sides

Listed Building – Working on behalf of Scottish Ministers, Historic Scotland inspectors identify buildings which are worthy of statutory protection. These are ‘Listed Buildings’. The criteria by which the Scottish Ministers define the necessary quality and character under the relevant legislation are broadly; Age and Rarity; Architectural Interest; and Close Historical Association

Listed building Consent – Listed Building Consent is obtained through an application process which is separate from, but runs parallel to, that by which planning permission is obtained. This separate regulatory mechanism allows planning authorities to ensure that changes to listed buildings are appropriate and sympathetic to the character of the building. Listed Building Consent must be obtained from the planning authority if you wish to demolish, alter or extend, either internally or externally, a listed building.

Mansard Roof – A four-sided roof having a double slope on all sides, with the lower slope much steeper than the upper.

Material Consideration - Any issue which relates to the use and development of land and is relevant to the planning process.

Permitted Development - an aspect of the planning system which allows people to undertake specified forms of minor development under a deemed grant of planning permission, therefore removing the need to submit a planning application.

Piended – scots term for hipped (pronounced peended)

Planning Authority – This is the term given to the Council in its role exercising statutory functions under Planning legislation. Authorities have three main planning duties: Development Management (assessing and determining planning applications); Development Planning (preparing, updating and monitoring the authority's Local Plan/Local Development Plan); and Enforcement (seeking to investigate and resolve breaches of planning control)

Porch - A covered shelter projecting in front of the entrance of a building.

Roads Authority - This is the term given to the Council in its role exercising statutory functions under Roads legislation. Where trunk roads are concerned, Transport Scotland is the relevant roads authority.

Sunlight – The sun's direct rays, as opposed to the background level of daylight

Supplementary Guidance – Supplementary Guidance is prepared by the planning authority in support of its Local Plan/Local Development Plan. These documents are generally intended to provide greater detail or more specific and focused guidance than might be practicable within the Plan itself.

Tabling – A raised horizontal surface or continuous band on an exterior wall; a stringcourse

Tree Preservation Order – The planning authority has the powers to make Tree Preservation Orders if it appears to them to be a) expedient in the interest of amenity and/or b) that the trees, groups of trees or woodlands are of a cultural or historical significance. The authority has duties to a) make such TPOs as appear to the authority to be necessary with any grant of planning permission; and b) from time to time to review any TPO and consider whether it is requisite to vary or revoke the TPO.

Wallhead – The uppermost section of an external wall.

APPENDIX B: APPLICATION CHECKLIST GUIDE

Have you discussed the proposed works with your neighbours?	
Is planning permission required? Remember, some works can be carried out as 'Permitted Development'	
Is any other form of consent required for the works?	
Have you considered the appointment of an architect, planning consultant or other agent to act on your behalf? Though not mandatory, this can be worthwhile as agents will be familiar with the planning system and should be able to provide the drawings and supporting information to the necessary standards.	
Will any supporting information be necessary to enable the planning authority to make a full assessment of issues relevant to the proposal? For example, are there trees or protected species within the site?	
Is the building a Listed Building or within a Conservation Area? If so, it is recommended that advice is sought from the planning authority prior to submission in order to gauge the potential impact on these designations.	
Have you considered your proposal in relation to the guidance contained within the Householder Development Guide? Any proposal for householder development will be assessed against this Supplementary Guidance	
Is the proposed design consistent with the character of the property and the surrounding area?	
Would the development proposed result in any significant adverse impact on your neighbours in terms of loss of light, overshadowing and/or privacy?	
Would the proposed development result in an insufficient provision of amenity space/private garden?	
Have any changes to access and/or parking requirements been discussed with the Council in its role as Roads Authority?	

APPENDIX C: DAYLIGHT AND SUNLIGHT

Daylight

It is appropriate to expect that new development will not adversely affect the daylighting of existing development. Residents should reasonably be able to expect good levels of daylighting within existing and proposed residential property.

A useful tool in assessing the potential impact of proposed development upon existing dwellings is the BRE Information Paper on 'Site Layout Planning for Daylight'. This document sets out techniques which can be applied as a means of assessing the impact of new development upon daylighting. These techniques should only be applied to "habitable rooms", which for the purposes of this guidance shall mean all rooms designed for living, eating or sleeping eg. lounges, bedrooms and dining rooms/areas. Kitchens without dining areas are not considered as habitable rooms.

For domestic extensions which adjoin the front or rear of a house, the 45° method will be applied in situations where the nearest side of the extension is perpendicular (at right-angles to) the window to be assessed. The 45° method is not valid for windows which directly face the proposed extension, or for buildings or extensions proposed opposite the window to be assessed. In such instances, the 25° method, also detailed below, may be appropriate.

It should be noted that these guidelines can only reasonably be applied to those buildings which themselves are good neighbours, standing a reasonable distance from the boundary and taking only their fair share of light. Existing windows which do not meet these criteria cannot normally expect the full level of protection. It is important to note that these tools will be used as and when the planning authority deems it appropriate due to a potential impact on daylight to an existing dwelling. The results of the relevant daylighting assessment will be a material consideration in the determination of an application, and should not be viewed in isolation as the sole determining factor.

The 45° Method for daylight

This method involves drawing 45° lines from the corner of a proposed building or extension in both plan and section views. If the shape formed by **both** of these lines would enclose the centre point of a window on an adjacent property, the daylighting to that window will be adversely affected.

DIAGRAM 1: 45° METHOD

The line drawn at 45° would pass through the mid-point of the window on elevation drawing, but not on the plan. This extension would therefore satisfy the 45° method for daylighting assessment. Were the proposal to fail on both diagrams, it is likely there would be an adverse affect on daylight to the adjacent window of the neighbouring property.

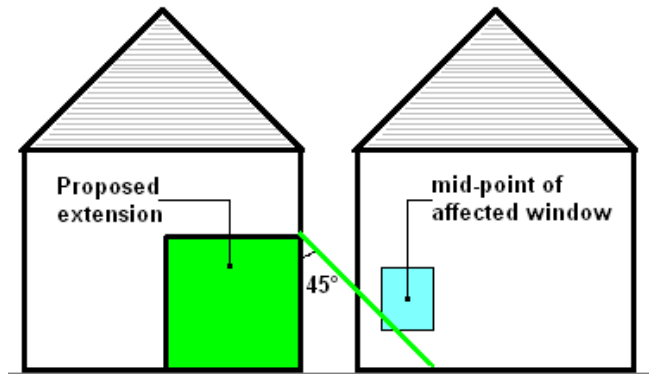


Fig A: Elevation view

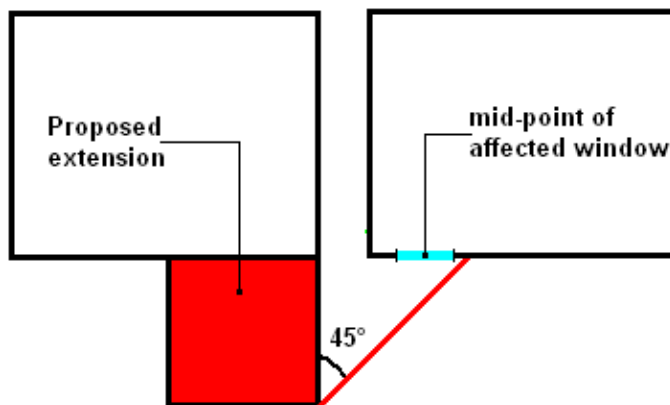


Fig B: Plan view

The 25° Method

The 25° method should be applied in situations where existing windows would directly face the proposed building or extension. Firstly, a section should be drawn, taken from a view at right angles to the direction faced by the windows in question. On this section, a line should be drawn from the mid-point of the lowest window, 25° to the horizontal, towards the obstructing building or extension. If the proposed building or extension is entirely below this line, it is unlikely to have a substantial effect on the diffuse daylighting of the existing building. Where the 25 degree approach is not satisfied, it will be for the planning authority to make a judgement on the degree of impact upon an adjacent dwelling.

DIAGRAM 2: 25° METHOD

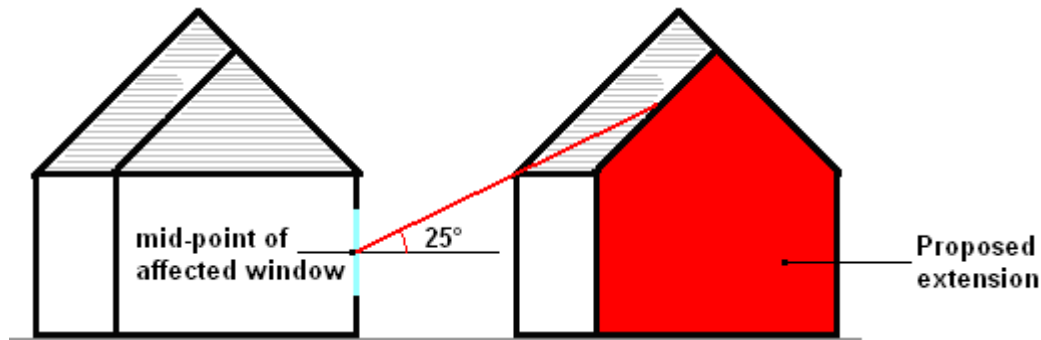


Fig A: Proposed extension may result in loss of daylight to adjacent window of a habitable room

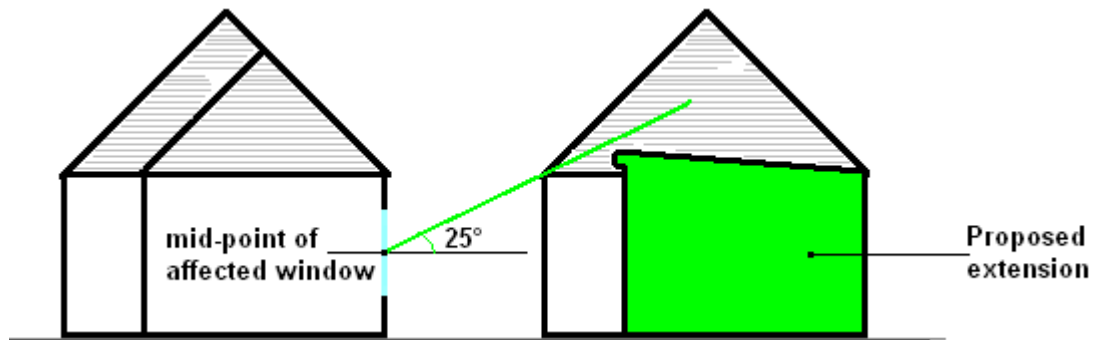


Fig B: Proposed extension would not result in loss of daylight to adjacent window of a habitable room

Both diagrams show line drawn from mid-point of affected window, at 25° to the horizontal.

Sunlight

In many instances, extensions to residential property will have at least some effect on the level of direct sunlight which falls on adjacent land or buildings. Where such overshadowing is excessive, substantial areas of land or buildings may be in shade for large parts of the day, resulting in a significant impact on the level of amenity enjoyed by residents. It is therefore helpful to have some means by which an assessment of any potential overshadowing can be made.

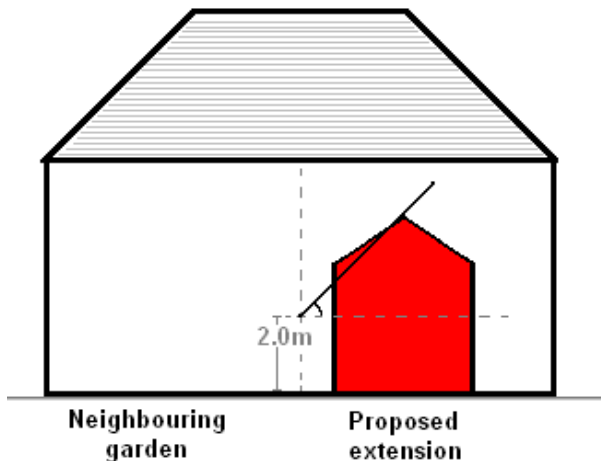
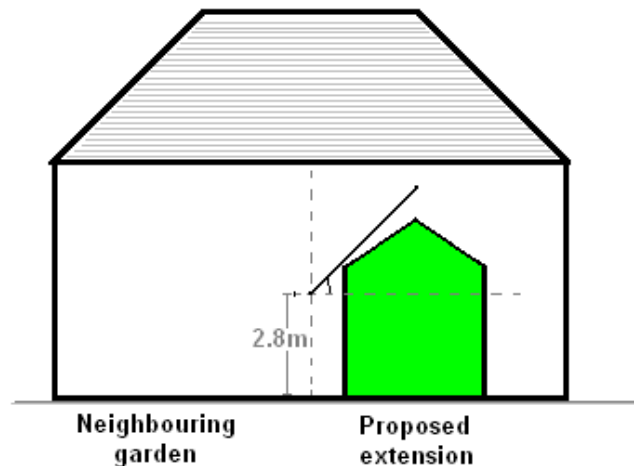
The method used involves drawing a line at 45 degrees to the horizontal. This line will begin at a point above ground level on the relevant boundary. The height above ground level will be determined by the orientation of the proposed building or structure relative to the affected space, as shown in the table opposite;

Orientation of extension relative to affected space	Height from which 45 degree line should be taken
N	4m
NE	3.5m
E	2.8m
SE	2.3m
S	2m
SW	2m
W	2.4m
NW	3.3m

This method is intended as a tool to assist case officers in their assessment of potential overshadowing, and it is important that this be applied sensibly and with due regard for the context of a particular site. Where a proposal is not able to satisfy the requirements of the relevant test, it will then be appropriate for officers to consider other factors relevant to the likely impact on amenity. These will include, but will not be limited to: the proportion of amenity space/garden affected; the position of the overshadowed area relative to windows (of habitable rooms) of an adjacent property; and the nature of the space affected (e.g. overshadowed driveway).

Example 1: In this example (right), the proposed extension would be located to the East of the neighbouring garden ground. A point 2.8m above ground level, on the site boundary, is found. From this point, a line is drawn at 45 degrees to the horizontal.

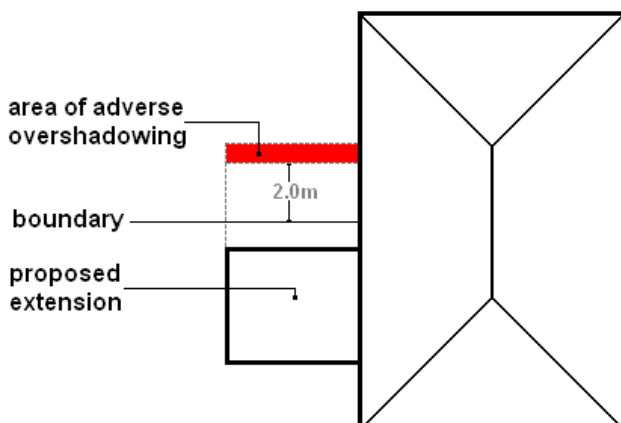
The diagram in Example 1 shows that the line drawn would not strike any part of the proposed extension, and therefore for the purposes of this test there would be no adverse affect on sunlight to the neighbouring garden.



Example 2: In this second example (left), the proposed extension would be constructed to the south of the adjacent garden ground. The same process is followed, but in this instance the line is drawn from a point 2m above ground level.

As the first diagram shows, the proposed extension would intersect the 45 degree line drawn. This suggests that there would be an area of adverse overshadowing in the neighbouring garden as a result of this proposal.

The second diagram demonstrates the area of adjacent garden ground which would be affected in plan view. This allows the case officer to make an assessment of the proportion of garden affected relative to the total useable garden area. As mentioned previously, the nature of the affected area will also be of relevance in



determining whether there is justification in allowing a proposal which does not satisfy the 45 degree test for sunlight. There will be instances where proposals will be approved on this basis.

Appendix D: Privacy

New development should not result in significant adverse impact upon the privacy afforded to neighbouring residents, both within dwellings and in any private garden ground/amenity space. What constitutes an acceptable level of privacy will depend on a number of factors. The purpose of this guide is not to create a rigid standard which must be applied in all instances, but rather to set out the criteria which will be taken into account in determining the impact of a particular development.

It is common practice for new-build residential development to ensure a separation distance of 18m between windows where dwellings would be directly opposite one another. Given the application of this distance in designing the layout of new residential development, it would appear unreasonable to then apply this to residential extensions to those same properties.

Assessment of privacy within adjacent dwellings will therefore focus upon the context of a particular development site, taking into account the following factors:

- existing window-to-window distances and those characteristic of the surrounding area;
- any existing screening between the respective windows;
- appropriate additional screening proposed
- respective site levels
- the nature of the respective rooms (i.e. are windows to habitable rooms); and
- orientation of the respective buildings and windows.

Any windows at a distance of 18m or more will not be considered to be adversely affected through loss of privacy. At lesser distances, the factors stated above will be considered in order to determine the likely degree of impact on privacy. For the purposes of this guidance, habitable rooms constitute all rooms designed for living, eating or sleeping eg. lounges, bedrooms and dining rooms/areas.

Any windows to habitable rooms should not look out directly over, or down into, areas used as private amenity space by residents of adjoining dwellings. In these circumstances the windows of non-habitable rooms should be fitted with obscure glass.

The addition of balconies to existing residential dwellings will require careful consideration of their potential impact upon privacy. Such additions, if poorly considered, can result in significant overlooking into adjacent gardens. Any

proposed balcony which would result in direct overlooking of the private garden/amenity space of a neighbouring dwelling, to the detriment of neighbours' privacy, will not be supported by the planning authority.



Equality and Human Rights Impact Assessment - the Form

There are separate guidance notes to accompany this form – “Equality and Human Rights Impact Assessment – the Guide.” Please use these guidance notes as you complete this form. Throughout the form, **proposal** refers to policy, strategy, plan, procedure or report.

STEP 1: Identify essential information

1. Committee Report No.

2. Name of proposal.

3. Officers completing this form.

Name	Designation	Service	Directorate
Gavin Evans	Planner	Planning & Sustainable Development	Enterprise, Planning & Infrastructure

4. Date of Impact Assessment.

5. When is the proposal next due for review?

6. Committee Name.

7. Date the Committee is due to meet.

8. Identify the Lead Council Service and who else is involved in the delivery of this proposal. (for example other Council services or partner agencies)

The intended proposal document is to be used primarily by officers within the Council's Planning & Sustainable Development Service as an important tool in the assessment and determination of planning applications. The guidance contained within the document may also be of use to other Council services when considering improvements/alterations to Council-owned residential property.

9. Please summarise this Equality and Human Rights Impact Assessment, (EHRIA). This must include any practical actions you intend to take / have taken to reduce, justify or remove any adverse negative impacts (if necessary continue on blank sheet of paper). **Please return to this question after completing EHRIA.**

Assessment of the proposed new Supplementary Guidance has indicated that there would be no adverse negative impacts on any specific equality target groups or related equality strands. The guidance will be applied consistently to all service users, regardless of matters of race, sexuality, gender, age, etc. It is therefore not necessary to take any action to reduce, justify or remove such adverse impacts.

10. Where will you publish the results of the Equality and Human Rights Impact Assessment? Tick all that apply.

- Summary of EHRIA will be published in committee report under section "Equality Impact Assessment"
 ✓ Full EHRIA will be attached to the committee report as an appendix
 Summary of EHRIA to be published on Council website within relevant service pages

STEP 2: Outline the aims of the proposal

11. What are the main aims of the proposal?

The purpose of this Supplementary Guidance document is to provide further information and detail in respect of policies set out in the Local Development Plan, in accordance with the Scottish Government's intention that the Local Development Plan itself focuses on vision, the spatial strategy, overarching and other key policies, and proposals.

At present, the Council has a variety of smaller supplementary guidance documents, relating to various types of domestic/householder development, such as window replacement, house extensions etc. Many of these have not been reviewed in a number of years, and it is considered that the production of a single, up-to-date guide to the most common forms of householder development would present an opportunity to update relevant guidance, incorporate additions or updates to that guidance where appropriate, and create a more user-friendly document for applicants, agents and officers. Subject to any representations received and amendments made as a result of the consultation process, it is intended that this documents be subsequently adopted as interim Supplementary Guidance. On successful adoption of the Local Development Plan, the documents would gain the status of policy alongside the Plan.

The progression of these Supplementary Guidance documents will provide a clear framework for decision making, allowing comprehensive guidance for applicants and thereby making a significant contribution towards the Council's aim of promoting and achieving sustainable development.

12. Who will benefit most from the proposal?

Residents and businesses in Aberdeen, including stakeholders who have been involved in the preparation of the Aberdeen Local Development Plan – the development industry, key agencies, citizens, Council planning officers and other Council services.

13. Tell us if and how the proposal will increase equality of opportunity by permitting positive action to redress disadvantage?

The document provides all citizens with the opportunity to contribute to the preparation of Supplementary Guidance regarding 'householder' development. As part of the modernisation of the planning system in Scotland, public consultation plays a vital role in the preparation of development plans, meaning that people's involvement can make a real difference to the content of the plan. There is no known disadvantage to any equality target groups or related equality strands, though the consultation process allows all parts of society equal opportunity to engage with the preparation of this guidance and to make representations accordingly.

The proposed Supplementary Guidance will help to improve the quality of life of all citizens in Aberdeen by improving the design quality of domestic development. The consultation process will enable local communities, groups and individuals influence the content of the document and have their say in the design of our built environment. No positive action is therefore required to redress disadvantage.

14. What impact will the proposal have on promoting good relations and wider community cohesion?

The proposed supplementary guidance is intended to form a part of the Proposed Aberdeen Local Development Plan on its eventual adoption. The Proposed Plan and its accompanying supplementary guidance set out Aberdeen City Council's vision for the growth and development of the city. The opportunity to make representations on that vision through the Proposed Plan consultation process allows citizens to influence the way in which their city will grow over time. It is hoped that this will foster a sense of belonging and involvement in those who have engaged with the process, and encourage a sense of community.

The consultation undertaken thus far and the consultation proposed for this document do not exclude or disadvantage any particular group or part of society. Documents are made available through a range of media sources, and representations can be accepted in a number of ways.

STEP 3: Gather and consider evidence

15. What evidence is there to identify any potential positive or negative impacts in terms of consultation, research officer knowledge and experience, equality monitoring data, user feedback and other?

The evidence base for this study relies mainly on the fundamental workings of the planning system, where all application proposals are assessed against policy and the personal circumstances of the applicant are of no relevance. Furthermore, the proposed Supplementary Guidance itself makes no reference to any particular equality groups, and will be applied equally to all development proposals, unless material planning considerations suggest otherwise.

STEP 4: Assess likely impacts on equality strands

16. Which, if any, equality target groups and others could be affected positively or negatively by this proposal? Place the symbol in the relevant box.

(Positive +, neutral 0, - negative)

Equality Target Group					
Race*	0	Disability	0	Gender**	0
LGB***	0	Belief	0	Younger	0
Older	0	Others e.g. poverty	0		

* Race includes Gypsies/Travellers

** Gender includes women, men, Transgender

*** LGB: Lesbian, Gay and Bisexual

17. Please detail the potential positive and/or negative impacts on the groups you have highlighted above? Detail the impacts and describe the groups affected.

Positive impacts (describe groups affected)	Negative Impacts (describe groups affected)
The publication of Technical Advice is an invitation for all groups to engage with planning issues within Aberdeen. Once we publish the Supplementary Guidance there will be a chance for everyone to make comments on the documents.	

STEP 5: Apply the three key assessment tests for compliance assurance

18. Does this policy/procedure have the potential to interfere with an individual's rights as set out in the Human Rights Act 1998? State which rights might be

affected by ticking the appropriate box(es) and how. **If you answer “no”, go to question 22.**

- Article 3 – Right not to be subjected to torture, inhumane or degrading treatment or punishment
- Article 6 – Right to a fair and public hearing
- Article 8 – Right to respect for private and family life, home and correspondence
- Article 10 – freedom of expression
- Other article not listed above

How?

Legality

19. Where there is a potential negative impact is there a legal basis in the relevant domestic law?

Legitimate aim

20. Is the aim of the policy a legitimate aim being served in terms of the relevant equality legislation or the Human Rights Act?

Proportionality

21. Is the impact of the policy proportionate to the legitimate aim being pursued? Is it the minimum necessary interference to achieve the legitimate aim?

STEP 6: Monitor and review

22. How will you monitor the implementation of the proposal? (For example, customer satisfaction questionnaires)

Should members agree, the Supplementary Guidance shall be issued for 8 weeks consultation, at which time all citizens will have equal opportunity to make representations on the proposed guidance.

23. How will the results of this impact assessment and any further monitoring be used to develop the proposal?

The results of consultation will be considered by officers, and any appropriate or otherwise necessary changes to the guidance will be made. Following this, the guidance will be reported back to a future meeting of the Enterprise, Planning & Infrastructure Committee, along with a summary of the consultation responses received and the Council's response. Members will be asked to note those responses and any changes made to the guidance before approval.

STEP 7 SIGN OFF

The final stage of the EHRIA is formally to sign off the document as being a complete, rigorous and robust assessment.

Person(s) completing the impact assessment.

Name	Date	Signature
Gavin Evans	4 October 2011	Gavin Evans

Quality check: document has been checked by

Name	Date	Signature
Gale Beattie	4 October 2011	Gale Beattie

Head of Service (Sign-off)

Name	Date	Signature
Maggie Bochel	4 October 2011	Maggie Bochel

Now –

Please send a copy of your completed EHRIA together with the proposal to:

Head of Service
Customer Service and Performance
Aberdeen City Council
St. Nicholas House, Broad Street
Aberdeen, AB10 1GZ

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Technical Advice Note: Retail Impact Assessments
REPORT NUMBER:	EPI/12/106

1. PURPOSE OF REPORT

- 1.1 The purpose of this report is to obtain Committee approval of Technical Advice Note on Retail Impact Assessments (Appendix 1). This is following a six week public consultation in which we received no representations. The document has been prepared in support of the Aberdeen Local Development Plan 2012 (LDP) and provides guidelines for applicants on what the Council expects a Retail Impact Assessment to cover within Aberdeen. Technical Advice Notes do not carry the same weight as Supplementary Guidance when adopted as part of the Local Development Plan.

2. RECOMMENDATION(S)

- 3.1 It is recommended that the Committee approve the Retail Impact Assessment Technical Advice Note.

3. FINANCIAL IMPLICATIONS

- 3.2 There are no direct financial implications arising from this report, other than costs incurred through consultation and publicity. Any such expenses incurred can be met through existing budgets. The implication for the priority based budgeting is positive. Detailed topic-based advice has value in reducing officer time spent on pre-application discussions, and will therefore prove fruitful in reducing cost. This relates to PBB option EPI PSD02 - Rationalise planning application management.

4. OTHER IMPLICATIONS

- 4.1 There are no known legal or equipment implications arising from this report.

4.2 As a major landowner in the city, proposals for the development of land and assets owned by Aberdeen City Council will be subject to assessment in line with the principles and standards set out in the Guidance, where applicable.

5. BACKGROUND/MAIN ISSUES

5.1 The Council agreed the content of the Aberdeen LDP on 25 January 2012 and it was formally adopted on 29 February 2012.

5.2 The Retail Impact Assessment Technical Advice Note does not have the same weight as Supplementary Guidance or the Aberdeen LDP. However, it does provide information and guidance which is considered a material consideration when determining planning applications.

5.3 The purpose of the Technical Advice Note is to provide guidelines for applicants on what the Council expects a retail impact assessment to cover for retail development proposals within Aberdeen. This will help to ensure that Retail Impact Assessments submitted as part of planning applications for retail development contain all the relevant information required. This will ensure that there is a consistent methodology applied for retail impact assessments.

6. IMPACT

6.1 The Local Development Plan continues to support the vision of Aberdeen becoming an even more attractive place to live and in which to do business and will ensure that high quality employment opportunities exist. This process aspires to improve the access that the people of Aberdeen have to high quality services that meet their needs. The development and refinement of fit for purpose guidance to assist the Aberdeen Local Development Plan is paramount to supporting this vision and achieving the goals that Aberdeen aspires to.

6.2 The vision for Aberdeen is to be a city which is vibrant, dynamic and forward looking – an even better place to live and work, where people can expect high-quality services that meet their needs. This means making a visible difference to the quality of the city's urban and natural environment by promoting high quality development and providing an effective infrastructure to make us a world class strategic location.

6.3 To do this we must think strategically, facilitate development, engage positively with communities and the business sector and be open and transparent in our decision making. We also have a key role in delivering the vision for the City and Shire as expressed through regional plans and strategies. Planning and Sustainable Development is tasked with seeing that Aberdeen stays at the forefront of planning for the future.

6.4 The guidance represented in this report relates to the following Single Outcome Agreement objectives: 1- We live in a Scotland that is the

most attractive place for doing business in Europe; 2- We realise our full economic potential with more and better employment opportunities for our people; 10- We live in well-designed, sustainable places where we are able to access the amenities and services we need; 12- We value and enjoy our built and natural environment and protect it and enhance it for future generations; 13- We take pride in a strong, fair and inclusive national identity; and 15- Our public services are high quality, continually improving, efficient and responsive to local people's needs.

6.5 The guidance represented in this report meets the vision of the Community Plan in promoting a strong image of the City and a sense of civic pride.

6.6 The guidance represented in this report supports the Council's 5 year Business Plan in terms of protecting and enhancing our high quality natural and built environment,

7. BACKGROUND PAPERS

- Aberdeen Local Development Plan 2012
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=42278&SID=9484>
- Appendix 1 – Technical Advice Note: Retail Impact Assessments

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Consultation Response to the Aberdeen Airport Master Plan
REPORT NUMBER:	EPI/12/113

1. PURPOSE OF REPORT

- 1.1 Aberdeen Airport has published a Draft Master Plan for consultation. The purpose of this report is to bring to the attention of this Committee the proposals contained in the Draft Master Plan and to agree a response to the consultation.

2. RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
- (a) Note the main proposals contained in the Aberdeen Airport Draft Master Plan
 - (b) Agree that the letter in Appendix 1 is sent to Aberdeen Airport in response to the consultation on the Draft Master Plan.

3. FINANCIAL IMPLICATIONS

- 3.1 There are no direct financial implications arising from this report.

4. OTHER IMPLICATIONS

- 4.1 There are no known legal or equipment implications arising from this report.

5. BACKGROUND/MAIN ISSUES

- 5.1 The Aberdeen Airport Draft Master Plan differs from the other land use masterplans that are regularly reported to this Committee for approval and the similarity of the title should be largely ignored. Airport master plans are required as a result of The Future of Air Transport White Paper 2003 and this master plan will not be adopted as supplementary guidance to support our Local Development Plan. However, once the

Airport publishes the final Master Plan we must have consideration of it when reviewing plans and strategies that relate to the Airport.

- 5.2 The Draft Master Plan sets out plans to develop the airport up to 2040 with targets of raising passenger numbers by 40% by 2040. Aberdeen is estimated to have been the UK's fastest growing airport last year, with forecasts produced by BAA predicting a growing demand in the long-term for air services to and from the city. BAA estimated passenger numbers at the airport would reach four million by 2020 and more than five million by 2040. The draft master plan sets out in two phases how Aberdeen Airport will develop to meet that future demand, and compete more effectively against UK and European rivals.

Phase 1: Airport Development to 2020

- A focus on making best use of current facilities and alterations to existing infrastructure to meet capacity requirements
- Construction of new aircraft parking stands
- Upgrade and refurbish parts of the main terminal to provide a more efficient and attractive facility
- Additional short stay car parking capacity and possible additional long stay parking
- Develop an air freight strategy with ACSEF and Scottish Enterprise
- Ancillary facilities to support growth, located within the land allocated for airport use

Phase 2: Airport Development to 2040

- Possible requirement to extend runway, part of which would be in addition to the current planning permission, this would require land not currently owned by the Airport at Stoneywood Cricket Club to be acquired
- Additional aircraft parking stands
- Further extensions and improvements to the terminal
- Land safeguarded for maintenance facilities
- Ancillary facilities to support growth located within the land allocated for airport use

- 5.3 Aberdeen Airport plays an important role in the economy of the North East, both directly in the jobs it creates and the connections it provides, which are essential for national and international business. This Master Plan sets a positive vision for improvements to Aberdeen Airport and the development proposed is, alone, estimated to create about 1,100 jobs. This vision should be strongly supported by the Council.

- 5.4 It is essential that airports providing international freight and passenger links are supported by an effective road and rail infrastructure and a main role of the Master Plan is to identify the surface access and transport connections to the airport. In promoting enhancements at our airports, the Scottish Government is placing emphasis on measures that improve surface access by public transport. The Aberdeen Airport

Surface Access Strategy 2008-2012 set out a number of targets and actions to improve access to the airport and increase the use of more environmentally sustainable modes of transport. Aberdeen Airport has committed to review and reissue this strategy in 2013. The previous strategy was produced in consultation with the Airport Transport Forum and the Council were involved in discussions. The Council has not been listed and, as the transport authority for the area, it is requested that specific reference is made to ensure that the Council is involved in future discussions.

- 5.5 Aberdeen Airport commits to work with transport authorities and operators on the surface access strategy to ensure that improvements are delivered in a timely manner to support the growth of the airport. The key objective of the surface access strategy is to support Government aims to increase public transport mode share. The following projects and improvements are identified in the Draft Master Plan to support expansion:

Road: The Aberdeen Western Peripheral Route and A96 link road projects, which will help to ease congestion at Aberdeen Airport. There is a commitment by the Council, with partners, to deliver both of these projects to support efficient and effective surface access.

Rail: Operation of the number 80 Dyce station shuttle bus. Aberdeen Airport has also committed to provide land, currently within its ownership, to support improvements to linkage with Dyce railway station. At this point in time detailed plans for the improvements are not available, but are currently being developed by Aberdeen City Council with the support of NESTRANS, the Regional Transport Partnership.

Walking and cycling: Footpaths link the airport with Dyce, cycle routes connect with National Cycle Network route 1 and a number of cycle parking facilities are located throughout the airport campus. There is commitment from Aberdeen Airport to upgrade and improve the cycle network facilities to provide functional and attractive routes.

- 5.6 The Draft Master Plan identifies some general development principles to guide and inform development. It would be valuable, given the gateway to Aberdeen that the Airport creates, to consider in more detail the issues such as those highlighted in the Council's own Masterplanning Process, namely Context, Identity and Connection. It is understood that there are constraints on public realm within the airport environment, but any future redevelopment could address sense of arrival, combined with ease of movement and comfort around the site for passengers. A detailed land-use plan could supplement the Master Plan to identify areas of improvement and aid in co-ordinating future development. This could also provide an opportunity to consider future building design, the relationship of the various functions of the airport and thereby create a positive impression co-ordinating all new proposals. Officers would be willing to work with Aberdeen Airport and provide advice should they wish to progress further detailed plans.

6. IMPACT

- 6.1 The proposals can be accommodated within the Local Development Plan allocations and as a planning authority and transport authority the Council will commit to working with the airport to achieve the growth of Aberdeen Airport to support the local and national economy.
- 6.2 The Master Plan allows the Council to identify where there is likely to be a requirement for planning permission, which will assist in the implementation process. The Council would also encourage that where planning permission is required for development, the Airport engage with the Planning and Sustainable Development Service, key stakeholders and the community at the earliest opportunity.

7. BACKGROUND PAPERS

Aberdeen Airport Draft Master Plan:

[http://www.aberdeenairport.com/static/Aberdeen/Downloads/PDF/GA12015%20Aberdeen%20MP%20Document%20\(Final\).pdf](http://www.aberdeenairport.com/static/Aberdeen/Downloads/PDF/GA12015%20Aberdeen%20MP%20Document%20(Final).pdf)

National Planning Framework 2:

<http://www.scotland.gov.uk/Resource/Doc/278232/0083591.pdf>

8. REPORT AUTHOR DETAILS

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ABERDEEN
CITY COUNCIL

31 May 2012

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**Enterprise, Planning &
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DX 529452, Aberdeen 9
www.aberdeencity.gov.uk

Dear Mr Peck,

Aberdeen Airport Draft Master Plan Consultation Response

Thank you for the opportunity to comment on the Draft Master Plan for Aberdeen Airport. Aberdeen City Council agrees that Aberdeen Airport plays an important role in the economy of the North East, both directly in the jobs it creates and the connections it provides, which are essential for national and international business. This Master Plan sets a positive vision for improvements to Aberdeen Airport and this vision is strongly supported by the Council.

The Draft Master Plan targets to increase the use of more environmentally sustainable modes of transport, as set out in the Airport Surface Access Strategy. This is a key element of the Aberdeen City Local Transport Strategy to promote the use of sustainable modes of transport. The Airport Surface Access Strategy provides a more detailed tactical response and a review of this is an important component in achieving the Airport's aspirations for growth. The previous strategy was produced in consultation with the Airport Transport Forum and the Council were involved in discussions. The Council has not been listed and, as the transport authority for the area, it is requested that specific reference is made to ensure that the Council is involved in future discussions.

The Draft Master Plan also identifies transport projects to help ease congestion at Aberdeen Airport to support future growth; these include the Aberdeen Western

GORDON McINTOSH
DIRECTOR



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Peripheral Route and Dyce Drive Link Road projects. There remains a commitment by the Council to deliver both of these projects.

The Draft Master Plan identifies some general development principles to guide and inform development. It would be valuable, given the gateway to Aberdeen that the Airport creates, to consider in more detail the issues such as those highlighted in the Council's own Masterplanning Process, namely Context, Identity and Connection. Further information available at:

<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31782&SID=1439>

4. It is understood that there are constraints on public realm within the airport environment, but any future redevelopment could address sense of arrival, combined with ease of movement and comfort around the site for passengers. A detailed land-use plan could supplement the Master Plan to identify areas of improvement and aid in co-ordinating future development. This could also provide an opportunity to consider future building design and the relationship of the various functions of the airport and thereby create a positive impression co-ordinating all new proposals. Officers would be willing to work with Aberdeen Airport and provide advice should you wish to progress further detailed plans.

The Master Plan allows us to identify where there is likely to be a requirement for planning permission, which will assist in the process. We would encourage you to engage with the Council's Planning and Sustainable Development Service at the earliest opportunity where planning permission is required for development.

On a more detailed point, on page 18 of the Draft Master Plan, the Local Development Plan policy that covers Aberdeen Airport is **BI4** rather than **B14**.

Yours Sincerely

Councillor Barney Crockett
Convenor
Enterprise, Planning and Infrastructure

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Planning Reform and Consultations
REPORT NUMBER	EPI/12/114

1. PURPOSE OF REPORT

This report summarises a number of consultations on planning reform which are being carried out by the Scottish Government. The consultations ask a number of specific questions on different parts of the planning system. The Council's proposed responses to these are set out in Appendices 1 to 5.

2. RECOMMENDATION(S)

It is recommended that Committee endorse this report and appendices as the City Council's response to the Scottish Government consultations on planning reform.

3. FINANCIAL IMPLICATIONS

There are no direct financial implications arising out of this report. However, implementation of some of the measures proposed could have positive implications in terms of increasing the efficiency of the planning system as a whole and reducing bureaucracy. The proposed changes to planning application fees are likely to result in an increase in income to the Council, although with an expectation that increased income is invested in improving the planning service. However, until the fee levels are set, we do not know the level of increase.

4. OTHER IMPLICATIONS

There are no other implications arising out of this report. No EHIRA is required as this is a consultation response to the Scottish Government. The Scottish Government has carried out their own equalities impact assessments on the documents referred to here.

5. BACKGROUND/MAIN ISSUES

On March 28, 2012 Derek Mackay, the Minister for Local Government and Planning made a statement to the Scottish Parliament setting out the Scottish Government's proposals for future reform of the planning system.

The Planning Reform Next Steps document summarises the package of proposals which aim to help the planning system reach its potential in supporting economic recovery. The emphasis is on non-legislative measures but legislative changes will be brought forward where necessary.

The key priorities for the next stages of planning modernisation are:-

- promoting the plan led system
- driving improved performance
- simplifying and streamlining processes
- delivering development

As a result, the Scottish Government is seeking views on five consultations.

- Fees for Planning Applications
- Development Delivery
- Development Plan Examinations
- Miscellaneous Amendments to the Planning System
- General Permitted Development Order

This Report summarises these consultations and our detailed responses are set out in Appendices 1 to 5.

Fees for Planning Applications

The first consultation paper discusses a new fee regime for planning applications. The key features of the consultation paper are:-

- Ensuring that fee levels more accurately reflect the resource employed in processing planning applications
- Moving towards one fee covering all aspects of processing, including advertising and providing pre-application advice
- Reducing the fee for the most straightforward classes of application
- Establishing a link between performance and fees.

The overall impact of these potential changes is likely to result in an increase in fee income for planning authorities. Our detailed responses to these issues can be found in Appendix 1.

Development Delivery

In the current climate where public and private sector funding is curtailed, there have been growing issues in relation to the funding and delivery of infrastructure, which is necessary to enable and service associated development. In some cases, the existing processes are struggling to provide the required infrastructure, with resultant impact on the delivery of development.

The purpose of this consultation is to get views on the efficacy of current processes in delivering development; and to invite views on what could assist the delivery of development and infrastructure. It seeks views on the current planning system, delivering development and developer contributions and

discusses some new and innovative approaches to development and infrastructure delivery.

Our detailed responses to these issues can be found in Appendix 2.

Development Plan Examinations

The 2006 Planning Act introduced a series of changes to the procedures for the examination of LDPs. This is bringing some significant benefits. Whereas local plan inquiries took on average 70 weeks, recent plans have taken around 24 weeks. However, the examination of some more recent plans has taken considerably longer (although this was not the case for the Aberdeen Local Development Plan) while the costs of the process are causing concern to planning authorities at a time when budgets are under pressure.

The “binding” nature of reporter’s recommendations is proving to be a source of concern for some planning authorities who see some recommendations, particularly on housing land supply, as undermining the work they have done with stakeholders to the extent that the resultant plan is no longer seen as their plan.

The consultation paper seeks views on the future approach to development plan examinations. It looks at options to improve current practice which are: allowing greater discretion for Councils to depart from the reporter’s recommendations; restricting the scope of examinations; and removing the independent examination from the process.

Our detailed responses to these issues can be found in Appendix 3.

Miscellaneous Amendments to the Planning System

The purpose of this consultation is to seek views on draft legislation for a number of refinements and amendments to the procedures on development management, schemes of delegation, local reviews and appeals.

Views are sought on a number of approaches to making Pre-Application Consultation requirements more proportionate in relation to applications to amend existing planning permissions (known as “Section 42 Applications”) for major and national development.

The Scottish Government intends to amend the current advertising requirements so that:

- a) advertising is not required where neighbouring land is a road or a private means of access to land; or land with no premises which is owned by the applicant or the planning authority
- b) advertising is not required where the application is for householder development and neighbouring land has no premises on it
- c) the separate charging regime for recovering the costs of advertising from applicants (the Town and Country Planning (Charges for Publication of

Notices) (Scotland) Regulations 2009) will be removed and such costs will be met out of fee income, with an adjustment to fee levels to cover this.

There is a proposal for a consultation requirement to consult Network Rail on developments within 10 metres of a railway line or the boundary of railway property.

Current legislation prevents the delegation of applications in which the planning authority has an interest (as applicant or as owner of or having a financial interest in the land to be developed) or which have been made by members of the planning authority. Many applications for relatively minor developments, which would previously have been delegated to an officer for decision, have therefore had to be referred to committee for a decision. This delays decisions and diverts planning authority resources. It is proposed to remove these restrictions.

It is proposed to allow time extensions to local review body cases under certain circumstances and there are minor amendments proposed to appeal procedures and in respect of planning conditions.

Our detailed responses to these issues can be found in Appendix 4.

General Permitted Development Order

Householders can now do more to their properties without the need to apply for planning permission. This consultation is on proposed changes to non-domestic permitted development. It proposes removal of some minor developments from planning controls in relation to the extension and alteration of existing commercial, industrial, retail and warehouse land and premises as well as more scope for local authorities and other institutions to carry out development without the need for specific planning permission. It also proposes that planning controls should be increased over hill tracks. The secondary legislation to bring the changes in to force will be laid in late summer.

Our detailed responses to these issues can be found in Appendix 5.

6. IMPACT

The Scottish Government has stated that an effective planning service is fundamental to achieving its central purpose of sustainable economic growth. As such the information in this report relates to a number of Single Outcome Agreement Outcomes:

- 1 - We live in a Scotland that is the most attractive place for doing business in Europe;
- 2 - We realise our full economic potential with more and better employment opportunities for our people;
- 10 - We live in well-designed, sustainable places where we are able to access the amenities and services we need;

- 12 - We value and enjoy our built and natural environment and protect it and enhance it for future generations;
- 13 - We take pride in a strong, fair and inclusive national identity; and
- 15 - Our public services are high quality, continually improving, efficient and responsive to local people's needs.

Public – The report may be of interest to the development community and certain matters referred to in the report may be of interest to the wider community.

7. BACKGROUND PAPERS

Scottish Government – Planning Reform Next Steps
<http://www.scotland.gov.uk/Publications/2012/03/3467>

Scottish Government - Consultation on Fees for Planning Applications 2012
<http://www.scotland.gov.uk/Publications/2012/03/3164>

Scottish Government - Development Delivery Consultation 2012
<http://www.scotland.gov.uk/Publications/2012/03/3965>

Scottish Government - Development Plan Examinations Consultation 2012
<http://www.scotland.gov.uk/Publications/2012/03/3942>

Scottish Government - Consultation on Miscellaneous Amendments to the Planning System 2012
<http://www.scotland.gov.uk/Publications/2012/03/5577>

Scottish Government - Consultation on the General Permitted Development Amendment Order 2012
<http://www.scotland.gov.uk/Publications/2012/03/8498>

8. REPORT AUTHOR DETAILS

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Appendix 1 - CONSULTATION QUESTIONS for Fees for Planning Applications

Question 1: Are there any costs or benefits not identified in the draft BRIA?

This question would best be answered by others.

Question 2: Do you have any information or can you suggest sources of relevant information on the costs and/or benefits detailed in the BRIA at Section C?

This question would best be answered by others.

Question 3: We would appreciate your assessment of the potential equalities impact our proposals may have on different sectors of the population. A partial EQIA is attached to this consultation at Section D, for your comment and feedback.

The changes are considered to be proportionate and it is not foreseen that they would more affect one sector of the population more than others.

Question 4: Do you consider that linking fees to stages within processing agreements is a good or bad idea? What should the second trigger payment be?

Aberdeen City Council considers this to be a bad idea. We are unsure how this approach would be consistent across Scotland. An additional level of bureaucracy and additional costs for the planning authority would be introduced into the process. Processing agreements will vary considerably depending on the type and complexity of the development and the internal procedures of each planning authority and thus it would be difficult to apply the second trigger in a consistent way both within each planning authority and across all planning authorities. It is also questioned what would be the fall back position for authorities should the '2nd' payment not be made on time or at all? Would the planning application be put on hold indefinitely? Would the applicant have the right to move for appeal/local review for non-determination?

Question 5: Do you agree or disagree with the proposal that where applications are required because permitted development rights for dwellings in conservation area are restricted, then a reduced fee should be payable?

Agree Disagree

Question 6: Do you agree or disagree with the proposal that there should be a separate fee for renewals of planning permission?

Agree Disagree

Question 7: Do you agree or disagree that the new fee is set at an appropriate level?

Agree Disagree

Question 8: Do you agree or disagree with the proposal that the fee should increase on an annual basis?

Agree Disagree

Question 9: Is using site area the best method of calculating fees for windfarms of more than 2 turbines? If not, could you suggest an alternative?

Yes No

In your response please provide any evidence that supports your view.

Aberdeen City Council does not have much experience in this field, but it is suggested the fee should be based on the 'per turbine' method as there is the potential for the site area to be manipulated so that the fee is a lot less than it may otherwise be, resulting in a fee being paid that does not cover the costs of the processing of the planning application by the planning authority. It is suggested that the fees should be based on the sliding scale fees for dwellinghouses

Question 10: We seek views on our intention to amend The Electricity (Applications for Consent) Regulations, and specifically on the following:

a) Should the fee for applications >50MW be set in line with those <50MW?

Yes No

b) Should the application fee be capped at £100,000?

Yes No

If not what should the fee level be capped at?

No comment

c) Should applications for thermal generation stations incur a larger fee?

Yes No

Question 11: Please list any types of developments not included within the proposed categories that you consider should be.

No comment

Question 12: We would welcome any other views or comments you may have on the contents and provisions on the new regulations.

No comment

APPENDIX 2 - DEVELOPMENT DELIVERY CONSULTATION QUESTIONS

Consultation question 1a: Do you think the current planning system supports or hinders the delivery of development and infrastructure?

- Strongly supports** ✓
- Mostly supports**
- Does not influence**
- Mostly hinders**
- Strongly hinders**
- Don't know**

Please explain why you have chosen your above answer.

Comments We believe that the current system enables Councils to support the delivery of development and infrastructure through the allocation of generous amounts of housing and employment land and any necessary supporting infrastructure in LDPs. Action Programmes can identify up front what the infrastructure requirements are and the potential means of delivering these requirements and this is what we will be doing in our own LDP Action Programme. We are currently progressing Masterplans and planning applications for major sites and this is happening quickly in Aberdeen, and we are welcoming the use of processing agreements to provide greater certainty in the timing of required information and decisions. However, the delivery of development is still dependent on market conditions to some extent and the availability of finance from the banking sector. In Aberdeen planning officers and elected members have been working in partnership with others through the Trinity Group to explore and facilitate solutions to these to support the delivery of development.

Consultation question 1b: What additional measures could be taken to support development and infrastructure delivery?

Comments The UK financial sector is concentrated in London and, to a lesser extent, Edinburgh and it is operating in what is a relatively flat UK property market. Conditions in Aberdeen, however, are more buoyant and demand for office space in particular is high. This has been highlighted in a number of commercial property reports such as Rydens and the Scottish Property Federation. This does not however, seem to be widely recognised and we think this could be holding back the finance of viable projects locally. Whilst Aberdeen will continue to promote itself as a place for doing business, as it has done through the Trinity Group, ACSEF and other mechanisms, we think that more promotion is required from both the Scottish and UK Governments that Aberdeen has the means, the demand and the will to become a major force for the recovery of the Scottish and UK economy if only financial institutions can see that potential and invest in the area.

Sustained investment in local and regional infrastructure is viewed by stakeholders as vital to the long term prosperity of Aberdeen City and Shire and to meeting the strategic priority within the Scottish Government's Economic Strategy of aligning investment in infrastructure and place to ensure sustainable economic growth in Scotland.

In this context, additional measures such as the establishment of a Regional Investment Fund ("RIF") to create a delivery mechanism to support development and infrastructure delivery (with a primary focus on the unlocking of regional economic potential).

Such a RIF could have the potential to operate in a 'banker role', whereby the RIF provides either upfront finance or finance-raising guarantees to facilitate project investment. Alternatively, investment could be wholly or partially repayable, where future income from successful developments could be identified and "recycled" into other projects.

Whilst this Authority is aware that a variation of this proposed Regional Investment Fund theme is operating elsewhere in Scotland under the aegis of the EU's Joint European Support for Sustainable Investment in City Areas ("JESSICA") programme, however due to Aberdeen's relative prosperity it is unable to participate in JESSICA. Therefore we consider it would be appropriate to seek Scottish Government support to establish a complementary Regional Investment Fund for those areas of Scotland ineligible for European Regional Development Fund Priority 3 support.

Consultation question 2: How well do you think the process of seeking developer contributions through Section 75 planning obligations is functioning?

- Process functions well
- Process requires some MINOR changes ✓
- Process requires some MAJOR changes
- Section 75 Planning Obligations is not an appropriate process for securing developer contributions

Please explain why you have chosen your above answer and identify what can be done to alleviate any issues raised?

Comments One of the main delays in finalising Section 75 Obligations is that they are necessarily registered against the property (as against the developer who could sell them on). There are frequently delays in completing Title Deeds through complexities in finding out who actually owns pieces of land. Sometimes this can take months. Providing greater resources for Council's legal services may help this but would not completely eradicate delays. One means of doing this could be to introduce a charge for processing Section 75 Obligations so that more resources could be put to them. Alternatively, developers could be encouraged to try and resolve as many of the title deed issues as possible prior their submission.

Consultation question 3: What additional measures or support could the Scottish Government undertake or provide to facilitate the provision of development and infrastructure within the current legislative framework?

Comments We recognise that the central funding of infrastructure, paid for by central or local government, has its limits in the current financial climate. Nevertheless, it is vital that major committed infrastructure projects such as the Aberdeen Western Peripheral Route, Haudagain improvements and Third Don Crossing continue to be supported because investment in such projects is vital in securing our long term growth and prosperity.

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Consultation question 4: What innovative approaches are you aware of in facilitating development and infrastructure delivery and what are your views on their effectiveness?

Comments The Strategic Development Planning Authority, Aberdeen City and Aberdeenshire Councils and NESTRANS have recently developed guidance on a Strategic Transport Fund. The Aberdeen City and Aberdeenshire Cumulative Transport Appraisal (CTA) demonstrates that new development across the north-east will have an impact on transport infrastructure and that movements rely on a network of road, rail and public transport with a high degree of interdependency between the two council areas. A package of defined transport projects are identified by the CTA to mitigate the impacts of new development and the purpose of this guidance is to provide a mechanism for securing contributions from development to fund the delivery of this infrastructure. In doing so, this guidance will help deliver the development potential identified in the structure plan and ensure support for sustainable economic growth priorities in Aberdeen City and Aberdeenshire.

The projects include road and public transport interventions in a variety of locations where the cumulative impact of new housing and employment uses is likely to cause increased congestion. By sharing the financial burden widely across the region, no one development will be liable for the cost of a specific strategic project or delayed by its implementation. By being upfront about the mechanism for making contributions, developers will have greater certainty over strategic transport requirements.

The Guidance details who will be expected to contribute, how much the contributions will be, how and when they will be paid and how the contributions will be used. The Guide can be found on the SDPA website; <http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=963&sID=38>

A number of other initiatives have also been undertaken locally to support new development. These include the recently approved Business Improvement District ("BID") in Aberdeen city centre, and the on-going preparation of a business case for Tax Incremental Financing ("TIF") to

support development activities in Aberdeen.

Aberdeen City Council has an active engagement with the North Territory 'hub' Programme and anticipates the 'hub' model of revenue funded infrastructure playing a significant future role in the delivery of community infrastructure facilities in the future.

Consultation question 5: Would you be supportive of the introduction of a Development Charge system in Scotland to assist in the delivery of development and infrastructure?

Yes

No



Please explain why you have chosen your above answer.

Comments We consider this a crude mechanism that may not be directly relevant to all development proposals. It could mean that developers are paying a charge for development in locations with few constraints, such as schooling or roads constraints. On the other hand, developments in highly constrained areas could pay the same charge. This does not seem fair and could be seen to discourage development from areas where there is spare infrastructure capacity.

Consultation question 6: Do you have any information or can you suggest sources of relevant information on the costs and/or benefits to support the preparation of a BRIA?

Comments No comment.

Consultation question 7: We would appreciate your assessment of the potential equalities impact these issues may have on different sectors of the population.

Comments See Question 5. An across the board development charge could discourage development in areas where there is spare infrastructure capacity. Some of these areas are likely to be in areas of multiple deprivation or regeneration areas – i.e. where development is most needed or would be of most benefit.

APPENDIX 3 – DEVELOPMENT PLAN EXAMINATIONS CONSULTATION QUESTIONS

Question 1: How well do you think the examination process is functioning and should any changes be made to the process at this stage?

ACC Response: In the case of Aberdeen's Local Development Plan, we felt that the examination process worked very well, especially when compared with the old system. Under the old system, the time spent between the start of the Inquiry and adoption of the 2008 Aberdeen Local Plan was approximately 28 months. Under the new system it took approximately 8 months. Costs were considerably less under the new system.

Comment is made in the paper that delays under the new system appear to arise because the reporters consider that some plans do not conform with the structure plan or government policy, particularly in regard to housing land. We would argue that it should be incumbent on planning authorities to ensure that their Local Development Plans (LDP) do conform to their structure plans and that full housing requirements are met within their plans (as was the case with the Aberdeen LDP). In addition, the full housing requirements and allowances should be clearly set out and be unambiguous in Strategic Development Plans. To do otherwise is inevitably going to cause problems at Examination and it is in the hands of planning authorities to avoid such situations.

Further potential problems can also be avoided through regular contact and training with the DPEA prior to examination so that both planning authorities and the DPEA are clear about what is expected from each other. This happened in the case of Aberdeen and officers found it very helpful. However, the Examination still generates a huge amount of paperwork and we think that more use of electronic formats and CD's and less hard copy would be helpful, cheaper and better for the environment.

One area which we feel could be re-examined is modifications to the Proposed Plan following the representation period. We would agree that from the Proposed Plan stages, authorities should proceed to adoption as quickly as possible. However, we think that the ability to make possible changes during the examination (through our response to the reporters) should be clarified and made more explicit where these are of a relatively minor nature.

Other than these minor issues we do not favour any major changes to the examination process.

Question 2: If you think changes are needed which option do you support, and why?

ACC Response: We do not favour option 1 whereby reporters can highlight the need for planning authorities to address issues such as housing land

supply at the end of the examination. This would not resolve such issues and provide closure and is likely to lead to further delays.

Allowing Council's greater discretion to depart from reporter's recommendations would be more locally democratic. However, it would need to be made clear that any such changes should not make the LDP inconsistent with the Strategic Development Plan (SDP) or National Planning Framework (NPF), otherwise the LDP could be challenged, or the planning authority could be directed by Scottish Ministers to prepare a new Proposed Plan.

From our recent experience early, effective, engagement greatly helps to overcome concerns that the public may have, although given the nature of the local development process and the direct impact on communities/developers there will always remain objections, and to some extent the Local Authority is best placed to understand these. However, it is important that the Government have a level of input into the content of Plans and maybe a process where the Government can choose to call in the plan for examination if it does not conform to SPP, NPF or any relevant strategic development plan may be of benefit. This process could involve close scrutiny of the participation statement, both at the start of the project and on submission of the plan. However, we are unsure that this would save a great amount of time. It also raises the prospect of different representations being processed differently. It is unclear to us what the implications of this option would be.

One option could be for the Examination to consider Council interest issues only. However, where there is a situation of competing sites, it is likely that other non Council sites could be drawn into the Examination.

Another option would be to consider issues where there are representations of a particular scale – essentially considering more controversial issues only. However, the level at which this is set is likely to be controversial in itself, particularly in cases which are close to any set threshold.

Councils could be allowed to determine which issues are considered at Examination. This is also likely to be controversial and could lead to challenges by aggrieved third parties.

Completely removing the independent examination from the process and having the Council determine the representations is not favoured. Under these circumstances, planning authorities are likely to be seen as judge and jury. Having an independent examination should increase confidence in the system that a fair hearing is available to all. Independent scrutiny also allows a fresh pair of eyes to look at our plans and this should lead to improvements in their quality.

Question 3: Are there other ways in which we might reduce the period taken to complete the plan-making process without removing stakeholder confidence?

ACC Response: In the case of the examination into the Aberdeen LDP, the Council accepted all of the reporter's recommendations. However, we were still required to await a 28 day period for Ministers to decide whether or not to issue a direction in the case of the LDP. We feel that in cases where a planning authority accepts all of the reporter's recommendations, this requirement should be waived and the planning authority should be allowed to adopt its LDP with immediate effect.

Question 4: Do you think any of the options would have an impact on particular sections of Scottish society?

ACC Response: The option of restricting the scope of the examination could see different representations treated differently. We feel that all representations, whether they be from developers, multi-national companies, government agencies or the general public should be treated the same.

Appendix 4 - CONSULTATION QUESTIONS ON MISCELLANEOUS AMENDMENTS TO THE PLANNING SYSTEM 2012

Question 1: Are there any costs or benefits not identified in the draft BRIA?

This question would best be answered by others.

Question 2: Do you have any information or can you suggest sources of relevant information on the costs and/or benefits detailed in the BRIA at Annex VI?

This question would best be answered by others.

Question 3: We would appreciate your assessment of the potential equalities impact our proposals may have on different sectors of the population. A partial EQIA is attached to this consultation at Annex VII for your comment and feedback.

The changes are considered to be proportionate and it is not foreseen that they would more affect one sector of the population more than others.

Question 4: Do you agree or disagree with the proposed removal of PAC requirements in relation to Section 42 Applications? Please explain why.

Agree

It is agreed that removing this requirement would make the process more proportionate, bearing in mind that PAC would already have been carried out for these proposals.

It is also suggested that the requirement for PAC be removed from renewal applications, when these are the first renewal of an application. This is because the PAC process would have been carried out just three years earlier.

Question 5: Do you think the proposed changes to advertising requirements are appropriate or inappropriate?

Appropriate

Please give reasons for your answer.

The removal of the requirement to advertise where there are no premises on neighbouring land, where that land is a road, or is owned by the applicant or the planning authority is a sensible change, which will avoid possible time delays and costs. It is considered that no harm would be caused to neighbouring owners by this change.

Similarly, for householder development where there is neighbouring land without premises it is agreed that there is no likely impact on the land through a development of this scale.

Including advert costs within the fee scale will help save on administration costs and is welcomed.

Question 6: Are there further changes to requirements or the use of advertising in planning which should be considered?

Yes

Please give reasons and evidence to support your answer.

It is considered that press adverts in general are of limited value. Any party wishing to regularly check for advertised applications could check with equal ease on-line or at libraries.

Site notices are also considered to be of limited value and effectiveness in some cases and are particularly resource intensive due to the necessity to send out a member of staff to post them on site.

Question 7: Do you agree or disagree with the proposed removal of the restrictions on the delegation of planning authority interest cases?

Agree

If you disagree, please give your reasons.

This would remove many minor, non-controversial applications from the agenda of committee meetings, which is to be welcomed. It is considered that adequate safeguards would exist by the controversial applications being 'caught' by the other triggers in the scheme of delegation.

Question 8: This section proposes a change to allow an extended period for the determination of an application to be agreed upon between the applicant and appointed person where local review procedures would apply. Do you agree or disagree with this change?

Agree

Please explain your view.

It is agreed that this a sensible amendment to allow the system to be more flexible. There are no disadvantages foreseen. This would also be consistent with the arrangements for major and non-delegated applications.

Question 9: Do you agree or disagree with this change to the time period on determining local reviews sought on the grounds of non-determination?

Agree

Please explain your view.

It is agreed that this is a sensible amendment, three months is a more reasonable time period. The LRB process can be longer depending on whether a site visit is required and more information sought.

Question 10. Do you agree or disagree with this change to the Appeals Regulations on procedure regarding minor additional information?

Agree

This is sensible and essentially a 'tidying up' of the legislation.

Question 11: Do you think the current requirements on applications for approval of matters specified in conditions on planning permission in principle are generally excessive?

Yes

Please explain your views, citing examples as appropriate.

For some conditions, the requirement for fresh applications is excessive. A return to the procedure whereby certain more fundamental matters are the subject of fresh applications, whilst agreement of the details of the proposal is dealt with informally would be welcomed as saving resources spent unnecessarily on administration.

Question 12: Are there are any issues in this consultation not covered by a specific question or any other aspects of the current planning legislation on which you would like to comment? If so, please elaborate.

None

Appendix 5 = CONSULTATION QUESTIONS on General Permitted Development Order

Q1. Are there any costs or benefits not identified in the draft BRIA?

This question would best be answered by others.

Q2. Do you have any information or can you suggest sources of relevant information on the costs and/or benefits detailed in the BRIA?

This question would best be answered by others.

Q3. We would appreciate your assessment of the potential equalities impact our proposals may have on different sectors of the population. A partial EQIA is attached to this consultation at Annex 3 for your comment and feedback.

The changes are considered to be proportionate and it is not foreseen that they would more affect one sector of the population more than others.

Part 1. Amendments to existing classes of permitted development.

Q4. Should we retain class 26 which allows for *the deposit of waste material resulting from an industrial process on any land comprised in a site which was used for that purpose on 1st July 1948 whether or not the superficial area or the height of the deposit is extended as a result?* If class 26 should be retained are there any changes to the controls that would strike a better balance?

Yes No

Not applicable to ACC

Q5. With regard to the proposed amendments to existing classes;

(a) Is the granting of permission, and the restrictions and conditions, clear?

Yes No

(b) Is the granting of permission, and the restrictions and conditions, reasonable?

Yes No

(c) Will the controls strike the right balance between removing unnecessary planning applications and protecting amenity?

Yes No

(d) Please identify and explain any changes to the controls that you think would strike a better balance?

It is suggested that the development value level should be higher, possibly up to £500,000 in order to 'future proof' for inflation.

Part 2. Proposed new classes of permitted development.

Q6. With regard to the proposed new classes 7E and 7F which relate to Electric vehicle charging points;

(a) Is the granting of permission, and the restrictions and conditions, clear?

Yes No

(b) Is the granting of permission, and the restrictions and conditions, reasonable?

Yes No

(c) Will the controls strike the right balance between removing unnecessary planning applications and protecting amenity?

Yes No

(d) Please identify and explain any changes to the controls that you think would strike a better balance?

No comment

Q7. With regard to the proposed new classes 7A and 7B which relates to extensions of a shop/financial or professional services establishment & provision of free-standing trolley stores;

(a) Is the granting of permission, and the restrictions and conditions, clear?

Yes No

(b) Is the granting of permission, and the restrictions and conditions, reasonable?

Yes No

(c) Will the controls strike the right balance between removing unnecessary planning applications and protecting amenity?

Yes No

(d) Please identify and explain any changes to the controls that you think would strike a better balance?

No comment

Q8. With regard to the proposed new class 7C which relates to extension or alteration of hospitals, universities, colleges, schools and nursing or care homes;

(a) Is the granting of permission, and the restrictions and conditions, clear?

Yes No

(b) Is the granting of permission, and the restrictions and conditions, reasonable?

Yes No

- (c) Will the controls strike the right balance between removing unnecessary planning applications and protecting amenity?

Yes No

- (d) Please identify and explain any changes to the controls that you think would strike a better balance?

No comment

Q9. With regard to the proposed new class 7D which relates to extension of offices;

- (a) Is the granting of permission, and the restrictions and conditions, clear?

Yes No

- (b) Is the granting of permission, and the restrictions and conditions, reasonable?

Yes No

- (c) Will the controls strike the right balance between removing unnecessary planning applications and protecting amenity?

Yes No

- (d) Please identify and explain any changes to the controls that you think would strike a better balance?

It is suggested that the amount of 'extension' permitted should reflect the Class &C by allowing up to 100sqm

Q10. With regard to the proposed new class 7H which relates to use of land for a pavement café;

- (a) Is the granting of permission, and the restrictions and conditions, clear?

Yes No

- (b) Is the granting of permission, and the restrictions and conditions, reasonable?

Yes No

- (c) Will the controls strike the right balance between removing unnecessary planning applications and protecting amenity?

Yes No

- (d) Please identify and explain any changes to the controls that you think would strike a better balance?

There is a concern that amenity issues may arise from this where there are residential properties to either side or above a property which might want a to provide a 'pavement café' and would suggest that consideration be given to the merits of restricting the hours of usage in such circumstances – 9am-9pm for example would not appear onerous

Q11. With regard to the proposed new class 7G which relates to erection, construction or alteration of an access ramp;

(a) Is the granting of permission, and the restrictions and conditions, clear?

Yes No

(b) Is the granting of permission, and the restrictions and conditions, reasonable?

Yes No

(c) Will the controls strike the right balance between removing unnecessary planning applications and protecting amenity?

Yes No

(d) Please identify and explain any changes to the controls that you think would strike a better balance?

Comments

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ABERDEEN CITY COUNCIL

COMMITTEE: **Environment, Planning and Infrastructure**

DATE: **31st May 2012**

DIRECTOR: **Gordon McIntosh**

TITLE OF REPORT: **Various small scale traffic management and development associated proposals (New Works)**

REPORT NUMBER: **EPI/12/098**

1. PURPOSE OF REPORT

This report is to advise Committee of the need for various small scale traffic management measures identified by officers, residents, local members, emergency services, etc and verified as necessary through surveys by officers. It also brings forward proposals associated with new developments as part of the development management process. In addition to these measures, this report also includes proposals for individual parking spaces for Blue Badge holders which now require to be progressed through the normal legal process for the required Traffic Regulation Order.

2. RECOMMENDATION(S)

That the Committee:

1. Approve the proposals in principle.
2. Instruct the appropriate officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation orders required as described in this report. If no significant objections are received, then progress with the public advertisement and report the results to a future meeting of this Committee.
3. Instruct the appropriate officers to commence the combined statutory consultation for the traffic regulation order for the list of Blue Badge parking spaces and report back to a future meeting of this Committee.

3. FINANCIAL IMPLICATIONS

The current Five Year Business Plan has identified savings from the Road Safety and Traffic Management budget. There has also been a comprehensive review of the Capital Plan which will result in proposals having to await funding for implementation.

Budget	Implementation costs (£)	Maintenance costs (£) after 5 years	Comments
(●) Cycling, Walking, Safer, Streets (Scot Gov grant-funded)	5250	4250	If budgets are not currently available locations will be placed on a priority list for when future funding becomes available
(❖) Developer financed	-	6750	Maintenance of these works generally falls to the council maintenance budget when they are on-street restrictions
(➤) Disabled Parking	To be confirmed	To be confirmed	Some of these spaces will require to be relined approximately every 10 years at a cost of £100 per space and some will require removal before this time at a cost of £108 per space.

4. OTHER IMPLICATIONS

There is a risk that any approved traffic regulation orders may have to re-enter the legislative process if they are unable to be implemented within the statutory implementation time of 2 years from the start of public consultation if funding is insufficient.

5. BACKGROUND/MAIN ISSUES

There are traffic management proposals for 12 locations brought forward during the course of routine examination of road safety and traffic flows and 1 proposal resulting from a request from a developer.

Key:
○ Funded from the Cycling, Walking, Safer Streets grant funded budget
❖ Funded by the developer
➤ Funded from the current Disabled Parking revenue budget
○ No funding required

The following proposals will be funded from Cycling, Walking, Safer Streets budget

Ashley Lane – Proposed “At any time” waiting restrictions

A complaint has been received from residents and through Grampian Police regarding vehicles parking in this lane, in particular parking opposite their garage and preventing vehicles being taken in or out of this garage. Access to the garage is required at all times of the day and so “At any time” waiting restrictions on the north side of Ashley Lane are required. Other options have been tried and failed

The proposals are indicated on the plan below.

Implementation cost - £80

Estimated maintenance cost – £80 every 10 years

Ward (10) – Hazlehead/Ashley/Queens Cross

Elected members –Corall, Greig, Stewart, Thomson

Cornhill Drive/Ash-Hill Drive – Proposed “At any time” waiting restrictions

There have been a number of complaints from residents and also through a local member with regard to vehicles parking too close to the Cornhill Drive/ Ash-hill Drive junction. This parking pattern reduces visibility on the exit from the junction and also creates problems for vehicles when turning into Cornhill Drive.

The “At any time” waiting restrictions will allow vehicles to wait whilst others pass and improve the visibility for drivers emerging into Ash-Hill Drive.

The proposals are indicated on the plan below.

Implementation cost - £180

Estimated maintenance cost – £180 every 5 years

Ward (5) – Hilton/Stockethill

Elected members – Adam, Blackman, Dunbar

Craigshaw Crescent – Proposed “At any time” waiting restrictions.

This street has a number of car dealerships trading along its length and congestion and access problems have arisen.

There have been complaints from these businesses about pedestrian access being restricted outside their premises by vehicles parking over the dropped kerbs at the access points. It would, however, appear that much of the problem stems from the businesses themselves as they tend to use the maximum amount of their

on-site parking for the sale of their vehicles resulting in the decanting of staff vehicles out onto the street.

There is a large number of vehicles parking in this street and it has also been noted that car transporters also park to off-load vehicles onto the footway, creating road safety issues in the area. Apart from the obvious damage that is being caused to the footway surfaces and utilities underneath the surface, these drivers are disregarding the safety of other road users and in particular cyclists and pedestrians.

There are a number of recently constructed cycle paths/lanes in the area encouraging commuters to cycle to and from their places of work in the south of the City which are now being well used at certain times of the day. This current parking situation is detrimental to their safe passage and that of any pedestrians in the area.

There is already in place a footway parking ban on Craigshaw Crescent and the short term parking problem whilst off-loading can be dealt with through more robust enforcement of this restriction, however the long-term commuter parking, restricting access to the commercial premises, requires more extensive waiting restrictions in line with other streets within the industrial estate.

The proposals are indicated on the plan below.

Implementation cost - £1800

Estimated maintenance cost – £1800 every 5 years

Ward (13) – Kincorth/Loirston

Elected members – Cooney, Finlayson, McCaig

Midsocket View – Proposed “At any time” waiting restrictions

It has been brought to officers’ attention that cars are being parked on both sides of the access road to Midsocket View restricting access to the car park and causing a road safety hazard for vehicles taking access to and egress from the main trunk road. Waiting restrictions “At any time” are required on both sides to alleviate this problem

The proposals are indicated on the plan below.

Implementation cost - £350

Estimated maintenance cost – £350 every 5 years

Ward (3) – Kingswells/Sheddocksley

Elected members – Cameron, Delaney, Ironside

North Deeside Road (Lane at 229/231), Peterculter – Proposed “At any time” waiting restrictions

The property at No 229 North Deeside Road has only one access by means of the front door from North Deeside Road. There is no access from the property to the back garden. The only means of accessing the rear garden is by walking from the front door, along the gable end of the property and into the back garden which is on a steep slope and a hand rail is required by some. The footway along the gable end of the property is very narrow and vehicles frequently park on it forcing pedestrians using the footway onto the carriageway where there is no hand rail to assist the pedestrians.

In order to ease this situation it is proposed to install “At any time” waiting restrictions along the east side of this lane to allow pedestrian access over the narrow footway again.

The proposals are indicated on the plan below.

Implementation cost - £120

Estimated maintenance cost – £120 every 10 years

Ward (9) – Lower Deeside

Elected members – Boulton, Malik, Malone

North Deeside Road, Peterculter – Proposed timed loading restrictions, “Mon – Fri; 7:30am to 9:00am”

It has been noted that a number of delivery lorries are now parking on the “At any time” waiting restrictions to the west of the SPAR grocers on North Deeside Road opposite Station Road West creating a hazard to vehicles travelling eastbound past the corner at Malcolm Road.

These proposals are to introduce loading restrictions around the sharp bend to improve both visibility and the free flow of vehicles.

The proposals are indicated on the plan below.

Implementation cost - £660

Estimated maintenance cost – £350 every 5 years

Ward (9) – Lower Deeside

Elected members – Boulton, Malik, Malone

North Deeside Road, Peterculter – Proposed “At any time” waiting restrictions

A complaint has been received from residents and through an elected member regarding vehicles parking on the footway and private land outside the Post Office and shop, in particular parking on the footway and preventing pedestrian access along the footway and access to the pedestrian crossing. At present, only the zig-

zag markings for the approach to a pedestrian crossing are present on-street and these are only enforceable on the carriageway area between the zig-zag markings. In order to take action against vehicles parking on the footway, "At Any Time" waiting restrictions must be imposed, as these are also enforceable for the footway behind the markings.

The proposals are indicated on the plan below.

Implementation cost - £510

Estimated maintenance cost – £410 every 10 years

Ward (9) – Lower Deeside

Elected members – Boulton, Malik, Aileen Malone

Old Skene Road, Kingswells – Proposed revocation of a section of "At any time" waiting restrictions

With the development of several residential properties opposite the Four Mile House and their associated accesses and also the extension of the car park within the Four Mile House, a section of the "At any time" waiting restrictions on the south side of the Old Skene Road is no longer required. It is therefore proposed to revoke this length of restrictions.

The proposals are indicated on the plan below.

Implementation cost - £400

Estimated maintenance cost – nil

Ward (3) – Kingswells/Sheddocksley

Elected members – Cameron, Delaney, Ironside

Oscar Road – Revocation of "School Keep Clear" zig-zags

With the closure of the primary school these restrictions are no longer required and the removal of the zig-zags will allow a greater degree of parking adjacent to the Health Centre.

The proposals are indicated on the plan below.

Implementation cost - £150

Estimated maintenance cost – nil

Ward (12) – Torry/Ferryhill

Elected members – Allan, Dickson, Donnelly, Kiddie

Provost Graham Avenue – Proposed timed “Mon – Fri; 8am – 4pm” waiting restrictions

Complaints have been received from Environmental Services with regard to access to the refuse bin area at the end of the residential access road off Provost Graham Avenue. The indiscriminate parking of vehicles at either side of the junction is preventing access and egress where often the access road is being blocked completely.

The proposals are indicated on the plan below.

Implementation cost - £250

Estimated maintenance cost – £250 every 5 years

Ward (10) – Hazlehead/Ashley/Queen’s Cross

Elected members – Corall, Greig, Stewart, Thomson

Royal Court, Queen’s Road – Proposed extension of “Pay and Display” parking bays

An elected member for this ward has requested that the marked “Pay and Display” parking bays be extended by about 5m at the east end of the street to allow for an additional parking space and officers agree that this can be implemented without compromising the existing spaces.

The proposals are indicated on the plan below.

Implementation cost - £90

Estimated maintenance cost – £50 every 5 years

Ward (10) – Hazlehead, Ashley, Queens Cross

Elected members – Corall, Greig, Stewart, Thomson

Summerhill Road – proposed “At any time” waiting restrictions

Concerns have been raised by local residents with regard to indiscriminate parking outside the adjacent Care Home. This results in the flow of vehicles through the junction being compromised as the lanes become blocked. It is therefore proposed to introduce “At any time” waiting restrictions to address this problem.

The proposals are indicated on the plan below.

Implementation cost - £900

Estimated maintenance cost – £900 every 5 years

Ward (10) – Hazlehead/Ashley/Queens Cross

Elected members – Corall, Greig, Stewart, Thomson

Virginia Street – “At any time” waiting restrictions

There have been several complaints and also observations by officers that vehicles are currently parking within a delivery bay on Virginia Street just east of the new Pure Gym and Ibis Hotel complex. This parking is not acceptable as up to 8 vehicles are parking, partly on the footway on both sides, blocking other vehicles as well as causing an obstruction to any pedestrians using the footways. It is also inevitable that those parking at the west end of the bay would not be able to exit this bay in forward gear and would have to reverse onto the dual carriageway just at the bend adjacent to Shiprow. These reversing vehicles would not necessarily be visible to vehicles travelling northbound on Trinity Quay/Virginia Street, the main HGV route through the City.

It is proposed to introduce “At any time” waiting restrictions within this bay which will still allow delivery vehicles to load and unload to the adjacent premises, but will exclude the indiscriminate and potentially hazardous parking that is occurring at present.

The proposals are indicated on the plan below.

Implementation cost - £270

Estimated maintenance cost – £270 every 5 years

Ward (8) – George Street/Harbour

Elected members – May, Jean Morrison, Nathan Morrison

The following proposals will be funded by the developer

❖ **Pharmacy/residential development at “Lea Rigg”, Kingswood Drive, Kingswells** – Proposed “At any time” waiting restrictions

Planning Consent has been granted for the above development at “Lea Rigg” on Kingswood Drive

As part of the Consent there is a requirement to include new ‘At any time’ waiting restrictions between the two pedestrian crossings on Kingswood Drive.

This type of development is likely to create short stay, high turnover parking and, if not restricted, the parking place of choice is likely to be on Kingswood Drive itself creating a degree of congestion. A number of parking places have been provided within this development for the purpose of high-turnover parking and the restrictions should encourage drivers to use this parking area.

The proposals are indicated on the plan below.

Implementation cost – nil

Estimated maintenance cost – £1000 every 10 years

Wards (3) – Kingswells/Sheddocksley

Elected members – Cameron, Delaney, Ironside

❖ **Wellheads Drive, Dyce – Industrial development** – Proposed “At any time” waiting restrictions

As a part of the consent for this development there is a requirement for “At any time” waiting restrictions along the full length of the access roads within it.

The proposals are indicated on the plan below.

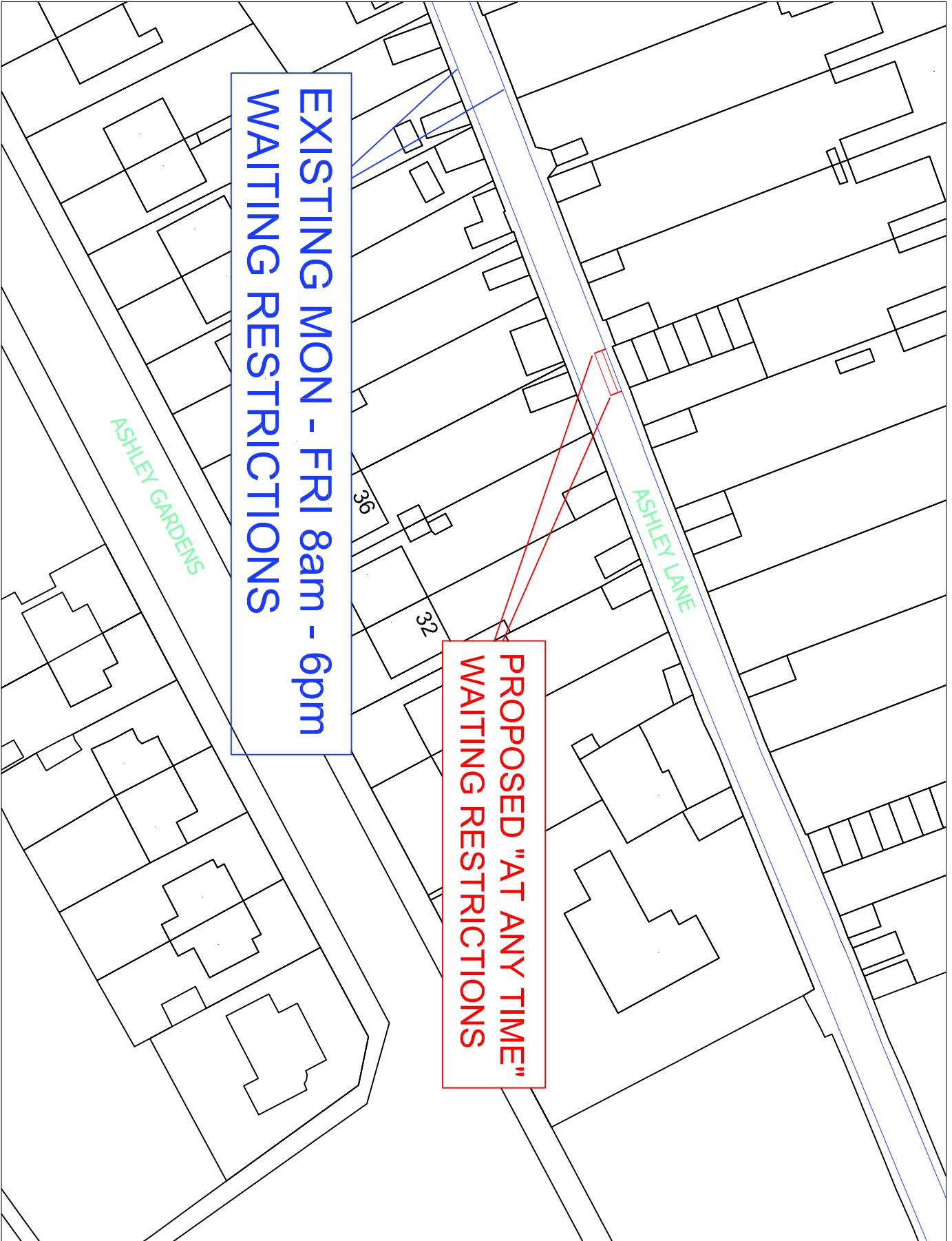
Implementation cost – nil

Estimated maintenance cost – £6750 every 5 years

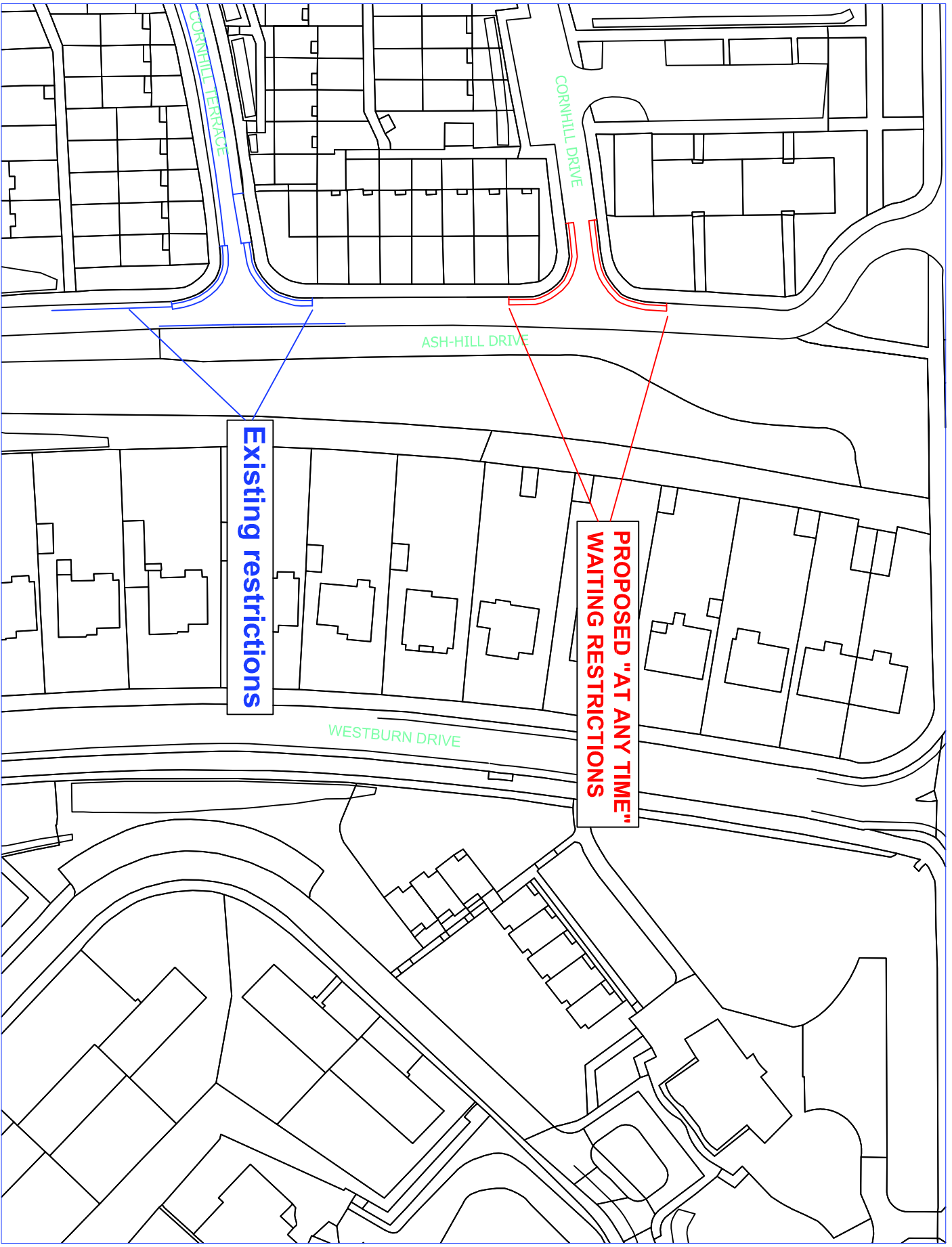
Wards (1) – Dyce/Bucksburn/Danestone

Elected members –Crockett, Lawrence, MacGregor, Samarai

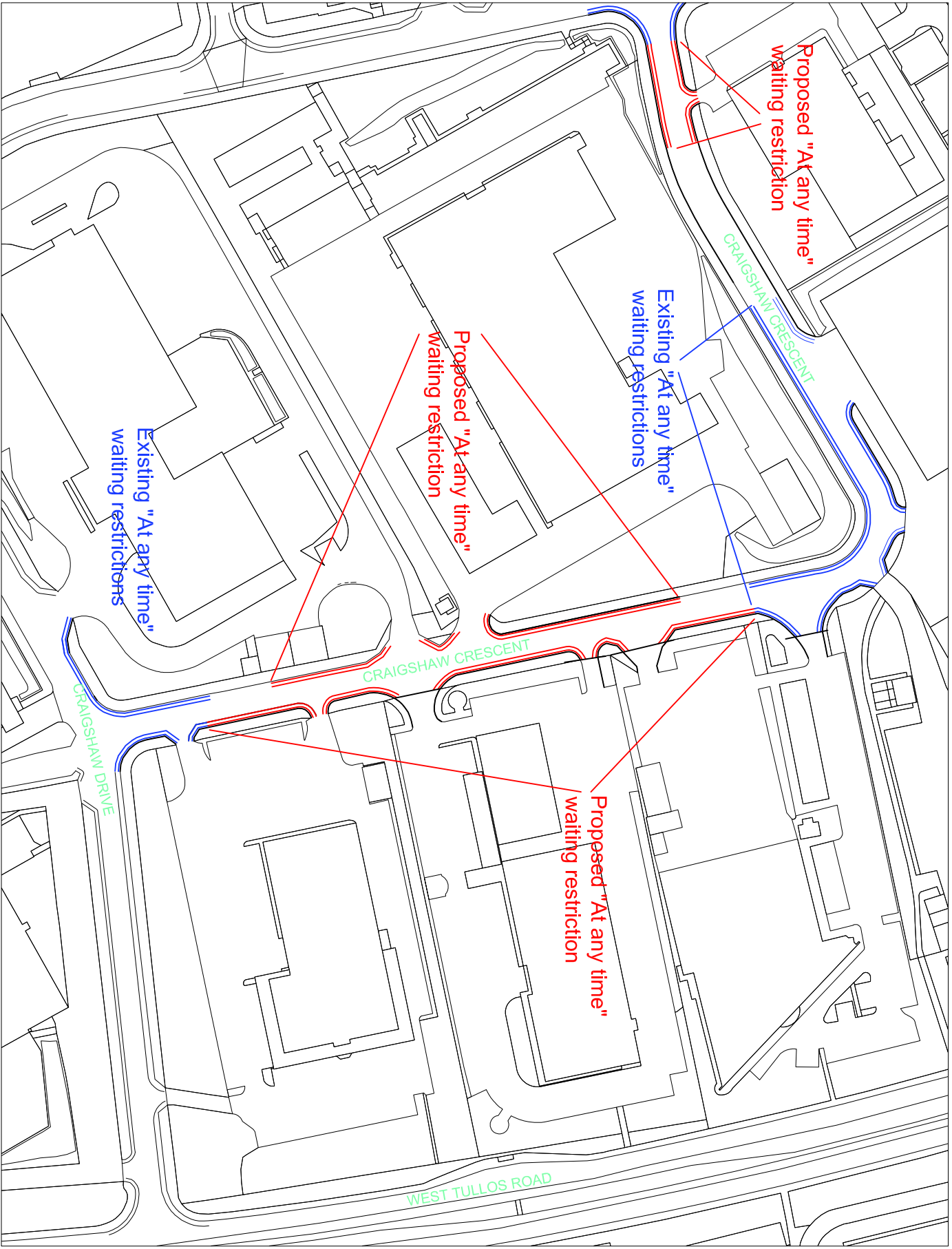
Ashley Lane - Proposed "At any time" waiting restrictions



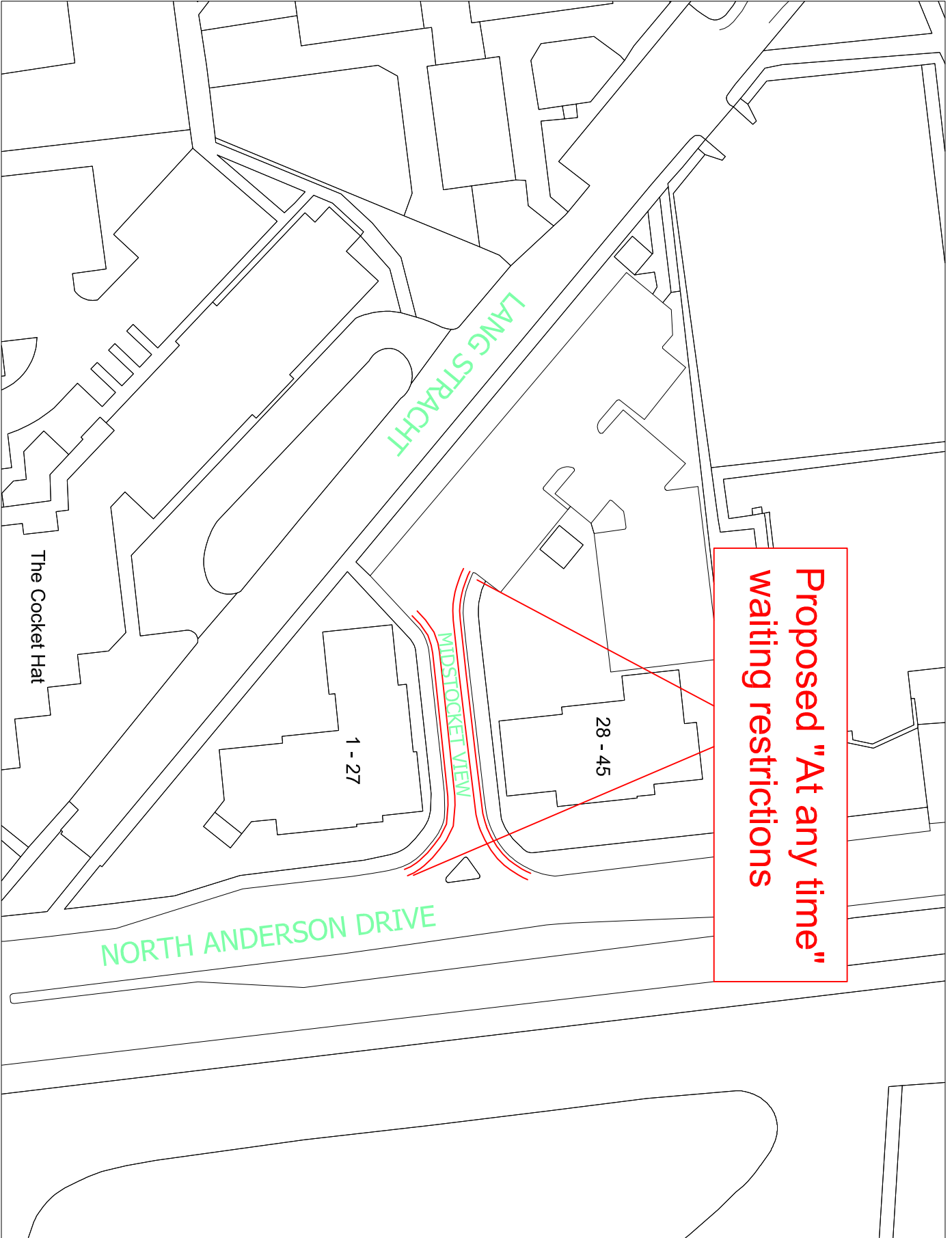
Cornhill Drive - Proposed "At any time" waiting restrictions



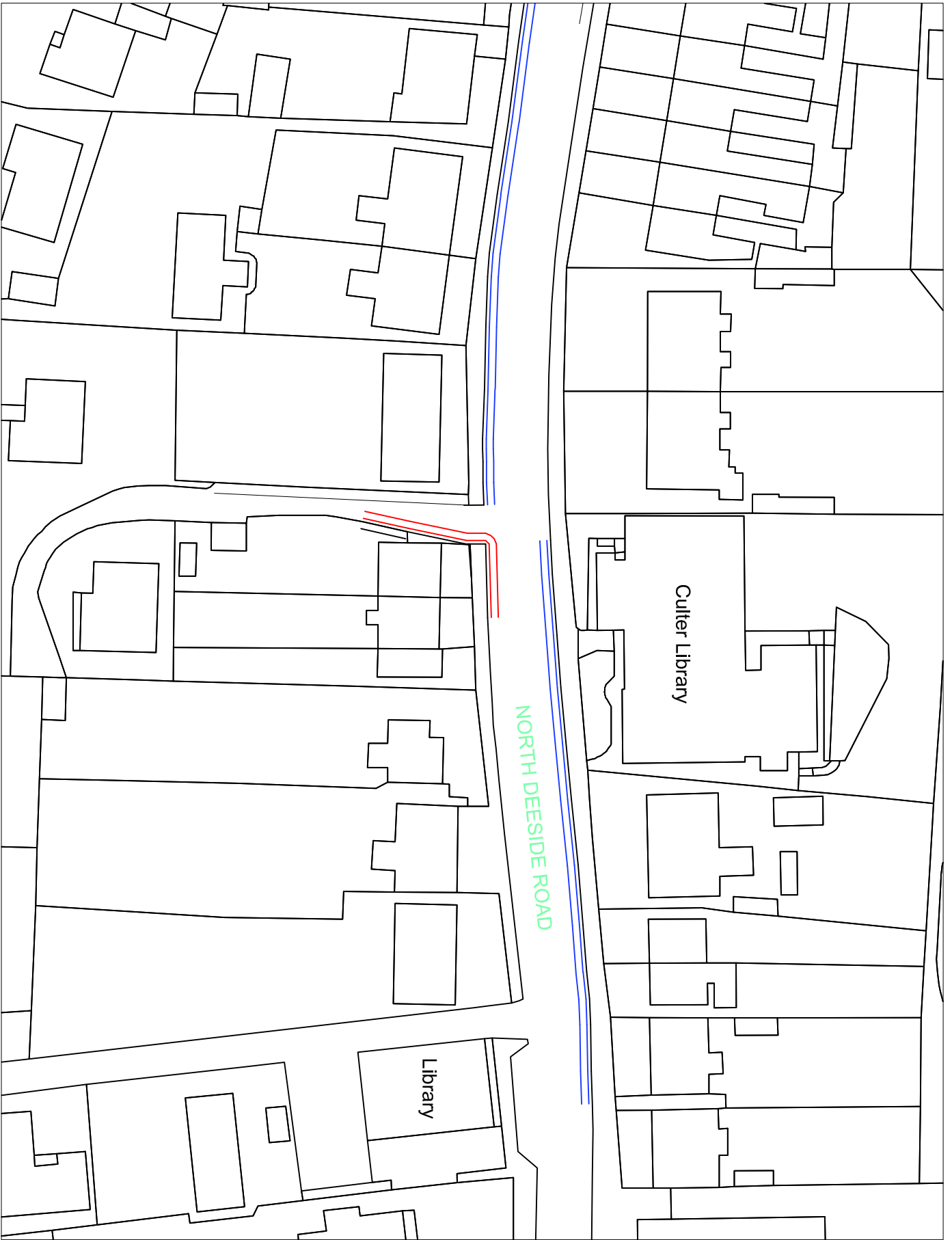
Craigshaw Crescent - Proposed "At any time" waiting restrictions



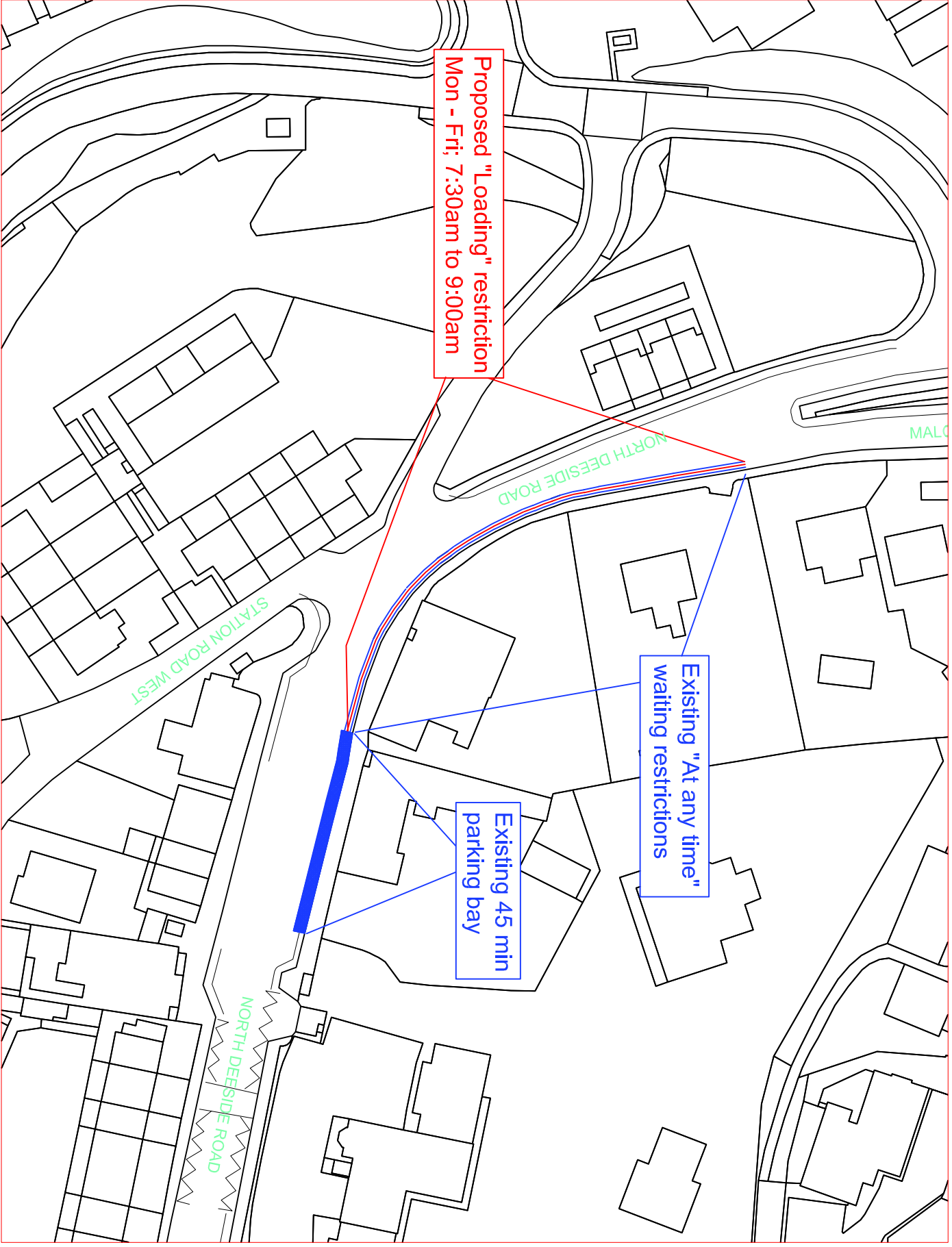
Midstocket View - Proposed "At any time" waiting restrictions



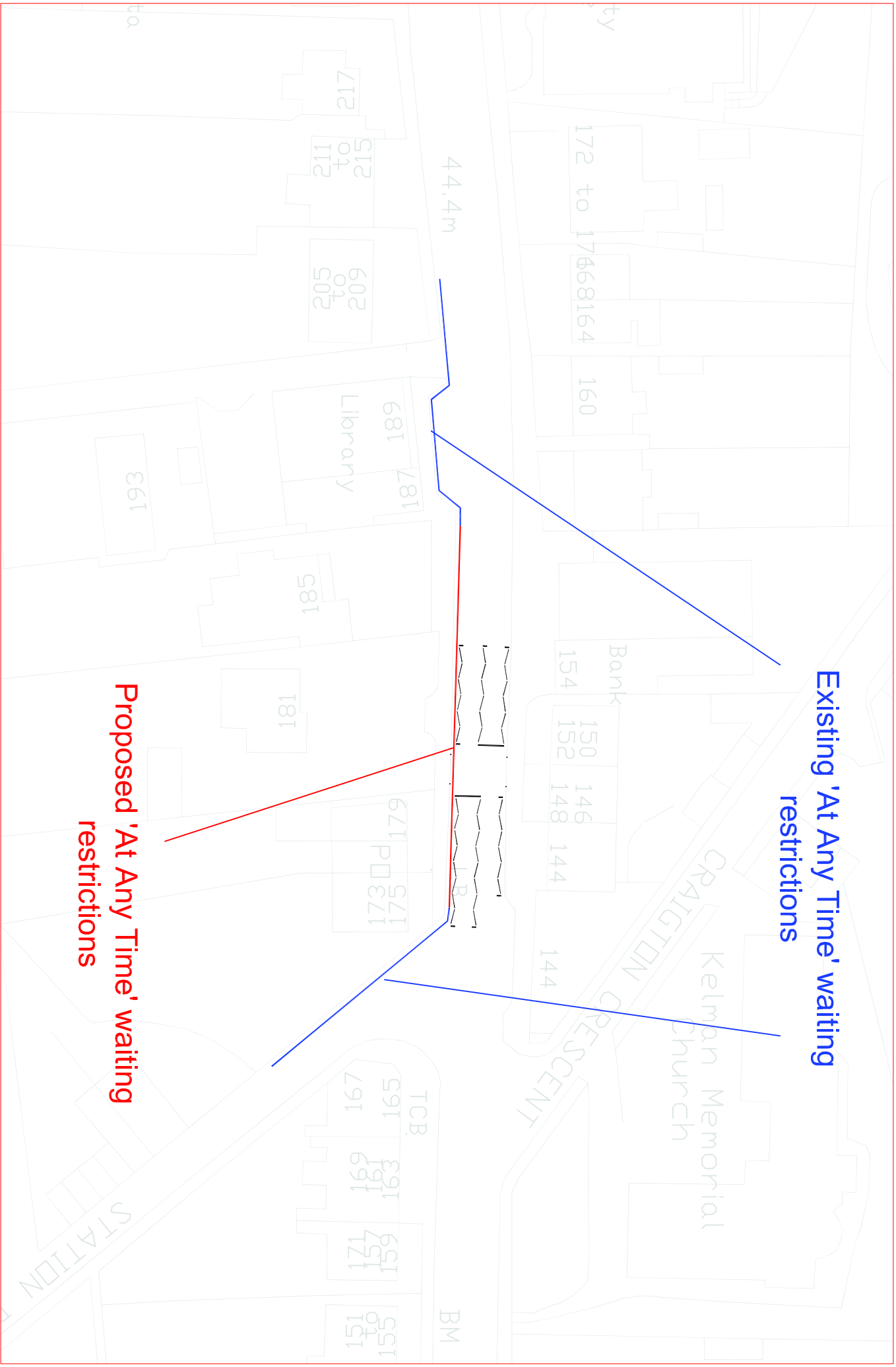
North Deeside Road, Peterculter - Proposed "At any time" waiting restrictions



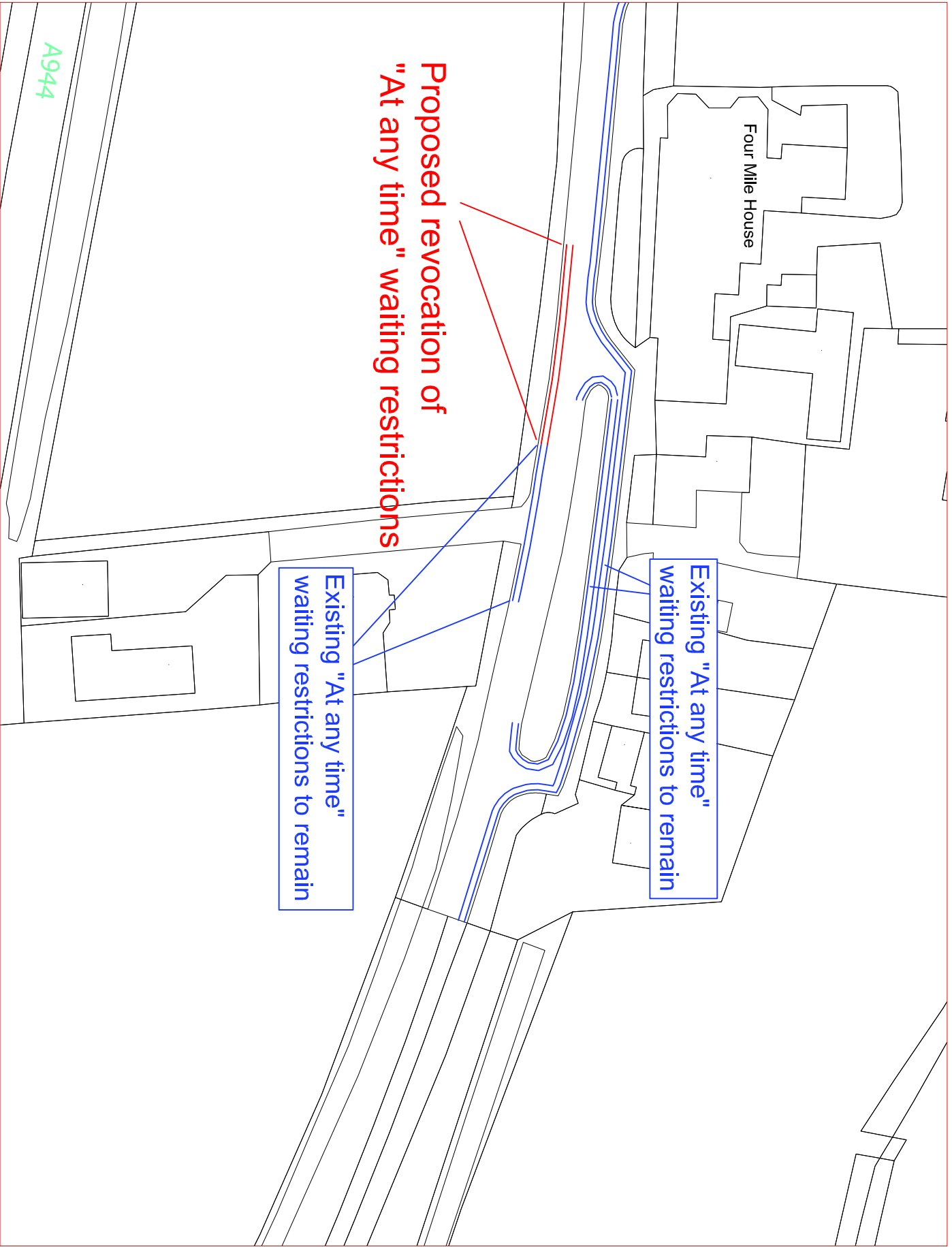
North Deeside Road, Peterculter - Proposed "Loading" restrictions; Mon - Fri
7:30am to 9:00am



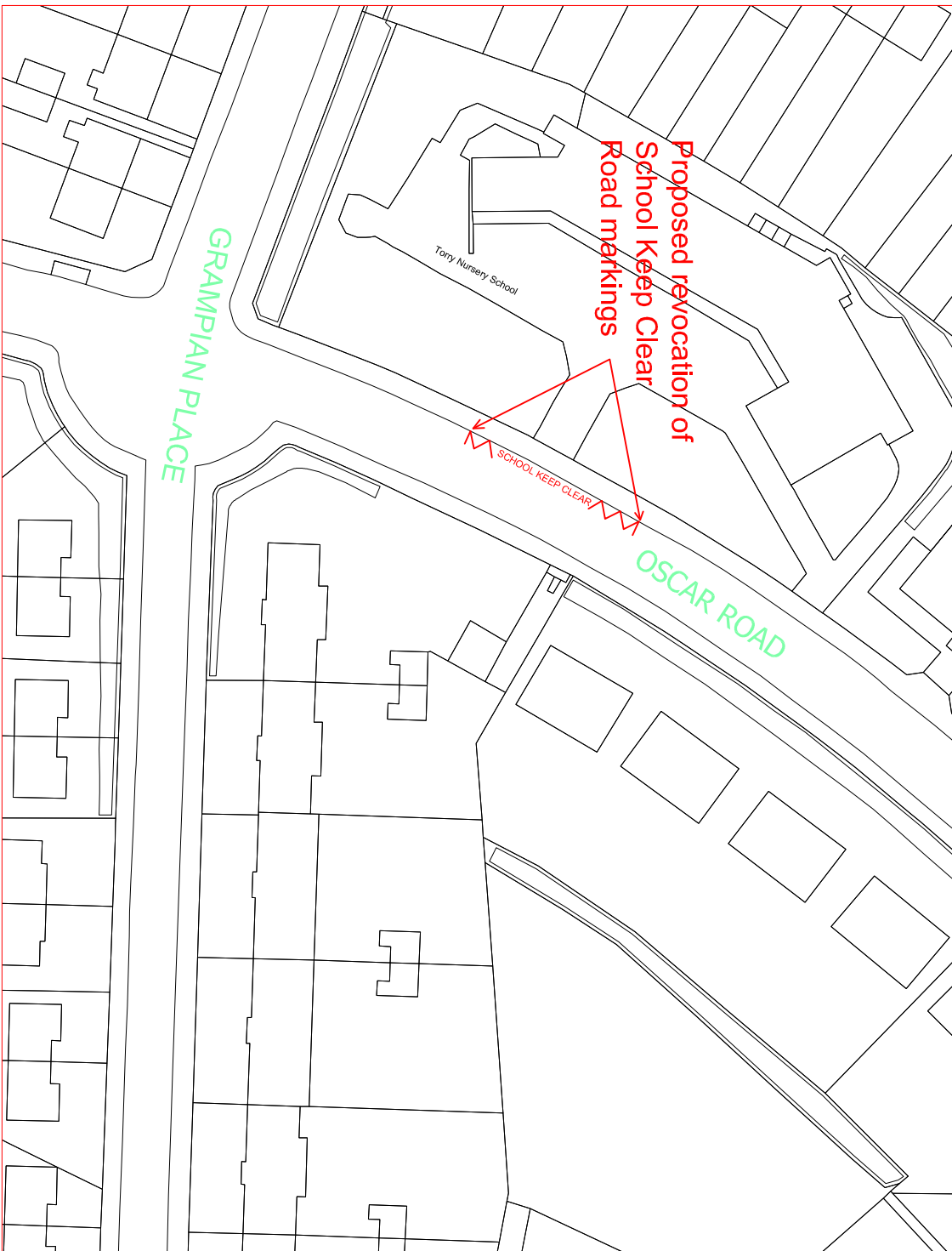
North Deeside Road, Peterculter - Proposed 'At Any Time' waiting restrictions



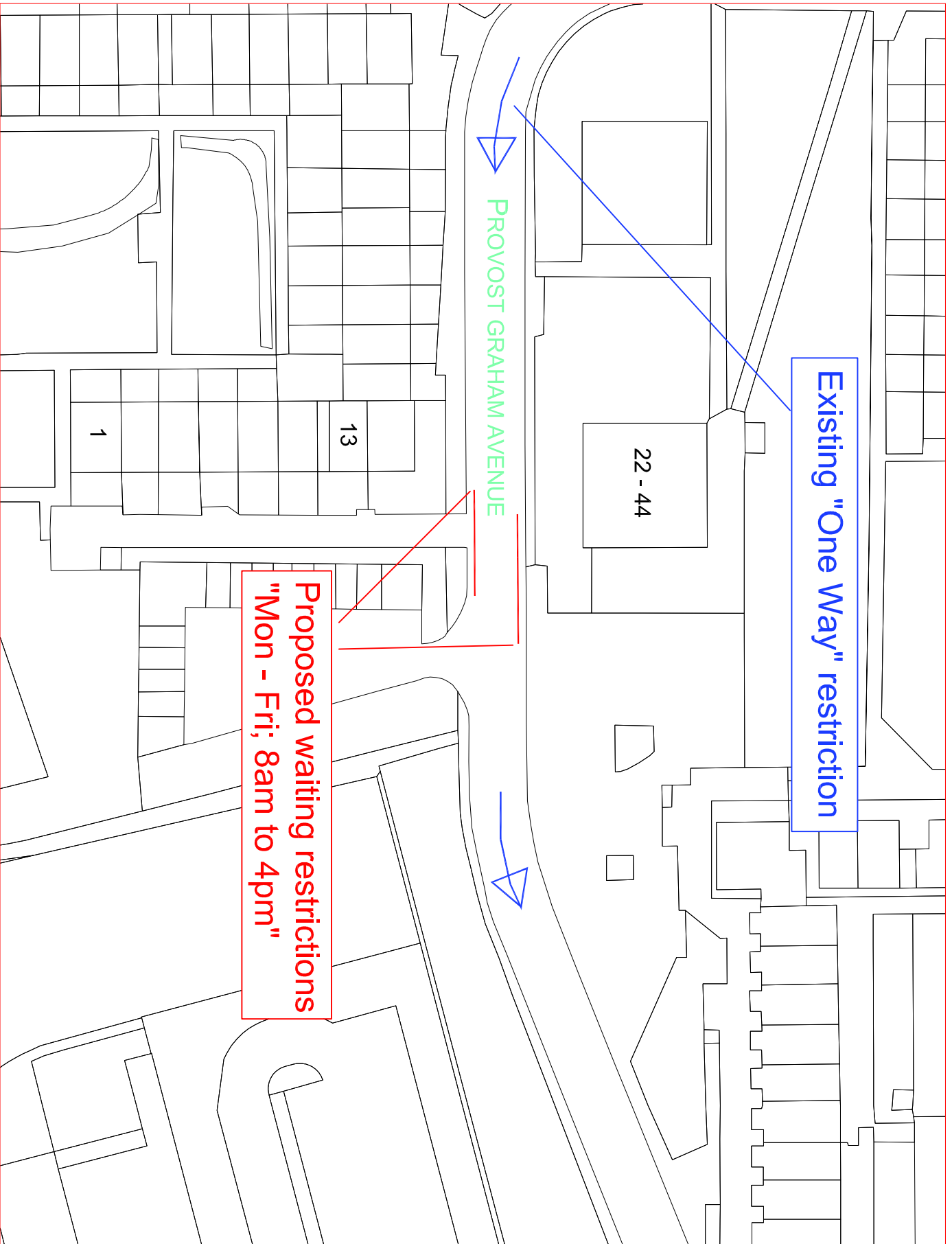
Old Skene Road - Proposed revocation of "At any time" waiting restrictions



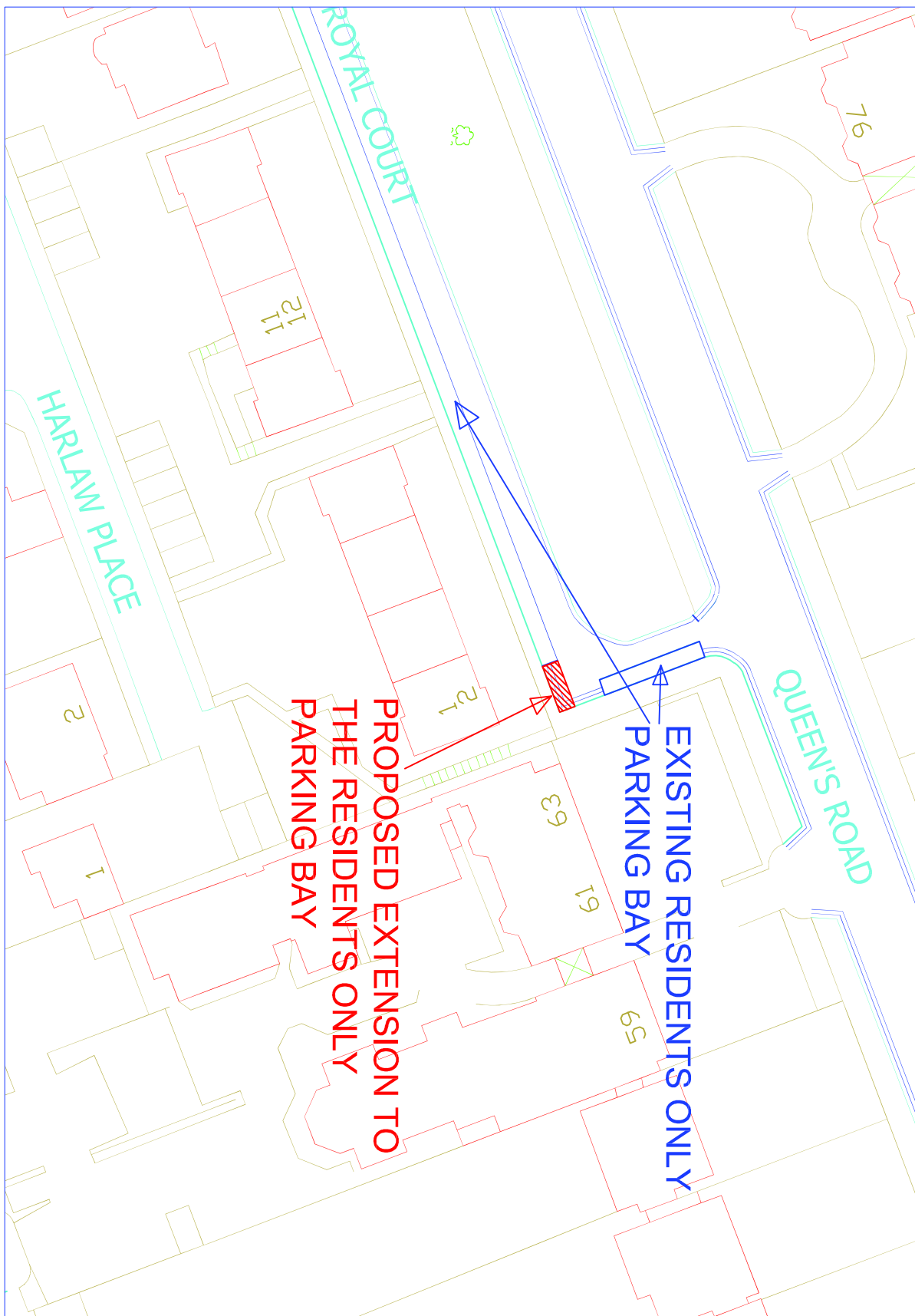
Oscar Road - Revocation of "School Keep Clear" road markings



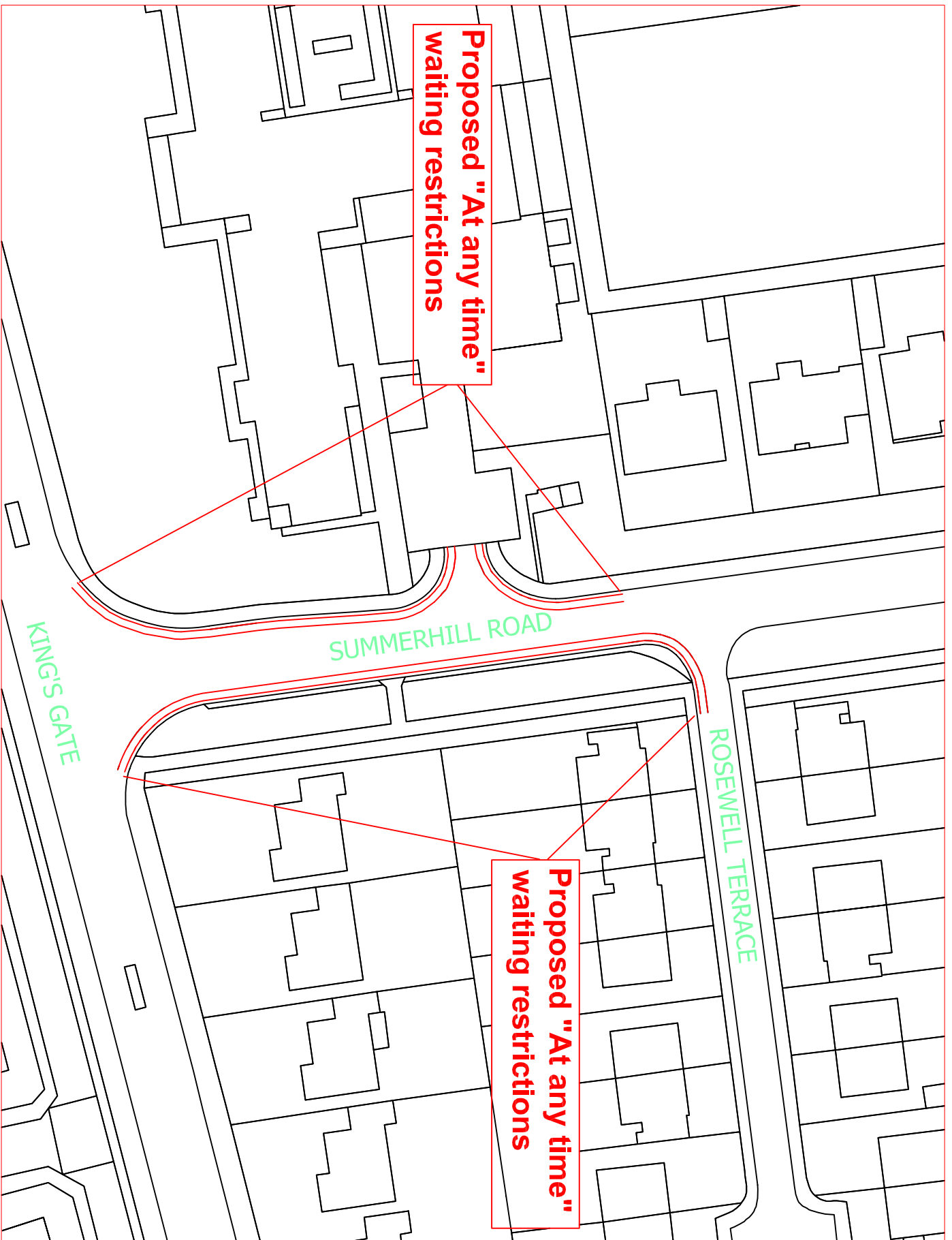
Provost Graham Avenue - Proposed waiting restrictions, Mon - Fri; 8am to 4pm



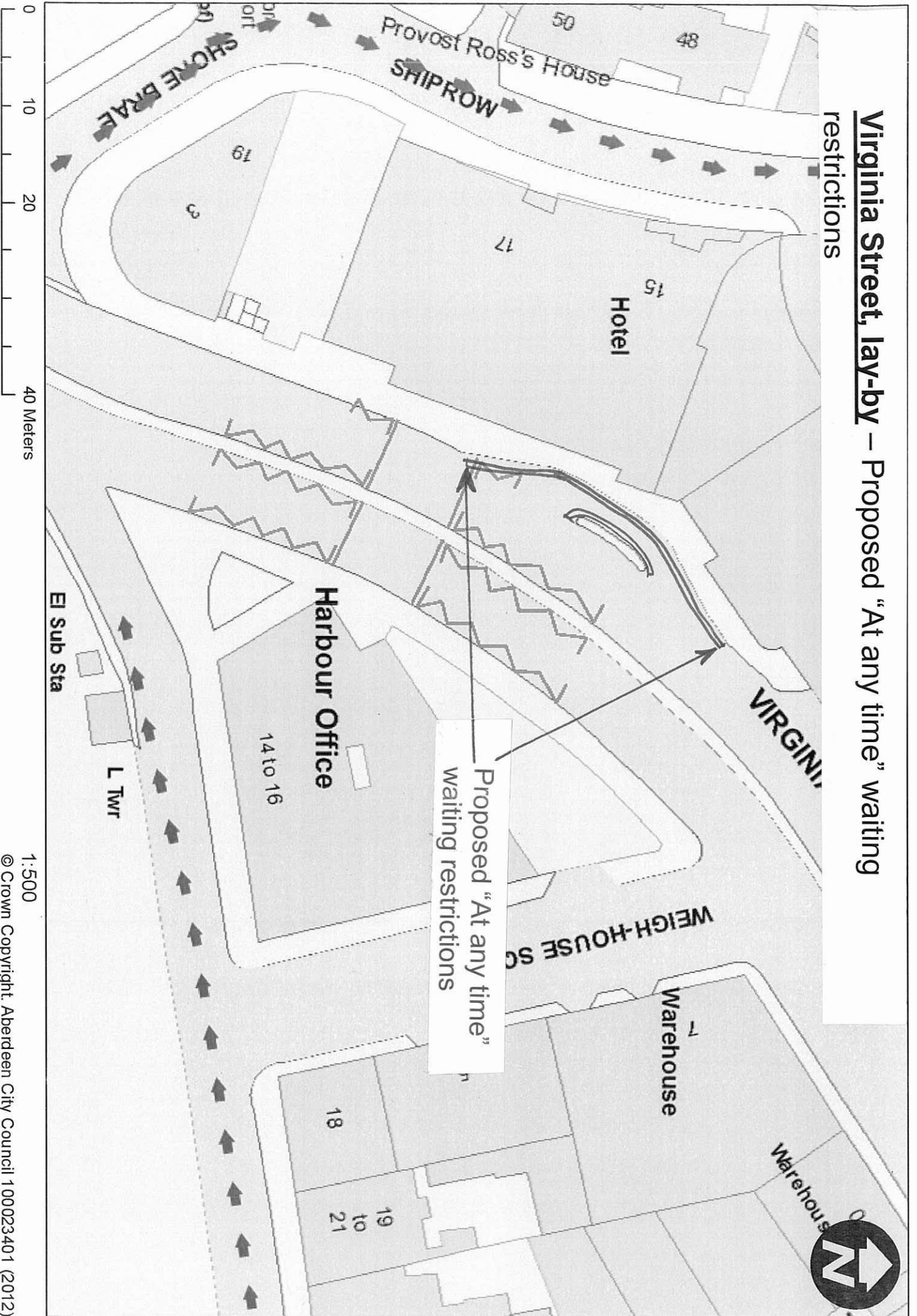
Royal Court, Queen's Road - Extension of "Residents Only" parking bay



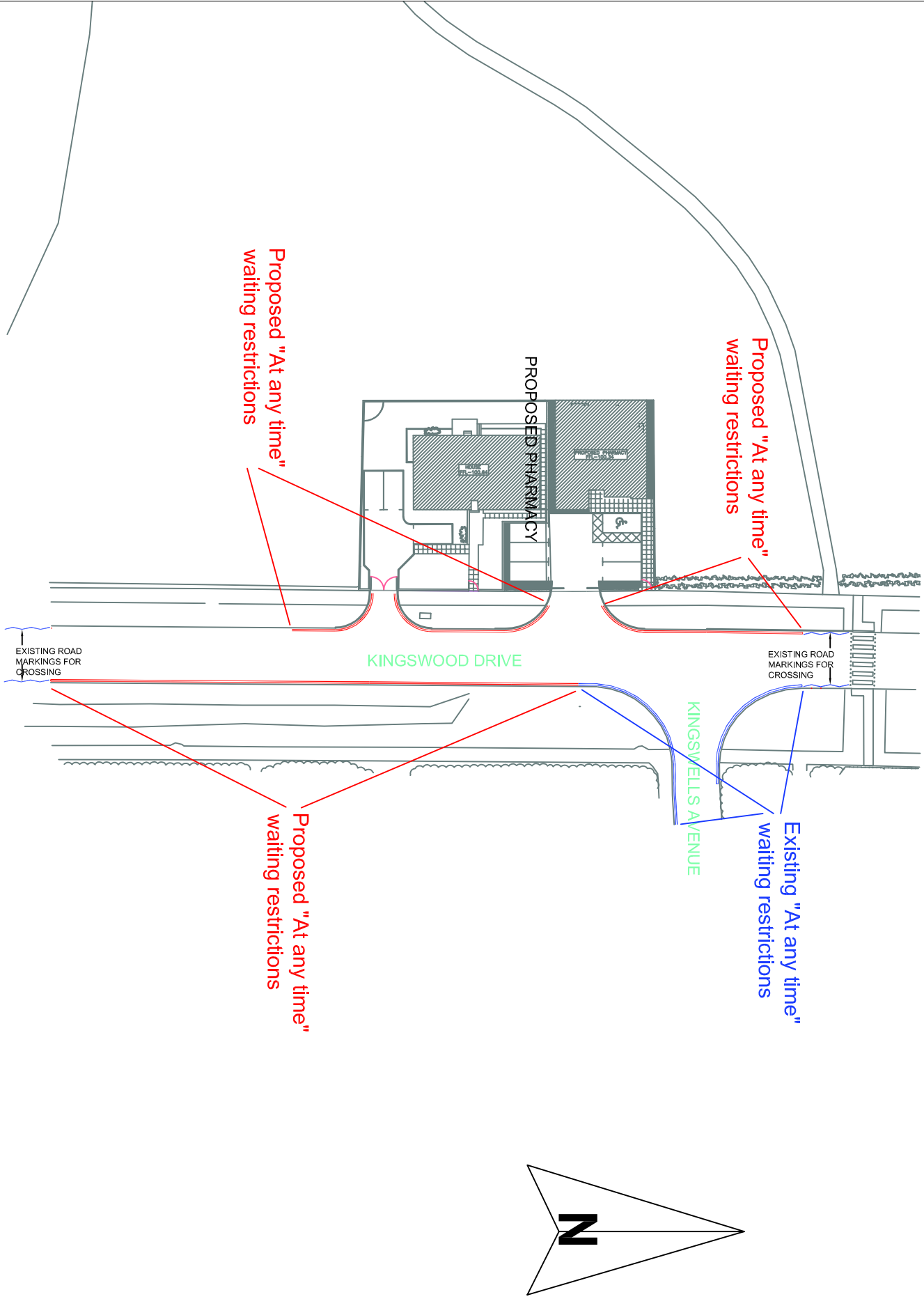
Summerhill Road - Proposed "At any time" waiting restrictions



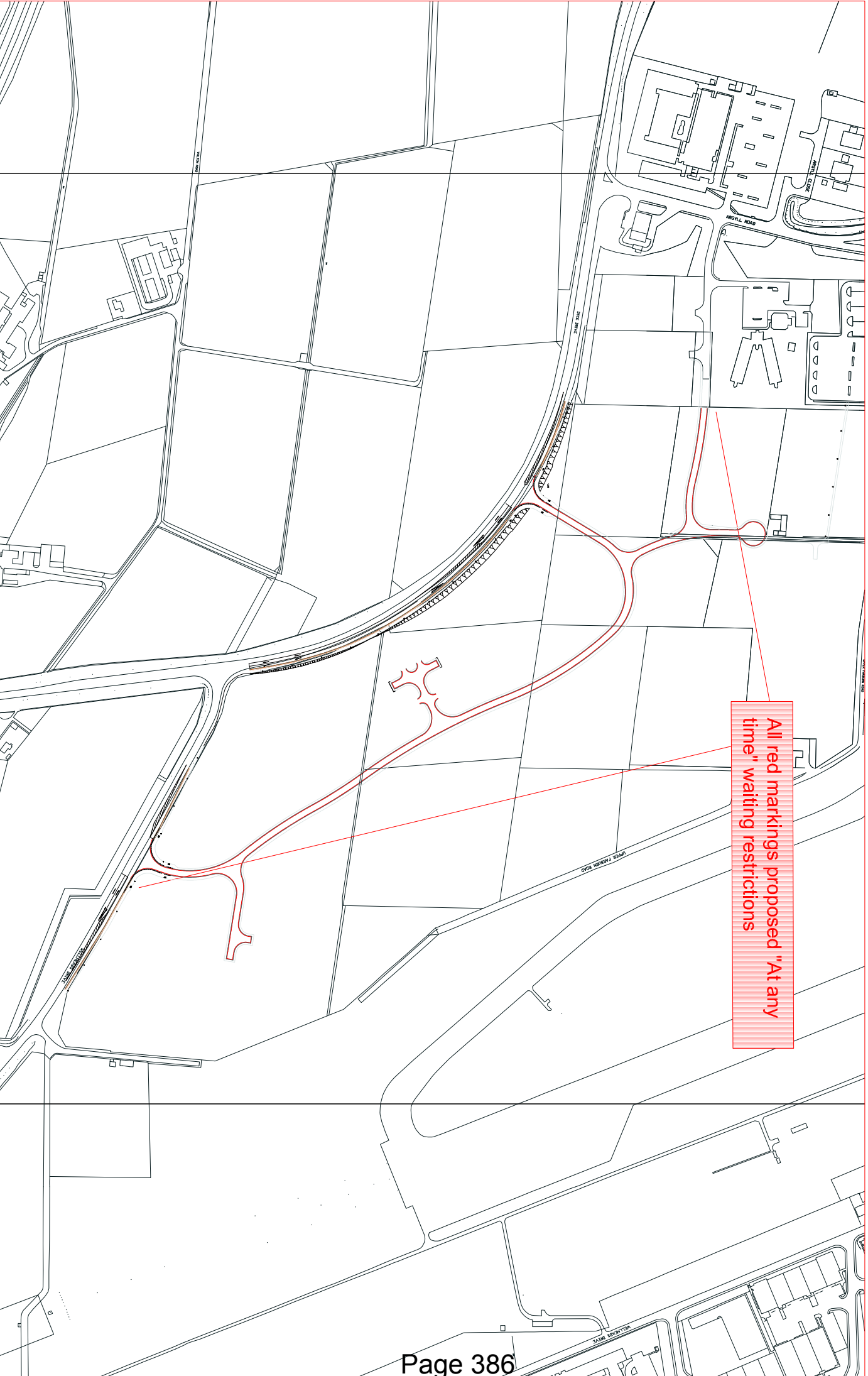
Virginia Street, lay-by – Proposed “At any time” waiting restrictions



Kingswood Drive - Proposed "At any time" waiting restrictions



Wellheads Drive - Proposed "At any time" waiting restrictions



All red markings proposed "At any time" waiting restrictions

➤ **The following proposals will be funded from the Disabled Parking Revenue budget**

➤ **Disabled parking bays to be provided through the Disabled Persons Parking Places (Scotland) Act 2009**

There are 52 on-street applications to be considered at this meeting and 16 off-street applications.

(Plans are not included as, under normal circumstance, they are located close to the properties.)

On-street parking

10b Northfield Place	26b Grampian Road
34 Kirkwall Avenue(Back door Lewis Rd)	31 South Grampian Circle
22 Sheddocksley Road	10 Jesmond Avenue
31 Annfield Terrace	30 Bressay Brae
8 Strathmore Drive	32 Eday square
Hallfield Road (rear of 160 Lang Stracht)	10 Gillahill Place
147 Hallfield Road	17 Annat Bank
137 Cairncry Road	16 Mile-end Avenue
143 Midstocket Road	37 Berrywell Gardens
142 Kincorth Circle	30b Logie Avenue
19 Faulds Gate	58 Mastrick Drive
56 South Grampian Circle	3 Hillock Way
135 School Drive	For 143 Provost Fraser Drive (in Circle)
93 Middlefield Place	36 Bankhead Road
15 Kincorth Place	300 North Balnagask Road
13 Nigg Way	19 Arran Avenue
102 Tollochill Drive	8 Hillside Place
15 Auchinyell Road	51d Bedford Avenue
77 Oscar Road	39 Coningham Gardens
87 Oscar Road	3 Strathburn Street
89 Oscar Road	7 Loirston Place
101 Pittodrie Street	15 Coull Gardens
131 Gray Street	87 Devonshire Road
137 Gray Street	23 Danestone Circle
5 Cardens Knowe	8 Pitmedden Crescent
43 Danestone Terrace	4 Strathburn Street

Off-street parking

39 Shetland Walk	17 Merkland Place
21 Shetland Walk	for 58 Howes Drive o/s 137 Howes Drive
47 Shapinsay Court	5 Whitestripes Place
61 Shapinsay Court	19 Balgownie Brae
4 x Cairncry Court	For 92 Stockethill Court in car park
108 Cairncry Road	2 Logie Place – Healthy Hoose
93 Cairncry Road	

6. IMPACT

This report meets with the local Community Plan objectives to continually improve road safety and maximise accessibility for pedestrians and all modes of transport.

The proposals are in line with the Councils Transportation Strategy to improve safety for all road users by continuing to reduce the number of casualties in traffic collisions.

It is also in accordance with the administration's Vibrant, Dynamic and Forward Looking, under the heading of Transport and highlighted in Paragraph 7.

This report is likely to be of interest to the public in the streets affected by the proposals.

There is no Equality and Human Rights Impact Assessment required as this report only recommends that these proposals progress to the Statutory Consultation process therefore there will be no changes effected as a result of the recommendations being approved by the Committee

7. BACKGROUND PAPERS

N/A

8. REPORT AUTHOR DETAILS

Ruth Milne
Technical Officer,
rumilne@aberdeencity.gov.uk
(01224) 538052

Consultees comments

Councillors

Callum McCaig - has been consulted

Council Officers

Ciaran Monaghan – Head of Service, Office of Chief Executive – has been consulted

Barry Jenkins, Head of Finance, Resources – *has been consulted and has no comments*

Jane MacEachran, City Solicitor, Continuous Improvement - has been consulted

Gordon McIntosh, Director of Enterprise, Planning and Infrastructure – has been consulted

Hugh Murdoch, Head of Service, Shelter and Environment – has been consulted

Margaret Bochel, Head of Planning & Infrastructure, Strategic Leadership – *has been consulted and is in agreement with the recommendations within this report*

Mike Cheyne, Roads Manager - has been consulted

Dave Young, Account Manager, Service, Design and Development - has been consulted

Anne Ross, Service Co-ordinator

Allison Swanson, Committee Services

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	31 May 2012
DIRECTOR	Director of Corporate Governance
TITLE OF REPORT	Disabled Persons' Parking Places Traffic Regulation Orders – Outcome of Main Statutory Advertisement Stage.
REPORT NUMBER:	CG/12/053

1. PURPOSE OF REPORT

This report deals with twenty-one orders at the final statutory stage; that is to say, the main statutory advertisement period is now over in respect of each of these orders and this report presents the objections (where relevant) in each case. The public notices are attached, from which members will be able to see the exact content of the proposals.

The statutory consultation period for the orders has been completed and no objections have been received.

2. RECOMMENDATION(S)

It is recommended that the Committee approve all of the orders, and that all the orders be made as originally envisaged and implemented accordingly.

3. FINANCIAL IMPLICATIONS

The proposals contained in each of the traffic orders detailed in the report will be fully funded by the disabled parking budget.

4. OTHER IMPLICATIONS

There are no other implications worthy of being identified in the abstract here.

5. BACKGROUND/MAIN ISSUES

This section has been sub-divided into sub-sections corresponding to the twenty-one orders under consideration.

5.1 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 4) Order 2012

No statutory objections received.

5.2 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 5) Order 2012

No statutory objections received.

5.3 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 7) Order 2012

No statutory objections received.

5.4 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 8) Order 2012

No statutory objections received.

5.5 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 10) Order 2012

No statutory objections received.

5.6 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 11) Order 2012

No statutory objections received.

5.7 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 12) Order 2012

No statutory objections received.

5.8 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 13) Order 2012

No statutory objections received.

5.9 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 13A) Order 2012

No statutory objections received.

5.10 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 14) Order 2012

No statutory objections received.

5.11 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 15) Order 2012

No statutory objections received.

5.12 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 22) Order 2012

No statutory objections received.

5.13 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 23) Order 2012

No statutory objections received.

5.14 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 24) Order 2012

No statutory objections received.

5.15 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 25) Order 2012

No statutory objections received.

5.16 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 27) Order 2012

No statutory objections received.

5.17 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 28) Order 2012

No statutory objections received.

5.18 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 29) Order 2012

No statutory objections received.

5.19 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 30) Order 2012

No statutory objections received.

5.20 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 32 and 33) Order 2012

No statutory objections received.

5.21 The Aberdeen City Council (Disabled Persons' Parking Places In Aberdeen) (Regulatory Parking Places) (Area 37) Order 2012

No statutory objections received.

6. SERVICE AND IMPACT

Section 5 above – and also the public notices attached – will allow members to consider the possible impact on communities compared with the intended virtue of the original proposals.

7. BACKGROUND PAPERS

No background papers were used as a point of departure for writing this report .

The statutory advertisements are published here for information, allowing members to see the import of each order as advertised.

8. REPORT AUTHOR DETAILS

Allison Swanson
Committee Services Officer
aswanson@aberdeencity.gov.uk
(01224) 522822

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 4) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Albert Place (1) - Ashvale Place (1)- Craigie Avenue (1) - Craigie Street (2) - Eden Place (1) - Huntly Street (1) - Hutcheon Street (1) - John Street (1) - Justice Mill Lane (2) - Kintore Place (1) - Leadside Road (2) - Queen's Terrace (1) Raeburn Place (1) - Richmond Street (3) - Richmond Terrace (1) - Richmond Walk (1)- Rose Street (1) - Rosemount Place (2) - Union Grove (3) - Union Row (1) - View Terrace (1) - West Mount Street (1) - Golden Square (1) - Loanhead Terrace (4) - Mount Street (4) - Northfield Place (1) - Spa Street (3) - Stanley Street (1) - Thistle Street (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 5) ORDER 2012

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SCHEDULE

Crown Terrace (1) - Dee Place (1) - Duff Street (1) - Jasmine Terrace (1) - Jasmine Way (1) - Jopp's Lane (2) - Rennie's Wynd (1) - Schoolhill (2) - Church Street (1) - Flourmill Lane (2) - Gerrard Street (3) - Loch Street (2) - Spring Garden (1) - St Andrew Street (2)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 7) ORDER 2012

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Berrywell Place (3) - Berrywell Road (3) - Kirkhill Place (2) - Kirkhill Road (6) - Polo Gardens (6) - Princess Drive (2) - Todlaw Walk (1) - Tornashean Gardens (3) - Gallowhill Terrace (3) - Gladstone Place (1) - Licklyhead Way (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 8) ORDER 2012

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Ruthriehill Road (1) - Waterton Road (3) - Foresters Avenue (1) - Greenburn Drive (1) - Greenburn Road (1) - Millhill Brae (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 10) ORDER 2012

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Jesmond Avenue North (4) - Whitestripes Drive (13) - Whitestripes Way (1) - Cameron Way (1) - Harehill Road (2) - Overhill Gardens (3) - Shielhill Gardens (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 11) ORDER 2012

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Greenbrae Crescent (2) - Greenbrae Drive (1) - Greenbrae Gardens South (2) - Greenbrae Gardens South (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 12) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Ashtown Walk (3) - Auchmill Road (1) - Bankhead Avenue (3) - Bankhead Road (1) - Bannerman Place (1) - Barvas Walk (1) - Bellfield Road (1) - Bonnyview Drive (3) - Bonnyview Place (6) - Brebner Crescent (3) - Brebner Crescent (2) - Cruden Crescent (2) - Cruden Park (1) - Davidson Drive (1) - Davidson Drive (1) - Davidson Place (2) - Davidson Place (2) - Deveron Road (6) - Dulnain Road (5) - Howes Drive (10) - Howes View (1) - Kepplehills Road (1) - Kingsford Road (2) - Kirkwall Avenue (2) - Lerwick Road (1) - Overhills Walk (1) - Pitdourie Walk (2) - Portree Avenue (5) - Tulloch Park (2) - Wagley Parade (1) - Waulkmill Crescent (1) - Westerton Crescent (1) - Westgate (1) - Brimmondside (5) - Brodinch Road (2) - Byron Crescent (2) - Cairnhillock Place (1) - Cloverfield Close (1) - Cloverfield Gardens (2) - Cloverhill Crescent (4) - Greenfern Avenue (2) - Lintmill Place (1) - Longview Terrace (1) - Lossie Place (2) - Marchburn Court Road (2) - Middlemuir Place (2) - Mossie Way (2) - Ness Place (1) - Netherhills Avenue (1) - Newhills Avenue (1) - Orkney Avenue (1) - Sheddocksley Road (5) - Shetland Walk (2) - Smithyhaugh Road (1) - Springhill Crescent (2) - St John's Road (1) - Sumburgh Crescent (1) - Taransay Crescent (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 13) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Arnage Crescent (1) - Arnage Drive (2) - Arnage Gardens (1) - Arnage Place (1) - Ashgrove Place (5) - Birkhall Parade (8) - Birkhall Place (2) - Craigendarroch Avenue (2) - Craigendarroch Place (2) - Cummings Park Circle (2) - Cummings Park Crescent (6) - Cummings Park Drive (5) - Deansloch Crescent (2) - Deansloch Terrace (1) - Dominies Road (2) - Heathryfold Circle (4) - Heathryfold Drive (3) - Heathryfold Place (1) - Hilton Avenue (6) - Invercauld Gardens (1) - Invercauld Road (5) - Kemp Street (1) - Plane Tree Road (1) - Privet Hedges (2) - Provost Fraser Drive (1) - Provost Rust Drive (2) - Provost Rust Drive (4) - Rosehill Avenue (2) - Rosehill Drive (1) - Ross Crescent (2) - Rowan Road (2) - Rowan Road (2) - Ugie Place (2) - Willowpark Place (1) - Willowpark Road (2) - Brierfield Terrace (3) - Cairnwell Avenue (2) - Cairnwell Drive (2) - Cairnwell Place (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 13A) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Caperstown Crescent (4) - Carron Place (1) - Cherry Road (1) - Clifton Road (3) - Clifton Road (4) - Corndavon Terrace (1) - Cornhill Drive (1) - Cornhill Drive (3) - Cornhill Terrace (4) - Gadie Crescent (1) - Gairnshiel Place (1) - Gillahill Place (2) - Gillahill Road (1) - Granitehill House (1) - Logie Avenue (1) - Logie Place (1) - Long Walk Place (1) - Long Walk Road (2) - Manor Avenue (5) - Manor Walk (2) - Marchburn Avenue (2) - Marchburn Crescent (1) - Marchburn Drive (2) - Middlefield Crescent (7) - Middlefield Place (6) - Middlefield Terrace (2) - Muirfield Road (3) - North Anderson Drive (3) - Oldcroft Place (3) - Oldtown Place (1) - Smithfield Drive (3) - Stewart Crescent (1) - Stewart Park Place (Unadopted) (11) - Stewart Terrace (1) - Stockethill Crescent (3) - Strathmore Drive (1) - Strathmore Drive (2) - Tay Road (4)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 14) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Alexander Drive (1) - Auchinleck Crescent (3) - Auchinleck Road (2) - Balgownie Brae (1) - Balgownie Way (3) - Beattie Place (1) - Bedford Avenue (4) - Coull Gardens (1) - Deer Road (3) - Denmore Gardens (2) - Don Street (1) - Don Street (1) - Don Street (1) - Donbank Place (1) - Erskine Street (2) - Hayfield Crescent (1) - Hayton Road (2) - Hilton Drive (5) - Hilton Road (5) - Hilton Street (2) - Hilton Terrace (2) - Hilton Walk (1) - Laurelwood Avenue (2) - Laurelwood Avenue (2) - Leslie Terrace (2) - Pennan Road (3) - Powis Circle (1) - Printfield Walk (2) - Queen Street (1) - Sandilands Drive (2) - Wavell Crescent (2) - Western Road (4) - Cadenhead Place (2) - Cadenhead Road (1) - Cairnfold Road (2) - Carnie Drive (3) - Cattofield Place (8) - Chestnut Row (2) - Chestnut Row (1) - Coningham Gardens (4) - Coningham Terrace (1) - Ferrier Crescent (2) - Ferrier Gardens (2) - Formartine Road (2) - Fowlershill Gardens (1) - Fraser Road (1) - Froghall Avenue (2) - Froghall Terrace (2) - Gillespie Place (1) - Gladstone Place (2) - Lilybank Place (1) - Marquis Road (1) - Stafford Street (2) - Sunnybank Place (2) - Tarbothill Road (2)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 15) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Ardarroch Place (1) - Ardarroch Road (2) - Danestone Terrace (1) - Erroll Place (3) - Erroll Street (4) - Pittodrie Place (8) - Pittodrie Street (3) - Regent Walk (1) - Regent Walk (3) - Roslin Street (3) - School Avenue (1) - School Drive (8) - School Drive (13) - School Place (5) - School Road (1) - School Road (1) - School Terrace (1) - School Walk (4) - Scotstown Gardens (3) - Seaforth Road (3) - Urquhart Street (2) - Golf Road (1) - Linksfield Road (7) - Lord Hays Court Car Park (7) - Merkland Place (2) - Nelson Street (1) - Seaton Avenue (1) - Seaton Crescent (1) - Seaton Drive (1) - Seaton Drive (2) - Seaton Place East (1) - Seaton Road (1) - Seaton Road (1) - Seaton Walk (2) - Simpson Road (3)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 22) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Anderson Avenue (2) - Angusfield Avenue (1) - Croft Place (1) - Croft Road (2) - Derbeth Crescent (5) - Eday Crescent (6) - Eday Drive (1) - Eday Road (4) - Eriskay Drive (2) - Fairley Road (1) - Hazlehead Gardens (3) - Hazlehead Terrace (1) - Hillylands Road (3) - Lewis Road (1) - Provost Graham Avenue (1) - Ronaldsay Square (6) - Westray Crescent (1) - Westray Road (3) - Willowpark Crescent (3) - Windford Road (1) - Bressay Brae (4) - Burnbrae Avenue (1) - Burnbrae Crescent (6) - Burnbrae Place (2) - Cloghill Place (2) - Fernhill Road (1) - Fernielea Road (1) - Gairsay Drive (2) - Gairsay Road (3) - Hallfield Road (5) - Mull Way (1) - Shapinsay Court (6) - Shapinsay Road (2) - Sheddocksley Drive (4) - Stronsay Crescent (1) - Stronsay Place (1) - Summerhill Crescent (2) - Summerhill Drive (5) - Taransay Road (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 23) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Allan Street (1) - Anderson Drive (3) - Annfield Terrace (2) - Ashley Gardens (1) - Beaconsfield Place (3) - Bonnymuir Place (2) - Craighton Road (4) - Devonshire Road (2) - Duthie Place (1) - Duthie Terrace (1) - Harlaw Road (1) - Hartington Road (2) - Hosefield Avenue (1) - Irvine Place (1) - Learney Place (1) - Richmondhill Place (3) - Ruthrieston Circle (10) - Ruthrieston Road (4) - Salisbury Terrace (1) - Whitehall Road (2) - Brighton Place (2) - Broomhill Road (2) - Burns Road (1) - Cairnfield Place (1) - Forbesfield Road (1) - Gordon Terrace (1) - Granville Place (1) - Gray Street (1) - Hamilton Place (1) - Hammersmith Road (1) - Louisville Avenue (1) - Mile-End Avenue (2) - Moray Place (1) - Nellfield Place (1) - Osborne Place (2) - South Anderson Drive (2) - Thorngrove Avenue (2)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 24) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

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Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Bank Street (2) - Battock Place (1) - Kintore Gardens (1) - Lemon Place (3) - Park Place (1) - Polwarth Road (2) - Polwarth Road (1) - Victoria Road (2) - Victoria Road (3) - Walker Road (4) - Walker Road (1) - Brunswick Place (1) - Correction Wynd (1) - Farquhar Road (3) - Girdleness Terrace (3) - Glenbervie Road (8) - Grampian Place (1) - Grampian Road (6) - Mansefield Place (2) - Morven Place (1) - Oscar Road (2) - South Grampian Circle (1) - South Silver Street (2)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 25) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Abbotsfold Place (1) - Balnagask Avenue (1) - Balnagask Circle (3) - Balnagask Road (10) - Pentland Crescent (1) - Tullos Crescent (2) - Tullos Place (1) - Girdleness Road (4) - Girdleness Road (5) - Loirston Place (5) - Mansefield Road (1) - North Balnagask Road (3)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 27) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Garden Road (1) - Golf Road (2)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 28) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Ashfield Road (1) - South Avenue (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 29) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Aboyne Place (1) - Auchinyell Road (1) - Auchinyell Terrace (2) - Craigievar Place (2) - Deevale Terrace (3) - Devenick Place (1) - Inchbrae Drive (1) -Ivanhoe Road (3) - Pitmedden Crescent (1) - Two Mile Cross (4) - Cairngorm Road (1) - Gardner Road (2) - Garthdee Drive (1) - Garthdee Gardens (1) - Morrison Drive (3) - Talisman Road (3)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 30) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Abbotswell Crescent (1) - Arbroath Way (1) - Bervie Brow (1) - Corthan Place (1) - Covenanters Drive (3) - Craigmaraoinn Gardens (2) - Crawton Ness (1) - Deevale Crescent (3) - Deevale Road (4) - Doolie Ness (1) - Kincorth Circle (3) - Kincorth Crescent (1) - Kincorth Gardens (1) - Kincorth Place (2) - Laws Road (2) - Laws Road (5) - Tollohill Crescent (3) - Tollohill Gardens (3) - Tollohill Place (3) - Buchanan Place (1) - Caiesdykes Drive (2) - Caiesdykes Road (4) - Caiesdykes Road (2) - Cairngorm Crescent (2) - Cairngorm Gardens (2) - Corthan Crescent (2) - Corthan Drive (1) - Faulds Crescent (2) - Faulds Gate (8) - Faulds Row (1) - Matthews Road (1) - Nigg Way (3) - Seal Craig Gardens (2) - Slessor Drive (1) - Slessor Road (1) - Strathburn Street (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 32 AND 33) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

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Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Johnston Gardens East (2) - Johnston Gardens West (2) - Towerview Road (1) - Ardbeck Place (1) - Crown Crescent (1) - Coronation Road (1) - Contlaw Place, Milltimber (1)

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (DISABLED PERSONS' PARKING PLACES IN ABERDEEN) (REGULATORY PARKING PLACES) (AREA 37) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984 and its duties under the Disabled Persons' Parking Places (Scotland) Act 2009.

The effect of the order is to make all existing advisory disabled person on-street parking places listed in the schedule below *regulatory* in accordance with the requirements of the Disabled Persons' Parking Places (Scotland) Act 2009. To that extent, the Order will be to make it an offence for anyone to park in a marked disabled persons' bay unless they are a holder of a valid blue badge and are using it in accordance with the national blue badge legislation. The existing parking place will be reserved for the exclusive use of any blue badge holder and will be re-lined and signed. A list of all existing parking places affected are detailed in the schedule below and can be found on the Council's website, www.aberdeencity.gov.uk, in all local libraries, and community centres.

The draft order, together with a schedule showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, ABERDEEN

SCHEDULE

Burnbutts Crescent (1) - Stoneyhill Terrace (1)

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ABERDEEN CITY COUNCIL

COMMITTEE Enterprise, Planning and Infrastructure
DATE 31 May 2012
DIRECTOR Director of Corporate Governance
TITLE OF REPORT Various Traffic Orders – Outcome of Main Statutory Advertisement Stage:-

1. **The Aberdeen City Council (Burnside Road, Dyce, Aberdeen)(Traffic Management) Order 2012**
2. **The Aberdeen City Council (Cairn Road, Peterculter, Aberdeen)(Prohibition Of Waiting) Order 2012**
3. **The Aberdeen City Council (Grampian Road, Torry, Aberdeen)(Traffic Management) Order 2012**
4. **The Aberdeen City Council (Scott Cassie Circle, Garthdee, Aberdeen)(Prohibition Of Motor Vehicles) Order 2012**
5. **The Aberdeen City Council (Greenbank Crescent, Aberdeen)(Traffic Management) Order 2012**
6. **The Aberdeen City Council (Foresterhill Road Area, Aberdeen)(Traffic Management) Order 2012**
7. **The Aberdeen City Council (A96 / Old Meldrum Road / Mugiemoos Road, Aberdeen)(Shared Pedestrian/Cycle Path) Order 2012**
8. **The Aberdeen City Council (Access Road Serving Balnagask House, Aberdeen)(Ambulance Bay) Order 2012**
9. **The Aberdeen City Council (Bedford Road Area, Aberdeen)(Traffic Management) Order 2012**
10. **The Aberdeen City Council (Frederick Street Off-Street Car Park) Order 2012**
11. **The Aberdeen City Council (Marischal College Multi-Storey Off-Street Car Park – Aberdeen City Council Staff Permits) Order 2012**

REPORT NUMBER: CG/12/055

1. PURPOSE OF REPORT

This report deals with eleven orders at the final statutory stage; that is to say, the main statutory advertisement period is now over in respect of each of these orders and this report presents the objections (where relevant) in each case. The public notices are attached, from which members will be able to see the exact content of the proposals.

The statutory consultation period for the orders has been completed and no objections have been received in relation to the orders hereto.

2. RECOMMENDATION(S)

It is recommended that the Committee approve all of the orders, and that all the orders be made as originally envisaged and implemented accordingly.

3. FINANCIAL IMPLICATIONS

The proposals contained in the each of the orders will be fully funded by individual developers.

4. OTHER IMPLICATIONS

There are no other implications worthy of being identified in the abstract here, although, again, both Section 5 and the appendix rehearse concerns raised by objectors.

5. BACKGROUND/MAIN ISSUES

This section has been sub-divided into sub-sections corresponding to the fifteen orders under consideration.

5.1 The Aberdeen City Council (Burnside Road, Dyce, Aberdeen)(Traffic Management) Order 2012

No statutory objections received.

5.2 The Aberdeen City Council (Cairn Road, Peterculter, Aberdeen)(Prohibition Of Waiting) Order 2012

No statutory objections received.

5.3 The Aberdeen City Council (Grampian Road, Torry, Aberdeen)(Traffic Management) Order 2012

No statutory objections received.

5.4 The Aberdeen City Council (Scott Cassie Circle, Garthdee, Aberdeen)(Prohibition Of Motor Vehicles) Order 2012

No statutory objections received.

5.5 The Aberdeen City Council (Greenbank Crescent, Aberdeen)(Traffic Management) Order 2012

No statutory objections received.

5.6 The Aberdeen City Council (Foresterhill Road Area, Aberdeen)(Traffic Management) Order 2012

No statutory objections received.

5.7 The Aberdeen City Council (A96 / Old Meldrum Road / Mugiemooss Road, Aberdeen)(Shared Pedestrian/Cycle Path) Order 2012

No statutory objections received.

5.8 The Aberdeen City Council (Access Road Serving Balnagask House, Aberdeen)(Ambulance Bay) Order 2012

No statutory objections received.

5.9 The Aberdeen City Council (Bedford Road Area, Aberdeen)(Traffic Management) Order 2012

No statutory objections received.

5.10 The Aberdeen City Council (Frederick Street Off-Street Car Park) Order 2012

No statutory objections received.

5.11 The Aberdeen City Council (Marischal College Multi-Storey Off-Street Car Park – Aberdeen City Council Staff Permits) Order 2012

No statutory objections received.

6. SERVICE AND IMPACT

Section 5 above – and also the public notices attached – will allow members to consider the possible impact on communities compared with the intended virtue of the original proposals.

7. BACKGROUND PAPERS

No background papers were used as a point of departure for writing this report.

The statutory advertisements are published here for information, allowing members to see the import of each order as advertised.

8. REPORT AUTHOR DETAILS

Allison Swanson
Committee Services Officer
aswanson@aberdeencity.gov.uk
(01224) 522822

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(BURNSIDE ROAD, DYCE, ABERDEEN)(TRAFFIC MANAGEMENT) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to introduce prohibitions of waiting at any time on *certain lengths* of Burnside Road, Dyce, Aberdeen. Exemptions will apply as usual to the picking up or setting down of passengers, loading or unloading, blue badge holders not causing an obstruction, funeral vehicles, and vehicles parked with the consent of the Council in direct association with authorised roadworks or building works.

This order also proposes the establishment a mandatory 20mph speed limit on a *certain length* of Burnside Road.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(CAIRN ROAD, PETERCULTER, ABERDEEN)(PROHIBITION OF WAITING) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to introduce a prohibition of waiting at any time on the west side of Cairn Road, Peterculter. Exemptions will apply as usual to the picking up or setting down of passengers, loading or unloading, blue badge holders not causing an obstruction, funeral vehicles, and vehicles parked with the consent of the Council in direct association with authorised roadworks or building works.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(GRAMPIAN ROAD, TORRY, ABERDEEN)(TRAFFIC MANAGEMENT) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to establish a certain length of parking bay on Grampian Road for exclusive use by Police vehicles. This order will also establish an additional length of parking bay on Grampian Road with a maximum stay of 45 minutes and no return within 15 minutes, Monday to Saturday, 8am to 6pm.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(SCOTT CASSIE CIRCLE, GARTHDEE, ABERDEEN)(PROHIBITION OF MOTOR VEHICLES) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to establish a prohibition of motor vehicles except for access on part of Scott Cassie Circle, Garthdee, Aberdeen.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(GREENBANK CRESCENT, ABERDEEN)(TRAFFIC MANAGEMENT) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to establish certain lengths of prohibition of waiting at any time on Greenbank Crescent. Exemptions will apply as usual to the picking up or setting down of passengers, loading or unloading, blue badge holders not causing an obstruction, funeral vehicles, and vehicles parked with the consent of the Council in direct association with authorised roadworks or building works.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(FORESTERHILL ROAD AREA, ABERDEEN)(TRAFFIC MANAGEMENT) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of this order will be to both restate existing traffic management measures and introduce further measures on Foresterhill Road and at the Bus Interchange at the main access to Aberdeen Royal Infirmary.

A prohibition of motor vehicles except for access will be established on a length of Foresterhill Road north of its junction with the southern access to the un-named perimeter road (the un-named perimeter road is the road to the west of Foresterhill Road that provides access to the main hospital car parking facilities and the National Hyperbaric Centre). Thereafter, beyond the accesses to the Aberdeen Blood Donor Centre (on the west side of this road) and the service roads to the hospital complex (on the east side of this road), a no entry will prohibit access to northbound vehicles except for buses. A further no entry will be established at its junction with the northern access to the un-named perimeter road and prohibit access to southbound vehicles except for buses. Access for pedal cycles will be maintained by way of bypass facilities at the no entry points. The aforementioned measures therefore prohibit general access to the mid-section of Foresterhill Road, adjacent to the hospital complex, and those vehicles not exempt from these restrictions will be required to travel by way of the un-named perimeter road.

At the northern access to the un-named circular road serving as the Bus Interchange, adjacent to the main access to Aberdeen Royal Infirmary, a no entry except for buses will be established. On this road vehicles will only be permitted to travel in a clockwise direction.

There would also be certain lengths of prohibitions of waiting at any time established on these roads.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD SCOTLAND ACT 1984

**THE ABERDEEN CITY COUNCIL
(A96 / OLD MELDRUM ROAD / MUGIEMOSS ROAD, ABERDEEN)(SHARED PEDESTRIAN/CYCLE PATH)
ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Scotland Act 1984. The effect of this order will be to establish *certain lengths* of shared pedestrian/pedal cycle path on the A96, Old Meldrum Road and Mugiemoos Road.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(ACCESS ROAD SERVING BALNAGASK HOUSE, ABERDEEN)(AMBULANCE BAY) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of this order will be to establish a *certain length* of parking bay on the access road serving Balnagask House for exclusive use by ambulances.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

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Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(BEDFORD ROAD AREA, ABERDEEN)(TRAFFIC MANAGEMENT) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to introduce prohibitions of waiting at any time on *certain lengths* of Bedford Road and the access roads serving Aberdeen University Library. *Certain lengths* of prohibition of waiting, Monday to Friday, 8.00am to 6.00pm, would also be established on an access road serving Aberdeen University Library. Exemptions will apply as usual to the picking up or setting down of passengers, loading or unloading, blue badge holders not causing an obstruction, funeral vehicles, and vehicles parked with the consent of the Council in direct association with authorised roadworks or building works.

Fourteen individual disabled parking bays will also be established on two of the access roads serving Aberdeen University Library and will be reserved for exclusive use by any 'blue badge' holder.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

THE ABERDEEN CITY COUNCIL (FREDERICK STREET OFF-STREET CAR PARK) ORDER 2012

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to introduce pay and display parking within the new Frederick Street off-street car park. The charges will be applicable from 8.00am to 10.00pm, Monday to Saturday; and Sunday, 1.00pm to 5.00pm. The cost for a period up to 2 hours will be £2.00, up to three hours will be £3.00, and up to a maximum of 4 hours will be £4.00.

With respect to the proposed pay and display regime, an invalid carriage or disabled person's vehicle will be exempt from payment of any charge, and from compliance with any limitation of time, providing that it displays a disabled persons badge ("the blue badge") issued by a local authority. The Council may also reserve a suitable number of spaces for invalid carriages or disabled persons' vehicles and no person shall cause or permit any vehicle to wait in any such space at any time without displaying the related badge ("the blue badge") issued by a local authority. The Council may also reserve a suitable numbers of spaces for use only by motorcycles (at no charge), and no person shall cause or permit any vehicle to wait in these spaces unless that vehicle is a motorcycle.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April and 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL (MARISCHAL COLLEGE MULTI-STOREY OFF-STREET CAR PARK –
ABERDEEN CITY COUNCIL STAFF PERMITS) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to allow Aberdeen City Council staff permits to be utilised within the new Marischal College multi-storey off-street car park only from 6pm to 10pm Monday to Friday.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	31 May 2012
DIRECTOR	Director of Corporate Governance
TITLE OF REPORT	Four Traffic Orders – Outcome of Main Statutory Advertisement Stage:-

- 1. The Aberdeen City Council (Citywide Traffic Management Measures) (No 1) Order 2012**
- 2. The Aberdeen City Council (Citywide Traffic Management Measures) (No 2) Order 2012**
- 3. The Aberdeen City Council (Advocates Road, Aberdeen)(Prohibition Of Waiting) Order 2012**
- 4. The Aberdeen City Council (Mid Stocket Lane, Aberdeen)(Traffic Management) Order 2012**

REPORT NUMBER: CG/12/054

1. PURPOSE OF REPORT

This report deals with four orders at the final statutory stage; that is to say, the main statutory advertisement period is now over in respect of each of these orders and this report presents the objections (where relevant) in each case. The public notices are attached, from which members will be able to see the exact content of the proposals.

The statutory consultation period for the orders has been undertaken and the objections attracted are detailed in the section 5 below and also appended to the report.

2. RECOMMENDATION(S)

It is recommended that the Committee:-

- (a) In relation to the Aberdeen City Council (Citywide Traffic Management) (No 1) Order 2012:-
 - (i) approve the proposal for the north side of Queen's Lane South and with regards the proposal for the south side, to approve the proposed relaxation to the order, as detailed in section 5.1.1 below and shown in the appendix to the report; and
 - (ii) request officers to monitor the impact of the introduction of the measure on the south side of Queen's Lane South and should the impact be detrimental to the access to garages, to request officers to commence the traffic regulation order procedure for the

- introduction of further waiting restrictions in front of the garages concerned (as shown in the appendix);
- (iii) overrule the objections in relation to the proposal at Queen's Road, and approve the proposed relaxation to the order, as detailed in section 5.1.2 below and shown in the appendix to the report; and
 - (iv) approve that the order be introduced as originally envisaged, with the exception of the relaxations proposed for Queen's Road and the south side of Queen's Lane South;
- (b) in relation to The Aberdeen City Council (Citywide Traffic Management Measures) (No 2) Order 2012:-
- (i) overrule the objection in relation to the proposal at Forest Avenue, and approve the proposal as originally envisaged for inclusion in the order;
 - (ii) overrule the objections in relation to the proposal at Newhills Avenue, and approve the proposed relaxation to the order, as detailed in section 5.2.2 below and shown in the appendix to the report; and
 - (iii) approve that the order be introduced as originally envisaged, with the exception of the relaxation proposed for Newhills Avenue;
- (c) agree to defer consideration of The Aberdeen City Council (Advocates Road, Aberdeen)(Prohibition Of Waiting) Order 2012 to the next meeting of the Committee to allow for further investigation to be undertaken; and
- (d) in relation to The Aberdeen City Council (Mid Stocket Lane, Aberdeen)(Traffic Management) Order 2012, to request officers to recommence the traffic regulation order process with the proposed police parking pay at a new location on Mid Stocket Lane which was not directly adjacent to an access point.

3. FINANCIAL IMPLICATIONS

The proposals contained in the traffic orders detailed at 5.1 and 5.2 will be fully funded by the 2012/2013 Cycling, Walking Safer Streets budget.

The proposals contained in the traffic orders detailed at 5.3 and 5.4 will be fully funded by individual developers.

4. OTHER IMPLICATIONS

There are no other implications worthy of being identified in the abstract here, although, again, both Section 5 and the appendix rehearse concerns raised by objectors.

5. BACKGROUND/MAIN ISSUES

This section has been sub-divided into sub-sections corresponding to the four orders under consideration.

5.1 The Aberdeen City Council (Citywide Traffic Management Measures) (No 1) Order 2012

5.1.1 Queen's Lane South

Two statutory objections from Dr Lynch, and Mr Craib, have been received in relation to the proposal to alter the operational time of the existing waiting restriction along the majority of the lane.

Both of the objectors raise concerns regarding the impact the increased operational hours will have on their ability to park at their homes. The objectors accept that there are marginal problems during the morning and evening rush hour period however do not believe that this problem is caused by the parking on Queen's Lane South and also do not believe that the problems are sufficient to warrant the change in operational hours. The objectors have detailed numerous other areas which they believe are contributing to the marginal problems experienced. In addition to concerns about available parking, concerns were also raised regarding access to garages and it was believed that the proposal would increase the number of vehicles causing obstruction.

In response roads officials have advised that it has been brought to their attention through a number of residents that there are problems in keeping the lane clear of parked vehicles in the vicinity of the Albyn School at certain times during the day. This indiscriminate parking impacts on normal vehicular access but also on access for larger vehicles such as refuse collection vehicles, delivery vehicles to the businesses along the lane and possibly also emergency vehicles. In light of this, it is deemed appropriate that the existing operational times of the waiting restrictions in this lane be altered from Monday to Friday 10am – 4pm to Monday to Friday 8am – 6pm over the majority of the lane on both sides, with short sections of the lane left at Monday to Friday, 10am – 4pm. Officers were aware of parking concerns and therefore have proposed the short sections to enable parking for a small number of residents.

In light of the objector's comments and following informal objector meetings, officers have reconsidered the proposals and now propose that the measure proposed for the north side of Queen's Lane South remain, however that the proposal for the south side be relaxed so that when travelling on the south side east bound from Forest Avenue the Monday to Friday 8am to 6pm waiting restriction be installed to appoint where the residential garages begin. The proposed relaxation is shown in the plan appended to the report. Officers have also suggested, in light of concerns regarding vehicles obstructing access to the garages, that the measures be monitored on installation and that should there be evidence of obstruction that they commence the traffic regulation order procedure to provide for waiting restrictions in front of the residential garages. A plan showing this second measure is also appended to the report.

On consideration of the objections and the rationale for the proposal it is recommended that the Committee agree to approve the proposal for the north side of Queen's Lane South and with regards the proposal for the south side, to approve the proposed relaxation to the order detailed above and shown in the appendix to the report. Following introduction of the scheme to request officers to monitor the scheme and should there be evidence of vehicles obstructing access to garages, to request officers to commence the traffic regulation order procedure for the introduction of further waiting restrictions in front of the garages concerned;

5.1.2 Queen's Road

Two statutory objections have been received from Mr Silver and Mr Oswald in relation to the proposed restriction the remainder of the southern carriageway with a Monday to Friday 8am - 6pm waiting restriction, to improve the traffic flows at peak times but still allow evening parking.

Both objections recognise that this is a busy stretch of road in the morning and evening peak, however question the justification of the proposals, and express their concern at the detrimental impact this will have on the availability of parking. Although both objectors question the requirement for any restriction, both would presser if restrictions were to implemented that these be relaxed so that they apply only during the morning and evening peak periods, i.e. 8am to 9.30am and 4pm to 6pm.

In response to the objections road officials have advised that there have been a number of complaints from road users regarding the vehicles parked on Queen's Road between Hazledene Road and the Hazlehead roundabout. When vehicles park on the south side of the road, they force all vehicles travelling westbound into the offside lane creating a bottleneck and this has had an adverse effect on the peak flows especially in the evenings. Currently a short section on the south side of the road is unrestricted except for a bus clearway and a small number of vehicles take advantage of this parking, however clearing this section of parked vehicles over the peak flow times would improve traffic flows. In light of this waiting restrictions at the location are deemed necessary.

In light of the objector's comments and following informal objector meetings, officers have reconsidered the proposals and it is now recommended that the original proposal be relaxed so that the remainder of the southern carriageway will have a Monday to Friday 8am to 9.30am and 4pm to 6pm waiting restriction.

On consideration of the objections and the rationale for the proposal it is recommended that the Committee approve the relaxation and agree that the order be implemented on this basis.

5.2 The Aberdeen City Council (Citywide Traffic Management Measures) (No 2) Order 2012

5.2.1 Forest Avenue

One statutory objection from Mrs Goodall, has been received in relation to the proposal to introduce 45 minute waiting restrictions on the south west side of Forest Avenue. Mrs Goodall raises concern that the proposal will exacerbate the existing traffic problems in the area rather than alleviate them.

In response to the objection roads officials have advised that requests have been received from individuals, but also through local Councillor Jennifer Stewart, to have some restriction on long-term parking at the south end of this street. This street is not within a parking zone and is just outside the relatively new Zone X and is therefore a desirable street for long-term parking in the west end of the city. There are a small number of businesses at this location requiring a higher turn-over of parking for their customers. Given this the 45 minute maximum waiting restriction is deemed appropriate at the south west side of Forest Avenue.

On consideration of the objections and the rationale for the proposal it is recommended that the Committee overrule the objection and approve the proposal for inclusion in the order.

5.2.2 Newhills Avenue

Seven statutory objections have been received from Ms Lamont, Mr Buchan, Mr Esson, Mr and Mrs Sim, Mr and Mrs Frew, Mr Davidson (all residents of Dykeside Way) and Ms Hepburn in relation to the proposal to introduce "At any time" waiting restrictions around the corner of Newhills Avenue to improve visibility there.

The objectors all raise similar concerns in which they question the rationale for the proposal and highlight that there have been no incidents of safety at the location. Instead they believe that the measure will aggravate the current parking situation experienced by residents and question where they should park if the measure is introduced. They express concern that the removal of parking from this location will increase speeds and create safety problems. Alternative proposals suggested by objectors to alleviate the parking concerns include:- (1) the introduction of a one-way restriction on the length of Newhills Avenue; (2) the grass and the outer pavement on Newhills Avenue be removed so that all residents, nurses, doctors etc can park off the road safely; and (3) the corner of the road be widened and the double yellow only come as far as number 1 Dykeside Way and not number 11; and (4) extension of the existing bus lay-by to provide residential parking, the lay-by originally was used solely for cars. Concerns regarding consultation with residents and consideration of the impact the proposal would have on those in the area were highlighted.

In response to the concerns raised roads officials have advised that it has been highlighted by a resident, through Councillor MacGregor, that there is an increasing number of vehicles being parked on the carriageway of Newhills Avenue, at the bend, and the occupants are choosing to then use the internal paths to walk to their properties. It would appear to be an issue created by residents rather than visitors. Although this is not a major road it is on the number 18 bus route and the vehicles parked at the corner restrict the free movement of two-way traffic and reduce the visibility to those vehicles. Therefore, officers deem it appropriate to introduce "At any time" waiting restrictions around this corner to improve visibility.

In light of the objector's comments and following informal objector meetings, officers have reconsidered the proposals and it is now recommended that the original proposal be relaxed so that certain lengths of the sections of "At any time" waiting restriction proposed be removed to provide parking for approximately 11 cars. The plan detailing the proposed relaxation is appended to the report. This relaxation eases the concerns of objectors, however it does not cure the objections as their view remains that the proposal for any waiting restrictions at the location is unjust, unwanted and unnecessary.

On consideration of the objections and the rationale for the proposal it is recommended that the Committee approve the relaxation for Newhills Avenue and agree that the order be implemented on this basis.

5.3 The Aberdeen City Council (Advocates Road, Aberdeen)(Prohibition Of Waiting) Order 2012

Two letters of objection have been received from Mr and Mrs Harwood and Ms Birchley have been received in relation to the proposal to formalise "At any time" waiting restrictions on Advocates Road which had been implemented by a third party without informing Aberdeen City Council and without going through the required legal process. At present, these lines therefore cannot be enforced by the City Wardens.

Both objections questions the actions of First Bus which had led to the proposal already being implemented without the required legal process having been undertaken, as well as the evidence that the proposed measure was required, and the impact this would have on residents who already experienced difficulty finding available parking in this location. Specifically, Mr and Mrs Harwood highlight the existing difficulties they experience with regards available parking at both Kings Crescent and Advocates Road, as well as their concerns at the further detrimental impact the proposal will have on their ability to park. This is a matter that they have raised on numerous occasions with Council officers and feel strongly that a formal assessment of the overall parking position in this location was required to identify solutions rather than the introduction of further restrictions.

Similarly, within her objection, Ms Birchley calls for the proposal to be postponed until a road traffic survey was undertaken and the impact on residents considered.

In response to the points raised by the objectors' roads officials have advised that following the development of the First Bus headquarters new "At any time" waiting restrictions have appeared on Advocates Road. Parking did previously occur on the footway and these restrictions will improve pedestrian safety in this area. Therefore, following discussions with First Bus it has been agreed to promote a Traffic Regulation Order to cover these lines to make them legitimate and so that they can ultimately be enforced by the Wardens.

Having held informal meetings with both objectors, a number of points require further investigation at this stage and therefore it is recommended that the proposal be deferred a cycle until these matters can be clarified. The objectors are content with this.

5.4 The Aberdeen City Council (Mid Stocket Lane, Aberdeen)(Traffic Management) Order 2012

One statutory objection from Dr Macdonald has been received in relation to the proposed police bay outside the police office on Midstocket Lane. Dr Macdonald objects to the proposed location of the police bay which is directly adjacent to a garage entrance. Dr Macdonald does welcome the introduction of a police bay on the lane, however feels that the proposed location is unsuitable and detrimental to one garage owner and has suggested that it be relocated to further up the street across from an area not requiring access. In particular he has concerns regarding the ability to access this garage if a vehicle was parked in the proposed bay and also fears that the location of the bay would encourage others to also park on the lane.

In response to Dr Macdonald's concerns, roads officials have advised that they will review the position of the police parking bay and would be content to re-advertise the bay at a new location on Mid Stocket Lane. This cures the objection.

On consideration of the objections and the rationale for the proposal it is recommended that the Committee approve the re-advertisement of the proposed police parking bay at a new location on Mid Stocket Lane which was not directly adjacent to an access point.

6. SERVICE AND IMPACT

Section 5 above – and also the public notices attached – will allow members to consider the possible impact on communities compared with the intended virtue of the original proposals.

7. BACKGROUND PAPERS

No background papers were used as a point of departure for writing this report (other than the objections themselves).

The statutory advertisements are published here for information, allowing members to see the import of each order as advertised.

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(CITYWIDE TRAFFIC MANAGEMENT MEASURES) (NO 1) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to establish a range of traffic management measures at various locations in the city, all as shown in the schedule below (where the nature of the measure in each case is also indicated).

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

SCHEDULE

Abbotshall Crescent, Redmoss Road, Redmoss Walk, Berryden Road, Binghill Road, Binghill Grove, Brunswick Place, Claymore Avenue, Kingsford Road, Sheddocksley Road, Harthill Place, Hardgate, Hermitage Avenue, High Street, Howe Moss Drive, McDonald Court, Morningfield Road, Osborne Place, Queen's Road, Riverside Drive, Huntly Street, Ivanhoe Road, Roslin Place – there will be prohibitions of waiting at any time on *certain lengths* of each of these roads

Queen's Road, Craigton Road, Queen's Lane South – there will be prohibitions of waiting Monday to Friday, 8am to 6pm, on *certain lengths* of these roads.

Exchange Street, Stirling Street – Additional 'pay and display' bays (Monday to Saturday, 8am to 6pm; Sunday, 1pm to 5pm) will be established and replace existing lengths of bays exclusively reserved for the purposes of goods vehicles loading. Specifically one individual bay currently reserved for loading on each street will be modified to 'pay and display'.

Fonthill Road – a length of 'residents only' parking bays (Monday to Friday, 9am to 5pm) will replace the now defunct 'Police' parking bays.

Ivanhoe Road – 'residents only' parking bays (Monday to Friday, 10am to 4pm) will replace an existing length of 'residents only and voucher' parking.

Sheddocksley Road, Maidencraig Place, Springhill Road – revocation of defunct School Keep Clear restrictions.

Hermitage Avenue – A length of 'pay and display' parking (Monday to Friday, 10am to 4pm) will replace an existing length of prohibition of waiting at any time.

Berryden Road – Property nos. 3 and 5 will be added to the list of properties entitled to residential exemption permits within the Aberdeen City Council (Rosemount)(On-street parking places and waiting restrictions)(Zone M)(Extension) Order 2011.

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(CITYWIDE TRAFFIC MANAGEMENT MEASURES) (NO 2) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to establish a range of traffic management measures at various locations in the city, all as shown in the schedule below (where the nature of the measure in each case is also indicated).

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

SCHEDULE

Ashgrove Road, Springfield Road, Springfield Road (Service Road), Braeside Terrace, Broomhill Road, Girdleness Road, Rockall Road, Leslie Road, Hilton Place, Kettlehills Lane, Kettlehills Road, Kettlehills Crescent, Kirkhill Road, Morningside Road, Morningside Lane, Newhills Avenue, Palmerston Road, Stell Road, Mastrick Drive, Ross Crescent, South Esplanade West, Union Grove Lane, Raik Road – there will be prohibitions of waiting at any time on *certain lengths* of each of these roads

Urquhart Place – revocation of a length of prohibition of waiting at any time.

South Esplanade East – revocation of prohibition of waiting, Monday to Saturday, 8.00am to 6.00pm.

Hillside Road, Peterculter – motor vehicles will be prohibited except for access (this measure will close the Bush / Hillside Road to through traffic).

Bridge Street – revocation of Taxi rank.

Cornhill Road – modification to 'Residents only' and 'Voucher and Residents Only' parking. Specifically the two lengths of controlled parking places between the junction with Westburn Drive and the first access to Grampian University Hospitals will remain the same length but be reversed in location.

Forest Avenue – a length of parking will be established with a maximum stay of 45 minutes and no return within 15 minutes, Monday to Saturday, 8am to 6pm.

Littlejohn Street, Queen Street – establishment of lengths of parking for exclusive use by motor vehicles operating within the 'Car Club' scheme.

St Devenick's Place, Deeview Road South, Loirsbank Road, Park Road, West Cults Road, Belvidere Road, Belvidere Lane, Den of Cults, Station Road (Cults), Ashfield Road, Primrosehill Road, Primrosehill Avenue, Primrosebank Avenue, Primrosebank Drive, Inchgarth Mews, Inchgarth Road, Pitfodels Station Road, Garthdee Road – the current prohibition of vehicles exceeding a maximum gross weight of 7.5 tonnes except for access will be modified to a prohibition of vehicles over 2.1m in width except for access.

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(ADVOCATES ROAD, ABERDEEN)(PROHIBITION OF WAITING) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to introduce prohibitions of waiting at any time on *certain lengths* of Advocates Road, Aberdeen. Exemptions will apply as usual to the picking up or setting down of passengers, loading or unloading, blue badge holders not causing an obstruction, funeral vehicles, and vehicles parked with the consent of the Council in direct association with authorised roadworks or building works.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL
(MID STOCKET LANE, ABERDEEN)(TRAFFIC MANAGEMENT) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to establish a certain length of parking bay on Mid Stocket Lane for exclusive use by Police vehicles

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 11 April to 2 May 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 11 April to 2 May 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

The Aberdeen City Council (Citywide Traffic Management Measures) (No 1) Order 2012 – Queen’s Lane Objections

From: <_____
To: <trafficmanagement@aberdeencity.gov.uk>
Date: 4/12/2012 2:38 pm
Subject: Queen,s lane South Proposed parking changes

Dr F.P Lynch
14,Gladstone Place
Queen,s Cross,
Aberdeen
AB10 6 XA
12/4/12

Dear Sir,

Queen,s Lane Sourh---Parking Changes

I am writing to object to the proposal to allow parking on a short section on the north side of the lane opposite my garage and run in between 8am and 10am. When there are cars parked here,owing to the narrowness of the lane it is impossible to leave or enter my garage as there is not enough room to turn. As this is one of two short areas available to park in the lane between 8am and 10am I suggest that it will become a short term car park for parents taking their children to St.. Joseph,s School in St.Swithin Str.

Since parking regulations were first introduced .there are new occupants in nos12,16,18 20,and22 Gladstone Place .I have spoken to them all and also to Mrs. Duguid at no 7 Queen,s Lane Sth., who has her own run in,and none of them require to park in the lane between 8am and 10 am.

It is my custom to park my car in my garage overnight but I fear that, if the present proposals procede, I will be forced to park overnight in Gladstone Place.

I hope you will be able to accede to my request to restrict parking in this section between 8am and 6pm Mon to Frid. which applies to most of the lane,

Your,s Sincerely

Frederick P. Lynch

From: <_____
To: <trafficmanagement@aberdeencity.gov.uk>
Date: 5/1/2012 12:11 pm
Subject: Traffic Management

To the councillors trying to propose further parking restrictions at Queens Lane South running parallel with Gladstone Place.

Having read through the proposal which suggests to change the parking restriction times from 10am- 4pm to 8am-6pm on certain lengths of Queens Lane South, I strongly object to this proposal, why should the residents have to put up with inconveniences because the council has not thought properly about the parking implications when allowing huge building extensions to be given the go ahead. Are they short sighted or just don't care, this is a residential area and some seem to forget this.

I would also point out that the parking offenders come from the school parents dropping off and picking up their children, they don't care where they stop. I've witnessed there parking procedures, or what they would describe as parking, they park on double yellow lines, zigzag lines, on street corners, often double park and of course the single yellow line whether they are allowed or not. Surely the council cannot honestly believe that making the parking restrictions from 8am to 6pm is going to resolve this problem, I can assure you it won't. Perhaps the committee should visit the site at school opening and closing times to witness this atrocious parking behaviour and come back between 9.30-11.30am, 1.30-2.30pm and 4.30-6pm you would certainly see the difference - no parking problems. The only way to stop this parking dilemma is to have the area patrolled by traffic wardens or police at the pick-up and drop off times and penalise the offenders, they will soon get the message when it is money coming out of their pocket, putting them at an inconvenience for a change and the revenue from the parking tickets would be more than enough to cover the officers man hours spent at this area and times.

I look forward to your response

Graeme Craib

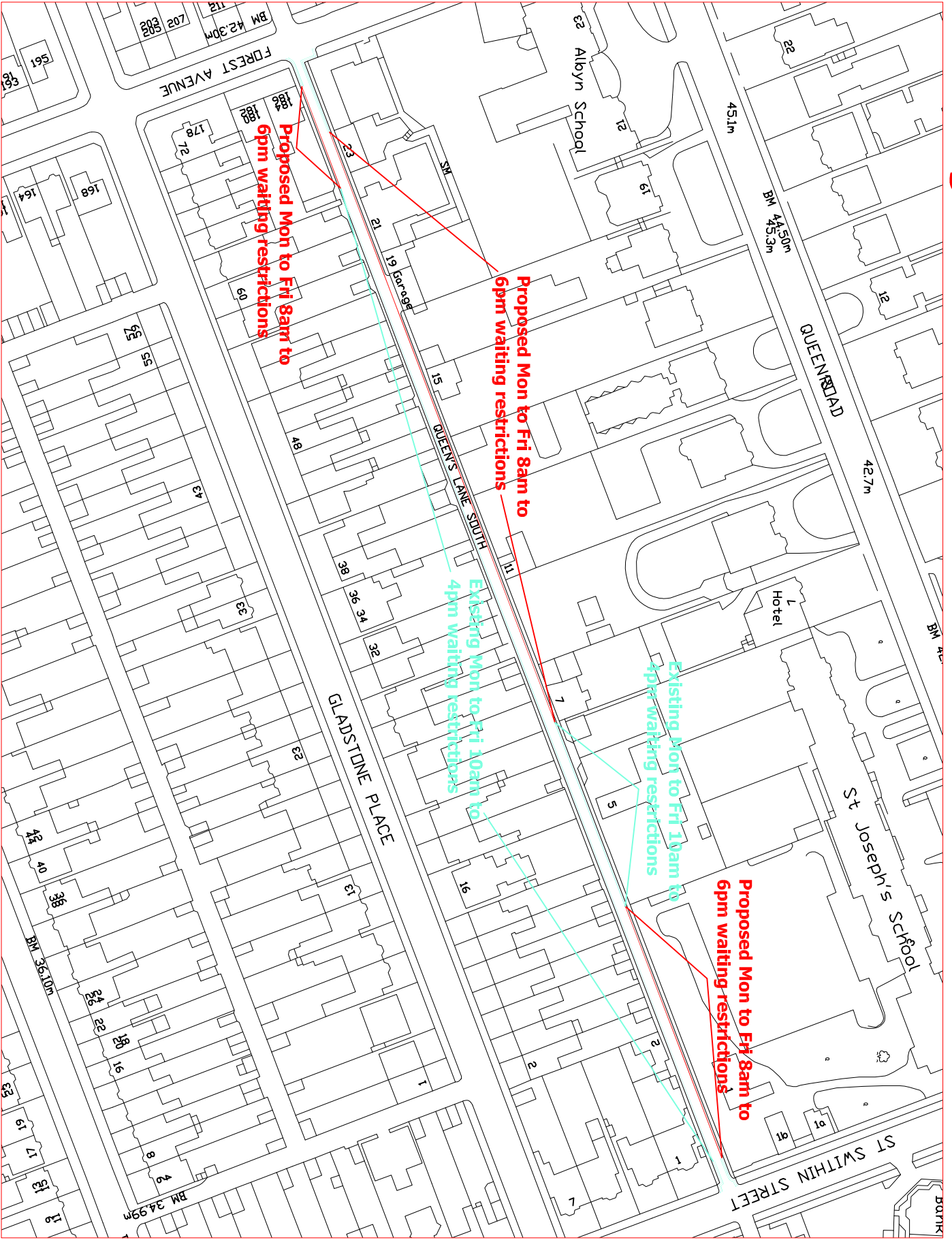
24 Gladstone Place

Queens Cross

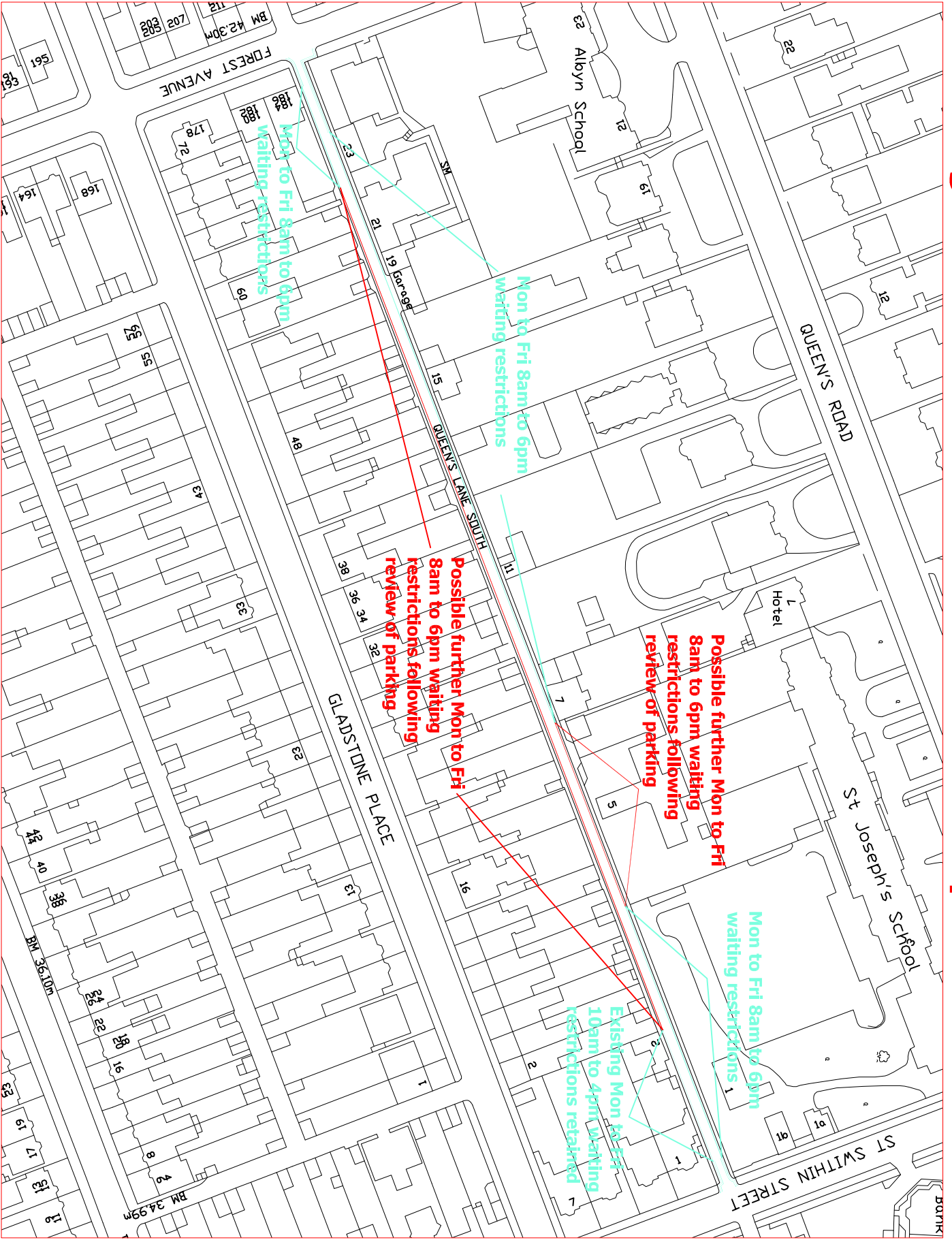
ABERDEEN

AB10 6XA

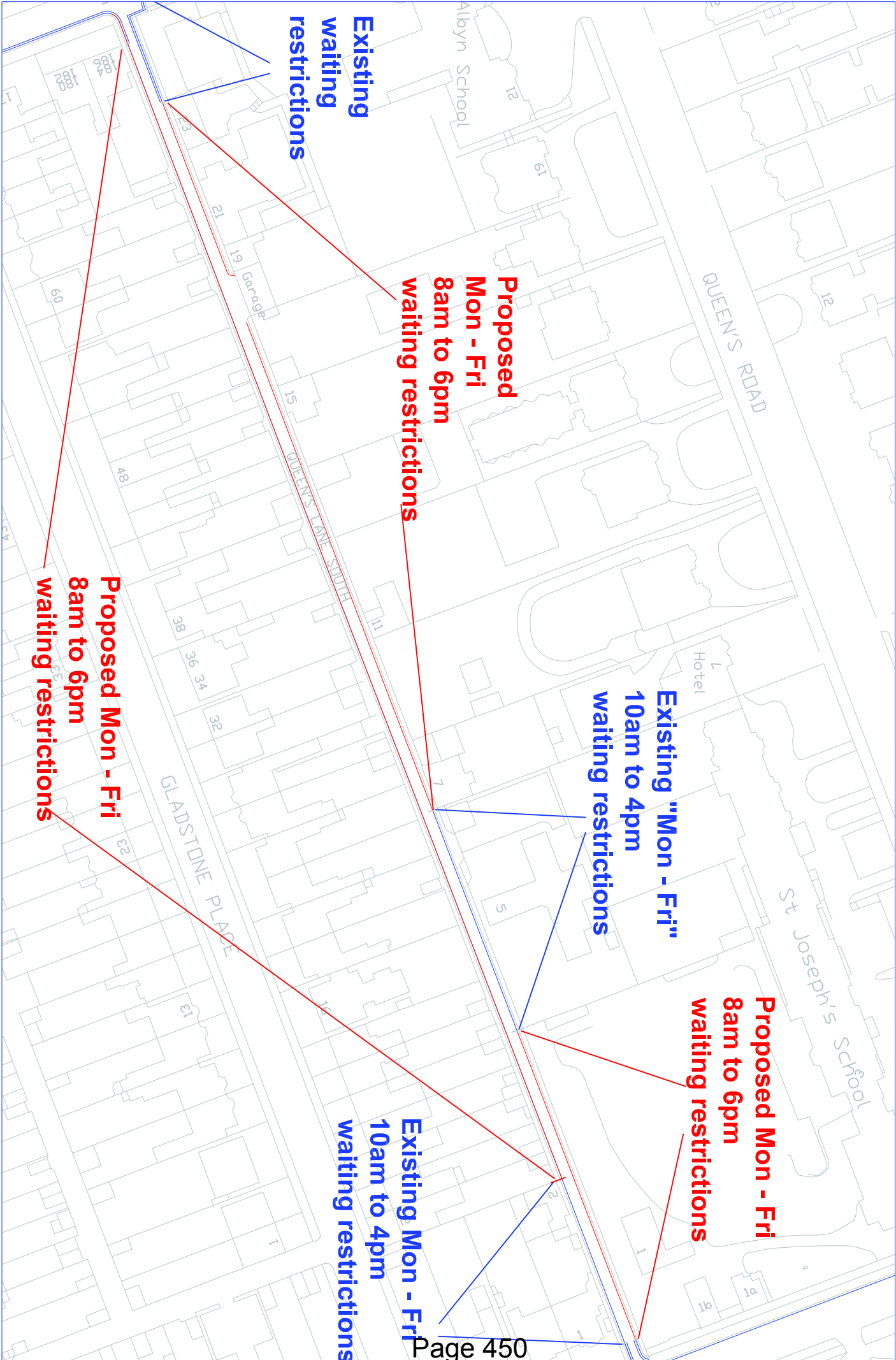
Queen's Lane South - Proposed initial phase of modifications following consultation



Queen's Lane South - Possible future modifications following review after installation of initial phase



Queen's Lane South - Proposed alterations to timed waiting restrictions



The Aberdeen City Council (Citywide Traffic Management Measures) (No 1) Order 2012 – Queen’s Road Objections

From: "Alan Oswald" <_____>
To: <trafficmanagement@aberdeencity.gov.uk>
Date: 5/2/2012 4:36 pm
Subject: The Aberdeen City Council (Citywide Traffic Management Measure) (No 1) order 2012

I have been on holiday and have returned to see the notice re a revision to the yellow lines on the South side of Queen's Road. I understand that this has been suggested by a member(s) of the public who perhaps are held up and are not aware of the whole picture and I should like to make some comments with the benefit of watching the traffic flow on a daily basis.

Queen's Road is a busy road and I am aware of some marginal problems during the morning and evening rush hour which I believe are not sufficient to warrant the imposition of whole day restrictions. Furthermore the problems that do exist are NOT due to parking on this stretch of road but more to the reasons I give below:

The build up of traffic in the stretch of Queen's Road between Hazledene Road and the King's Gate roundabout is due to:

- . a traffic build up on the WEST side of the roundabout which is caused by the four sets of traffic lights - 2 Pedestrian crossings further west along Queen's Road and the traffic lights at Provost Graham Avenue and Groats Road. This prevents traffic approaching the roundabout from the east from proceeding.

- . the influx of traffic from King's Gate which is turning right towards the west onto Queen's Road.

- . this traffic frequently blocks the roundabout preventing the traffic that is proceeding west on Queen's Road from entering the roundabout either to turn right into King's Gate or to continue westwards.

I am a firm believer that parking restrictions should be introduced where a benefit will accrue to all road users. However where parking in this stretch of road is NOT the cause of the congestion I feel that it would be unfortunate to introduce such a restriction.

I would welcome the opportunity to discuss this matter with you further, if you feel this that would be appropriate.

Yours faithfully

R Alan Oswald

280 Queen's Road

Aberdeen AB15 8DR

From: Alan Silver <_____>
To: <dritchie@aberdeencity.gov.uk>
Date: 5/1/2012 9:41 pm
Subject: Waiting Restrictions

I wish to object to the proposed 8-6 waiting restrictions on the south side of Queens Road between Hazledeane road and Kings Gate

I have been in to Spring Garden to study the proposed additional waiting restrictions on Queens Road

A new section of 8-6 is proposed between Hazledene Road and Hazlehead Avenue.

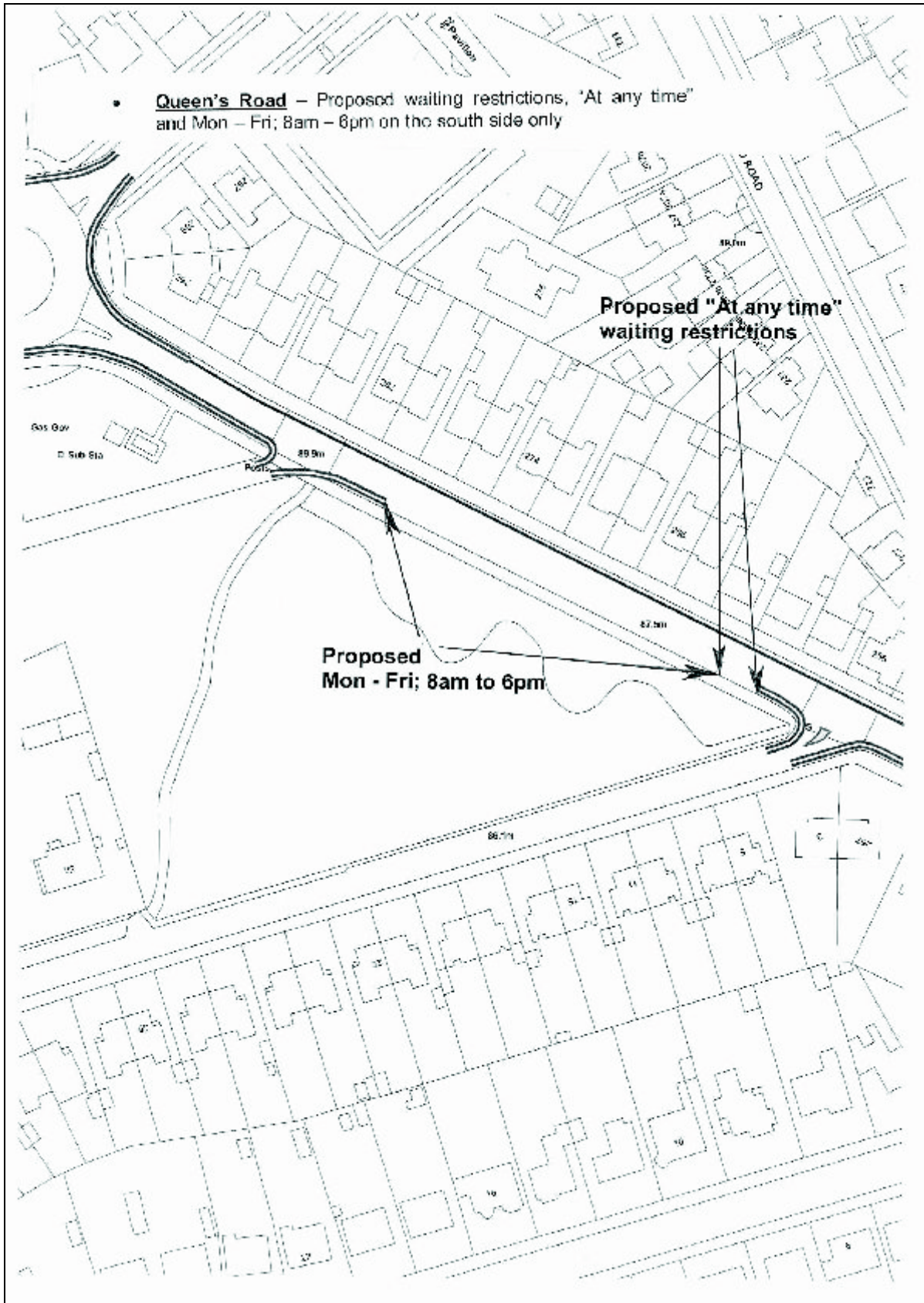
I have been advised that it is to protect the cycle lane but in this area cycling is permitted on the footway.

Queeus are also developing westbound in the evening peak.

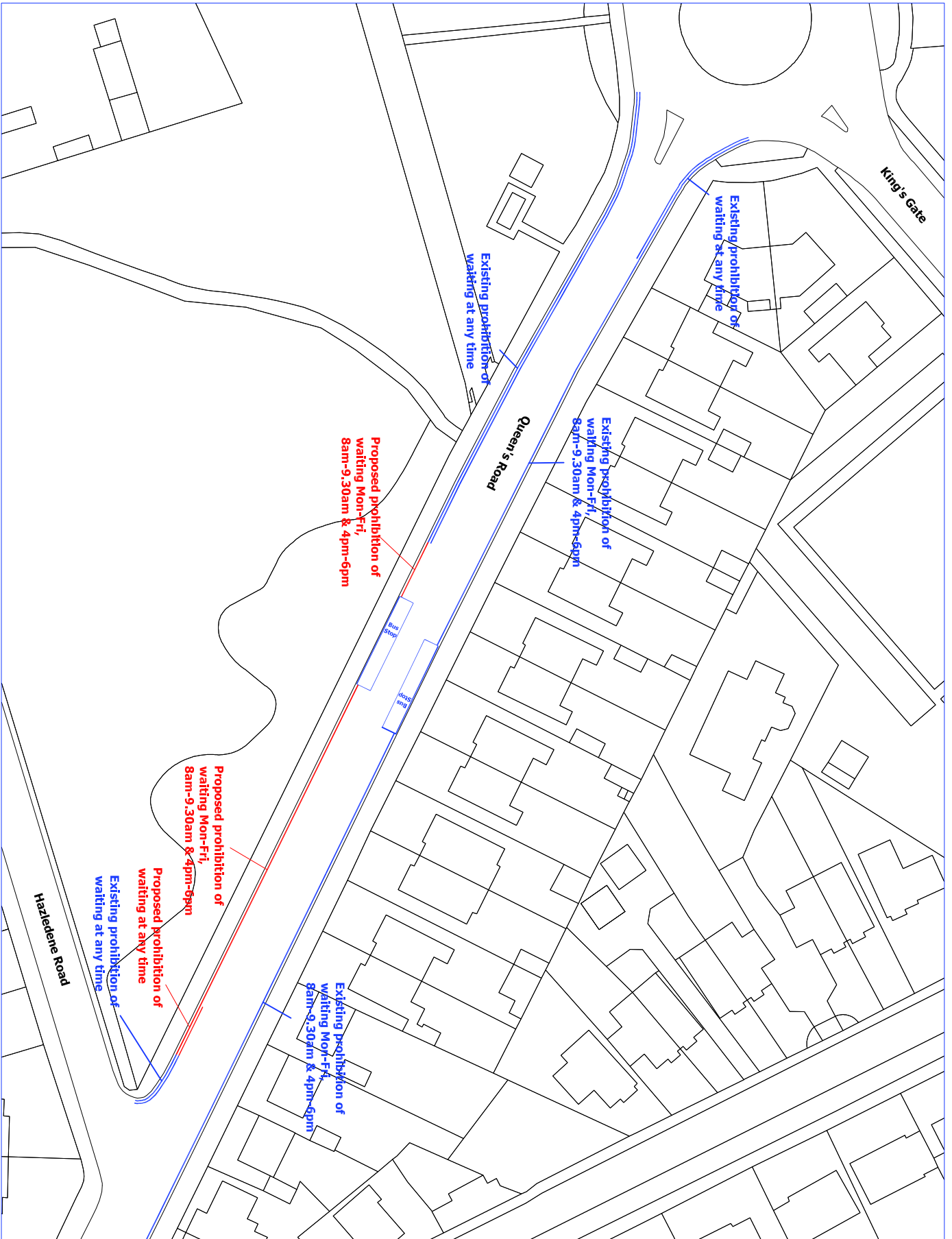
Would it not be appropriate to make this a 8-10am and 4-6pm restriction rather than all day.

As parking is to be removed in this area could the unused section of the old tram track not be made available for off street parking

I would ask that this be considered as an objection to the proposed order



Queen's Road - Proposed modification to original waiting restrictions proposal



The Aberdeen City Council (Citywide Traffic Management Measures) (No 2) Order 2012 – Forest Avenue Objection

>>> Lesley Goodall < > 4/18/2012 9:48 pm >>>

Dear Sir/Madam

I wish to object to plans to implement 45 minute restricted parking directly outside my door. I do not think this will solve any traffic problems in this area and know it will cause great inconvenience for residents. I do not believe this area has a problem with commuters leaving cars, and the only benefit of this restriction would be for business premises.

Mrs Lesley Goodall
1 Forest Ave
Aberdeen
AB15 4TU

Yours faithfully
Lesley Goodall

The Aberdeen City Council (Citywide Traffic Management Measures) (No 2) Order 2012 – Newhills Avenue Objections

From: S FREW <_____>
To: "TrafficManagement@aberdeencity.gov.uk"
<TrafficManagement@aberdeencity....>
Date: 4/19/2012 7:26 pm
Subject: The Aberdeen City Council (Citywide Traffic Management Measures) (No 2) Order 2012 - Newhills Avenue, Bucksburn

To whom it may concern.

I wish to register my objection to the above order to install double yellow lines on part of Newhills Avenue, Bucksburn, which is where myself and the other residents of Dykeside Way (odd numbers) park. We have no other places to park near our houses, we do not have the luxury of side street parking, like most of the residents in the scheme have, and wish to hear your views on how to overcome this.

We have, in the past, asked the Council to cut away some of the spare ground outside our houses and make way for parking bays, or even just extend the layby where the bus stop is, right along the length of the road. This has always been dismissed.

Forty odd years ago, when these houses were built, the layby (bus stop) was fully used for parking as the buses at that time did not come round the scheme. Years later, when the buses came round, it was made into a bus stop and so the residents of Dykeside Way had no option but to park on the street.

I have been informed that the section of road is now a road safety issue, however I fail to see why as it is a 20 mile hour zone and only on a sloping bend, not a corner. Surely if drivers drove correctly there would be no safety issue.

We then come to the problem of selling our houses now - who would want to buy a house with double yellow lines in the street? I know for a fact that if there had been double yellow lines there when I bought my house 18 years ago, I would not have bought it. This will devalue our houses for sure.

I have spoken to Councillor Neil McGregor regarding this issue and I am hoping he will be speaking to yourselves re any alternatives. I have also been in touch with Bucksburn Community Council.

I look forward to hearing from you as soon as possible.

Regards
Mr and Mrs Frew
9 Dykeside Way
Bucksburn
Aberdeen

From: Irene Sim <_____>
To: <trafficmanagement@aberdeencity.gov.uk>
Date: 4/18/2012 12:20 pm
Subject: City Wide Traffic Management Measures (no2) Order 2012

I wish to register an objection to the above order which I believe is to install yellow lines on part of Newhills Avenue. I am a resident of Dykeside Way, and a car owner and if we are unable to park outside our house, I wonder where we are supposed to park our car. At one time I rented a council garage, in the next street Cairnhillock, but 90% of the time I could not get access to it due to cars being parked in front of the door, so gave up the lease. We have a lay-by at the rear of our houses where we were able to park, but 80% of it has been marked off for 'busses' and marked as a 'terminus'. Many years ago it was a terminus, but it is no longer, and I'm sure if you check the 17 bus time-table or talk to First Bus they will confirm that the terminus for 17 bus is in fact at Hopetoun Grange, where there is no 'markings' for the bus to sit, it just sits in the street. The simplest solution would be to allow the residents of Dykeside Way and Watchman Brae use of the lay-by, taking all the cars off the road. This objection is on behalf of Irene & Gordon Sim, 13, Dykeside Way and James Masson, 15 Dykeside Way, Bucksburn Aberdeen AB21 9WE

Hello Allison,

Thank-you for meeting us on Tuesday 8th to discuss our concerns re the proposed restrictions at Newhill Avenue and the effect these restrictions will have on the car parking facilities for residents of Dykeside Way and the surrounding area.

The modified suggestions were very welcome and do go some way to ease the situation, although I have doubts that the area retained for 6 cars will in fact accommodate 6 cars, looks ok on paper, but in practice, I'm not so sure! I would put forward to the Committee that I feel very strongly that in these hard economical times the Roads Department of ACC has responded to one complaint regarding the 'parking' to the detriment of the residents of Dykeside Way, without any evidence to substantiate the complaint, (We moved into Dykeside Way in 1968 and cannot recall any accidents) I accept that if the restrictions are going to be put in place, we will have to learn to live with the modifications, not an ideal situation but an improvement on the original plans. However I would like my comments re the 'complaint' put forward to the committee on 31st May. Regards Irene & Gordon Sim, 13 Dykeside Way Mr James Masson 15 Dykeside Way, is of the view it eases, not cures, his concerns.

From: A BUCHAN <_____>
To: "TrafficManagement@aberdeencity.gov.uk"
<TrafficManagement@aberdeencity....
Date: 4/20/2012 11:55 am
Subject: newhills avenue

I wish to register my objection to the order(Citywide Traffic Management Measures) (No2) order 2012- Newhills Avenue.In doing so this leaves the residents of Dykeside Way with no alternatives for car parking . As an original tenant all cars were parked in the existing lay-by at that time, half of that is now taken up by the bus which was designated a terminus without any consultation with the residents . As the terminus has now been moved this lay-by could be better utilised.As for any safety aspect , in the fory three years I have been here there have been no accidents. Another aspect to consider is the grass area between the two footpaths which at the moment is used as a dog's toilet . Out of the nine residents of Dykeside Way five are pensioners of whom , two have recently undergone major heart surgery and one requires a home help and meals on wheels assistance. Given the above aspects above I would hope that the proposal is given more thought.

This objecton is raised on behalf of.
Alex Buchan
5 Dykeside Way
William
Esson 3 Dykeside Way
Jack Davidson 7 Dykeside Way

From: M LAMONT <_____>
To: "trafficmanagement@aberdeencity.gov.uk"
<trafficmanagement@aberdeencity....
Date: 4/20/2012 11:33 am
Subject: At anytime retrictions at NewhillsAvenue

Dear Sir/Madam

I wish to object to the anytime retriction at Newhills Avenue.

I purchased my house 14 years ago at 1 Dykeside Way and have never seen any incident at the bend on the road.

The only incidents of saftey have been when the bus has mounted the payments to allow another bus to pass, putting yellow lines would not resolve this.

I feel the decision to put double yellow lines is only going to encourage the buses and fast drivers to speed round that corner whereas just now they do have to slow down I cannot see how slowing down at a corner is regarded as a risk to safety.

As Dykeside Way is actually a path and therefore we have to park at Newhills Avenue if this is not allowed I cannot see where we are supposed to park our cars and thus causing health and safety issues for all resident in Dykeside Way.

My husband rented a garage for a period of time but unfortunately the access to the garage was not always accessible and found it more hassle than it was worth. The parking problems are already an issue and removing this street parking spaces would only add to the issue.

I am in favour of improving the roads and I know this all sounds very negative but I cannot see where this would leave us as a family of four to park our car safely and walk safely to our house, if I had viewed my property 14 years ago and there was no parking in sight I would not have purchased this property and cannot see that anyone wishing to buy any property with no solution to a parking space.

I would like to suggest that Newhills Avenue could be one way as then there would be no issue with passing places.

I know this next suggestion has been made several times over the last 45 years and there is a money issue but I do feel it is still the best option that the grass and the outer pavement be removed so that all residents, nurses, doctors etc can park off the road safely.

The other suggestion I have is that because the double yellows still does not allow busses that meet on the corner to pass safely that the corner of the road is widened and the double yellow only come as far as my property no.1

Dykeside Way and not no.11 .

Another suggestion is that the fact there is a layby but which is used by the buses that we should get that extended to allow us residents to park.

I feel there has been very little consideration to where carers, residents etc would be able to park and thus effecting a number of people not just on Dykeside Way but neighbouring streets aswell.

I do hope you take my suggestions into consideration and look forward to hearing from you.

Kind Regards

Suzanne Lamont
01224 761983

From: Karen Hepburn <_____>
To: <trafficmanagement@aberdeencity.gov.uk>
Date: 4/26/2012 9:38 pm
Subject: parking

to whom ever this may concern:

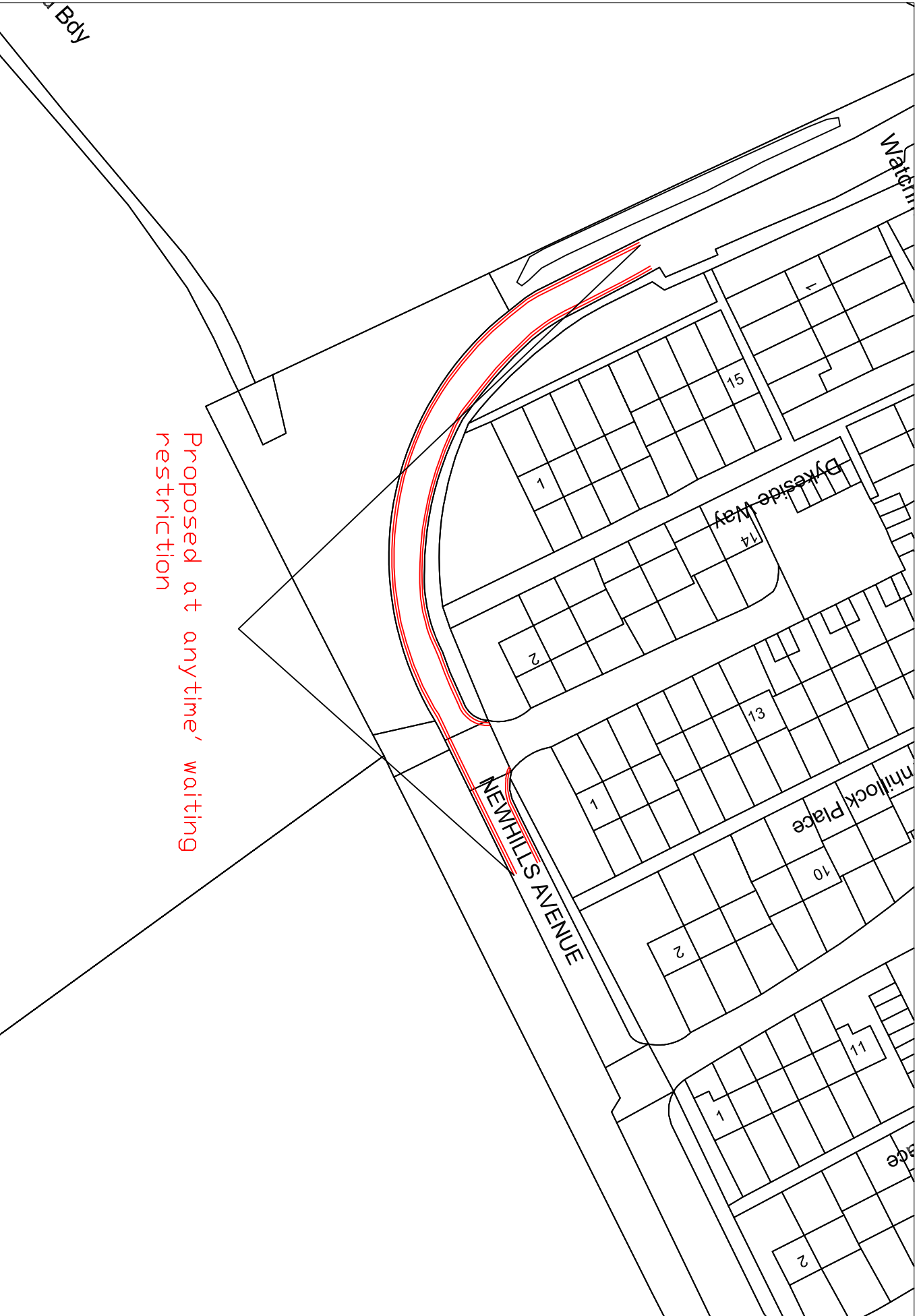
I am sending this e-mail as I am absolutely furious, and bewildered, about double yellow lines for newhills avenue - namely outside my dads front door, Dykeside Way, Bucksburn. My dad has stayed there for nearly 45 yrs and this is now just being decided. My daughter takes my dad shopping and appointments as he is not able to get there himself as he had triple heart bypass surgery just 7 weeks ago and cannot walk a great distance. How on earth is my dad supposed to get anywhere, if you are putting double yellow lines outside his door? You are basically putting my dad's health at risk.

Please also tell me why there is no double yellow lines getting put in Brimmondside or Kepplehills Drive? These are both areas that buses travel and are very heavy with traffic. Why is it that Dykeside Way is being targeted? There has been no trouble with parking there for as long as I can remember. I have grown up on above street and never in my wildest dreams could I have thought that this would happen. I am totally against it and am furious that my dads health is going to be put at risk once again.

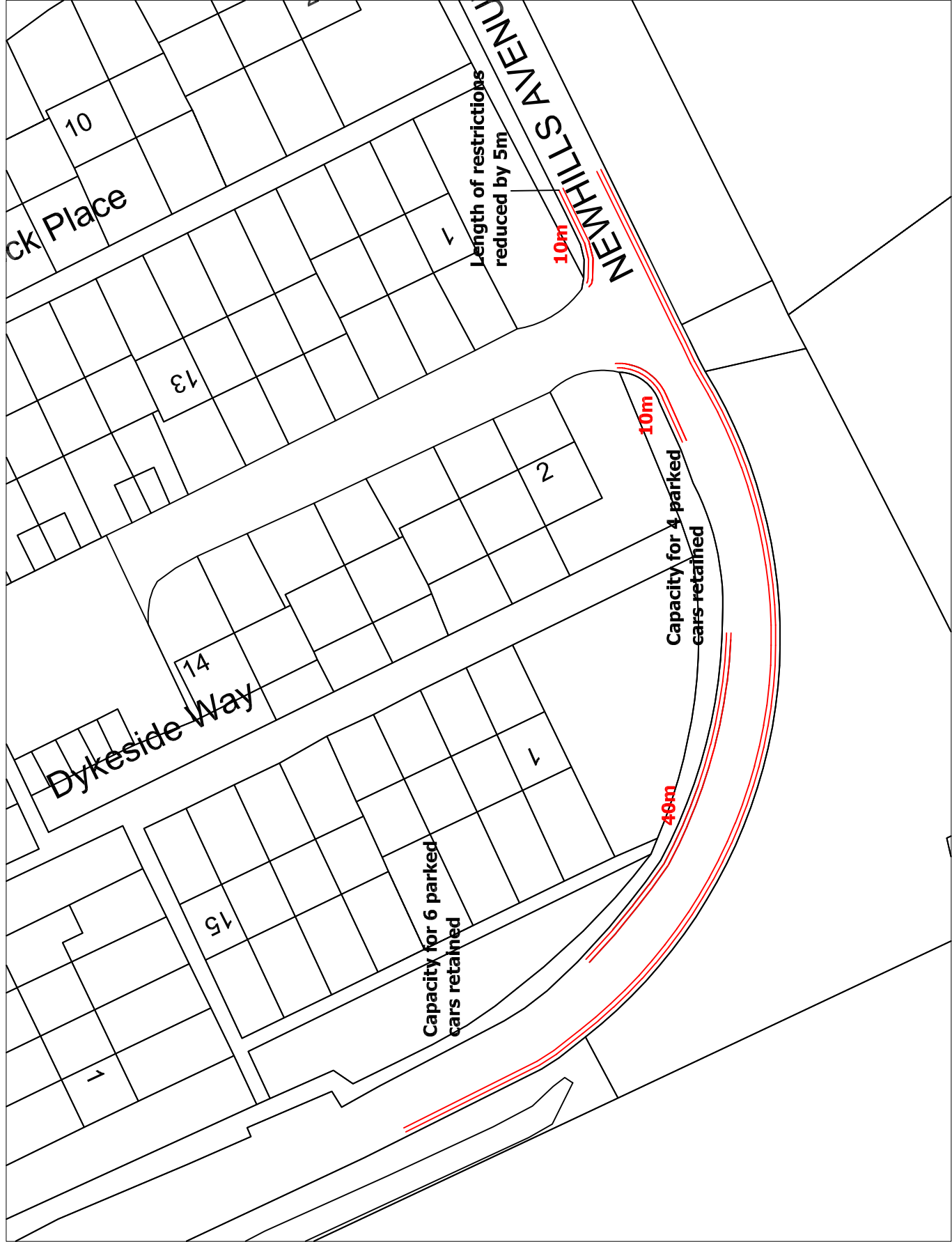
I hope that someone will take this into consideration when they decide, as my dads health is more important than double yellow lines. I look forward to hearing from you in the very near future.

Karen
Hepburn
Daughter of William Esson
Resident at 3 Dykeside Way
Bucksburn
ABERDEEN
AB21 9WE

Newhills Avenue - Proposed 'At any time' waiting restrictions



Newhills Avenue - Modifications to proposed lengths of prohibition of waiting at any time.



**The Aberdeen City Council (Advocates Road, Aberdeen)(Prohibition Of
Waiting) Order 2012 Objections**

By EMail

24 Spital
Aberdeen
AB24 3HS

30 April 2012

Traffic Management
Aberdeen City Council

Dear Sir/Madam

Advocates Road: Proposed “At any Time” waiting restrictions.

I wish to object to the retrospective application by First Bus to allow the double yellow lines with no waiting at any time restrictions they have painted on a public highway to be approved at this time.

When the application for the redevelopment of the First Bus headquarters site in 2007 was passed it was not felt by ACC Road department that it was necessary to apply any waiting time restrictions to this section of Advocates Road. The decision to paint yellow lines was taken unilaterally taken by First Bus with no consultations with Aberdeen City Council, the local community council or residents. No proper parking/public safety survey has been carried out.

A way forward would be to commission a roads/parking survey being done of the area before this retrospective application is passed. The impact the resident's only parking restrictions in the area near the university has had a significant impact on the availability of parking for locals in this area and it needs addressing. The survey could possibly show it may be best for the Council to consider bringing residents only parking restriction into the area, a move I would support.

It is not fully explained in the report to the Environment, Planning and Infrastructure committee of 31st January as to why the officers recommend the currently illegal restrictions stay. Advocates Road is a 200m dead end street currently allowing parking on both sides. At the end of the road is the entrance to the First Bus employee car-park. As it currently exists, with no waiting restrictions, it does not pose a threat to pedestrian safety.

I would urge the committee to put this application on hold and commission a parking survey/public safety of the area with the aim of considering a residents only area and take the issue of the no waiting restriction retrospective application on Advocates Road into this survey.

Yours sincerely

Jacinta Birchley

Jane MacEachran
Head of Legal & Democratic Services
Aberdeen City Council
Town House
ABERDEEN

Ref: GC.LT 45114/0001
Your Ref:
Date: 30 April, 2012

Dear Ms. MacEachran

Mr. Richard and Mrs. Teresa Harwood
Objection to Proposal to impose Traffic Restrictions on Advocates Road, Aberdeen

We have been instructed to act on behalf of our clients, Mr. and Mrs. Harwood of 37 Kings Crescent, Aberdeen and object to the proposal to impose traffic restrictions on part of Advocates Road as detailed in the Report dated 31st January, 2012 addressed to the Environment Planning and Infrastructure Committee of the Council and which Report was prepared under Reference EPI/12/009.

I refer to Appendix 1 attached which shows our clients' house at 37 Kings Crescent hatched in black and which lies opposite the First Bus Depot which extends to the edge of Kings Crescent.

Our clients have significant problems with parking already and if this Report is implemented this will make life very difficult for them in attempting to park close to their home which they have owned for some 30 years. Although there are eight parking spaces on Kings Crescent close to our clients' house (assuming everyone parks with consideration) these are used by the owners and occupiers of numerous houses on Kings Crescent and also by First Bus drivers despite them having their own car park off Advocates Road. As a result our clients are far from guaranteed parking on Kings Crescent at any time. The position is made worse as there are parking restrictions imposed on this area of road when there are football matches and events at Pittodrie Stadium which means that should our clients continue to park during a football match they will inevitably receive a parking ticket.

Moving on to Advocates Road itself, when our clients moved into their home they were able to park along the length of Advocates Road. Appendix 1 attached shows the part of Advocates Road which our clients understand First Group have now acquired. The boundary line is clearly shown on said plan. Although there are parking spaces in the remainder of Advocates Road, First Group for some unknown reason have apparently been responsible for adding yellow lines, purporting to prevent parking right up to the edge of Kings Crescent. This is identified in the Council Report referred to above. Although the Report indicates that First Group were

Ledingham Chalmers LLP, Solicitors

Johnstone House 52-54 Rose Street Aberdeen AB10 1HA (Registered Office) DX: AB15 Aberdeen LP-39 Aberdeen-1

Tel: 01224 408408 Direct Dial: 01224 408570 Fax: 01224 408409

Email: graham.cooper@ledinghamchalmers.com www.ledinghamchalmers.com

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A list of members is available for inspection at the above address

responsible for painting these yellow lines it does not reveal either why the yellow lines were painted nor what action the Council have taken to deal with the matter. It simply says that these parking restrictions are non-enforceable which is quite clearly the position. In addition First Group have constructed a raised pavement on the north side of Advocates Road presumably also without permission. This means that there is a pavement on both sides of Advocates Road which does not appear to be consistent with First Group's stated aim of doing this for safety reasons. There is an existing footpavement along the south side of Advocates Road and my clients believe that this is sufficient for the pedestrian users. In addition the new pavement turns round a 90 degree angle at the end of Advocates Road and when pedestrians reach the mechanised barrier/gate leading to the car park they require to cross the road at that point in order to take access into the car park. Pedestrian access to the First Group car park can only be taken from the righthand side of the barrier/gate leading thereto. My clients therefore take the view therefore that for safety reasons the only footpavement that requires to be used is the original one running along the south boundary of Advocates Road. Appendix 3 incorporates a photo showing the pavement running along the south side of Advocates Road before turning at a right angle towards the First Group car park. In addition the plan attached to Appendix 4 shows the sole pedestrian entrance to the car park and of course the First Group complex lying to the east of Advocates Road and consequentially why there is no requirement for the other pavement.

The above position can clearly be seen with reference to Appendix 2 attached. My clients believe that for reasons of safety the only pavement which requires to be used is the one lying to the south of Advocates Road and by continuing along this path into the First Group compound there is no need to cross any road thereby making this a safe pedestrian route. The vehicular turn off shown at the southeast boundary of Advocates Road on Appendix 2 does not exist and is therefore misleading.

The enclosures combined demonstrate that only the southernmost footpavement is required as pedestrians using the north footpavement require to cross the access road when they arrive at the First Group barrier – as explained this does not appear to offer pedestrians a safe solution.

Moving on to these proposals themselves if they are approved when the whole of the north side of Advocates Road will be unavailable for parking and there will only be four public spaces left on the south side of Advocates Road. One of these spaces has already been reserved for a disabled resident which means that in effect only three spaces in Advocates Road will be available for public parking.

Our clients have explained to me that this is grossly inadequate bearing in mind the parking situation at Kings Crescent.

I am aware that our clients have raised this matter on various occasions both with the Council officials and elected members and they feel that they have not made any progress. They have therefore asked me to write to you recording their strong objections to this proposal which does not appear to pay any attention to the needs of residents in the area. If this proposal is to be implemented our clients will have severe problems in being able to park close to their home which is clearly an unacceptable situation.

As indicated above the position is accentuated as some First Bus drivers tend to park both in the spaces in Kings Crescent and also within the public parking spaces in Advocates Road rather than use the car park provided by First Group leading off Advocates Road. Our clients advise that there are always spare spaces in that car park and, accordingly, parking by First Bus drivers on Kings Crescent and Advocates Road cannot be justified. Is there any way of enforcing a proposition that First Bus drivers require to park within the staff car park?

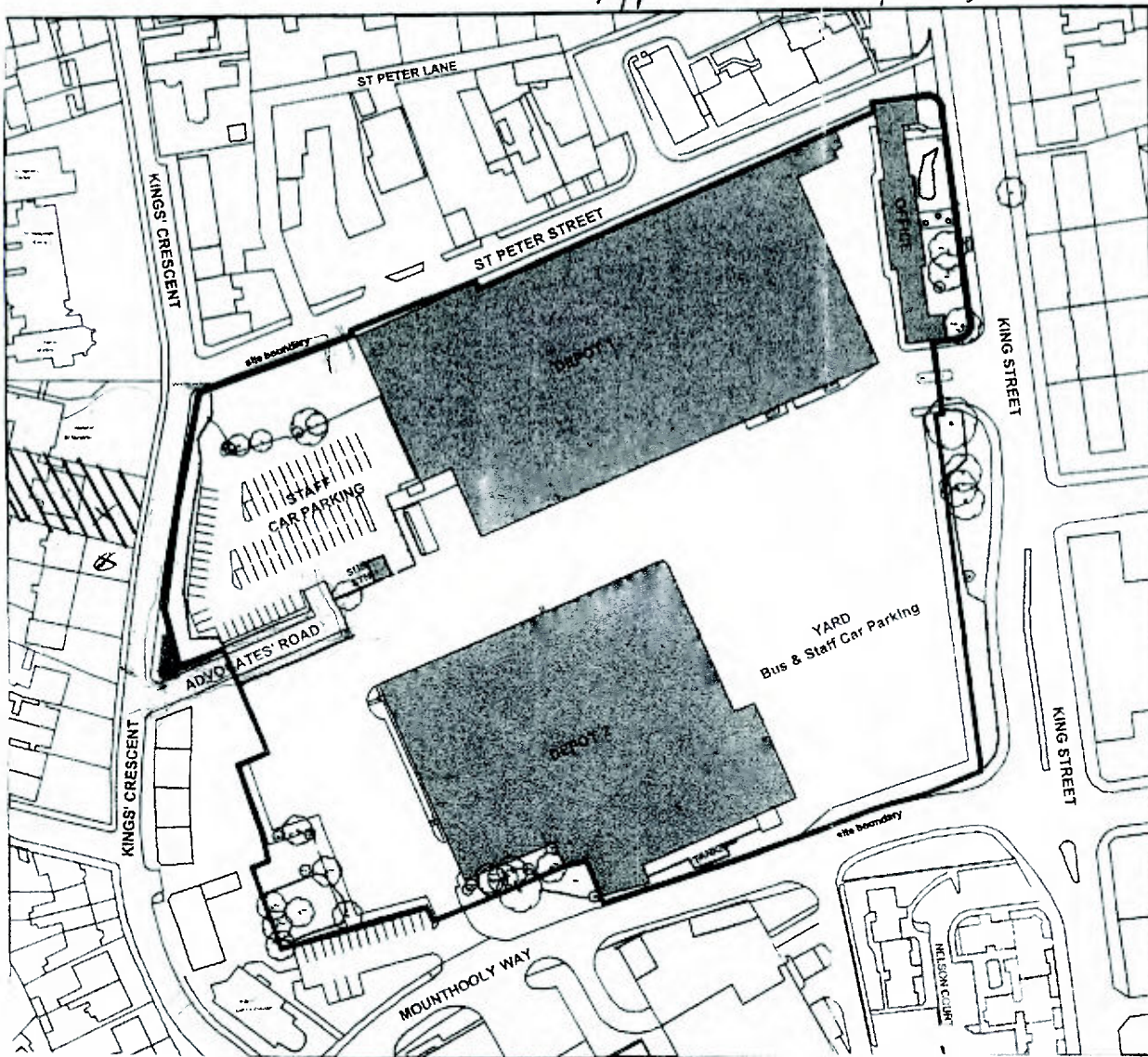
In summary and regardless of the terms of the foregoing paragraph our clients feel that their needs and requirements have been to a significant extent ignored by the Council especially over recent years and short of the application to Committee being refused they would favour that a decision on this proposal is deferred pending a full and thorough investigation into all aspects of parking in Advocates Road and Kings Crescent being carried out with the views of residents being sought as part of that exercise.

In the meantime please acknowledge receipt confirming that this letter has been received timeously and also confirming that it will be considered along with the Application. Should you require clarification on any point or wish to raise any query please get in touch with me.

Yours sincerely

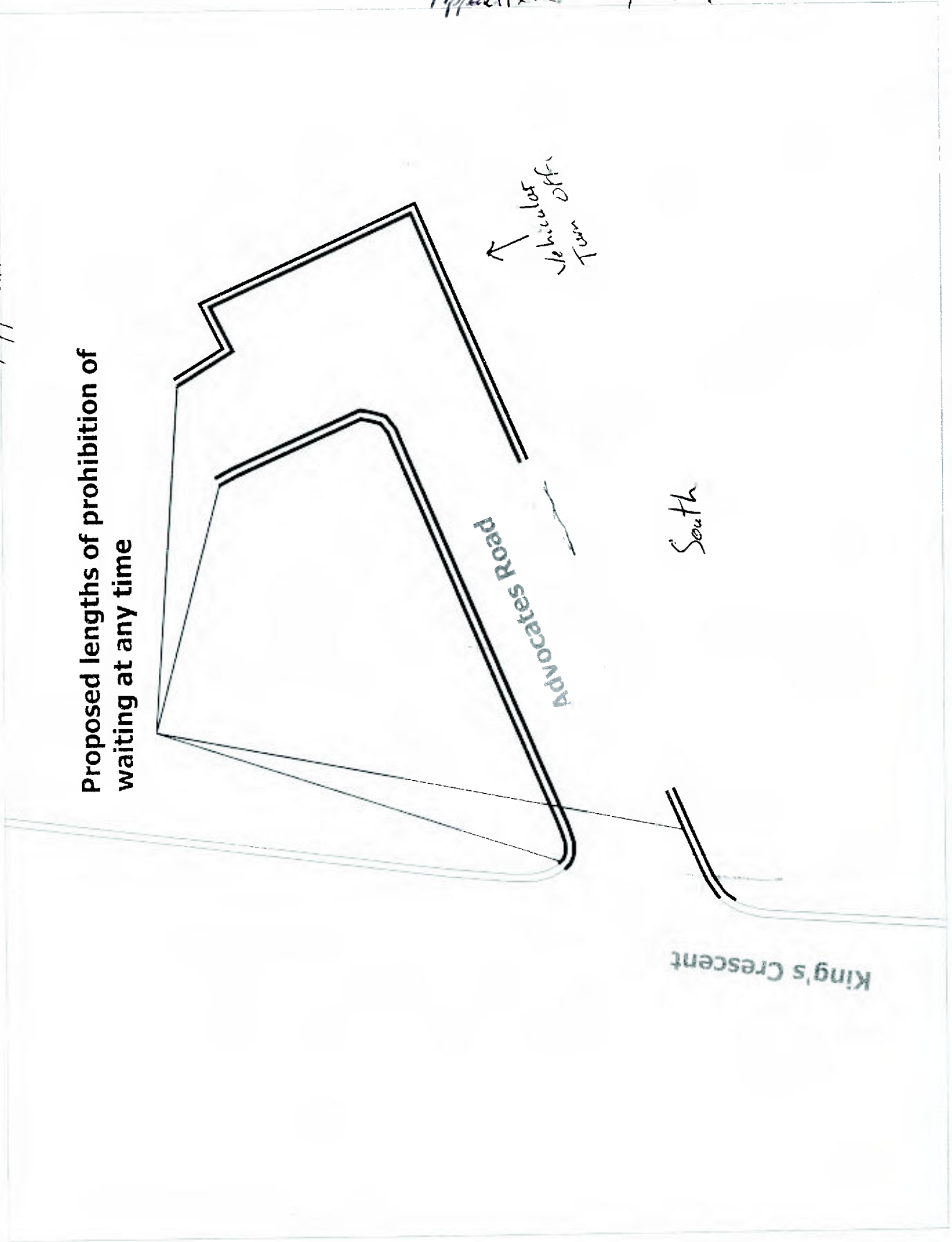
Graham W. J. Cooper
Partner – Ledingham Chalmers LLP

Appendix 6 ~~Site~~

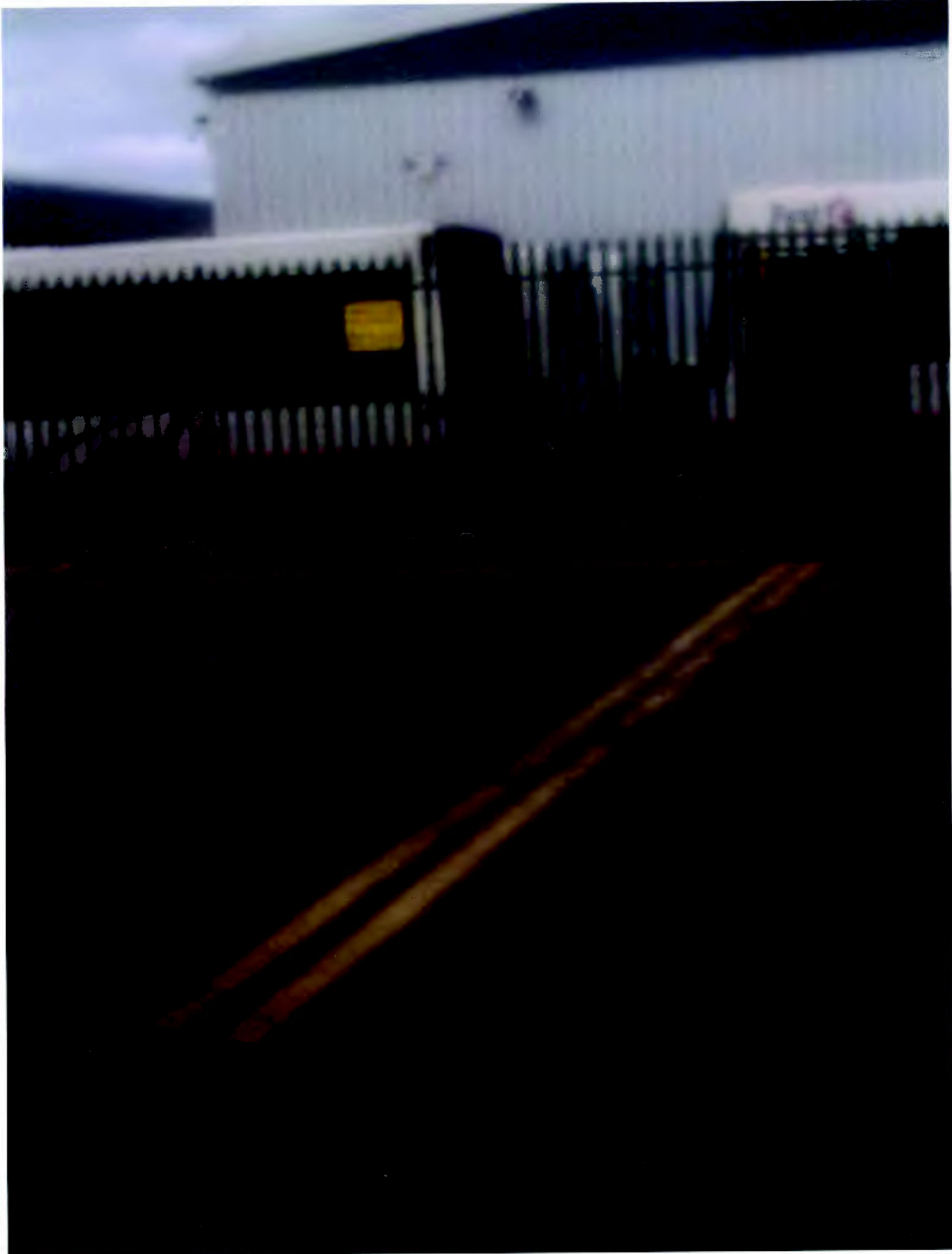


Advocates Road - Proposed lengths of prohibition of waiting at any time

Appendix 2.



Appendix 3.





The Aberdeen City Council (Mid Stocket Lane, Aberdeen)(Traffic Management) Order 2012 Objection

From: Macdonald, Dr Alister G.
Sent: 27 April 2012 12:13
To: 'traffmanagement@aberdeencity.gov.uk'
Subject: midstocket lane - parking space for the police

I have 3 comments on the proposed position of the parking space.

When the present yellow lines were decided (double on the west, single on the east side) the representative of the Police wanted to ensure clear access for emergency vehicles to evacuate the sheltered flats adjoining the lane. The present proposal fails to meet that need.

Parking close to a garage entrance is not a good idea. If my garage entrance was threatened I would oppose the present proposal vigorously. I am sceptical of anyone other than the owner/driver making the judgement about the space needed to reverse a car into or out of a garage conveniently.

The space for the police vehicle should be further down the lane, opposite a garden wall, and be clearly marked for Police only. Any parked vehicle seems to attract others and the threat of parking wardens is manifestly not a deterrent. The lane is frequently used, illegally, as a short term car park, often to the inconvenience of the residents of Argyll Place. However the allocated space for a police vehicle should improve the situation as the Police are frequent offenders. This is a well documented problem.

Yours Sincerely, A G Macdonald

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	31 May 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Strategic and Local Transportation Projects Update
REPORT NUMBER:	EPI/12/092

1. PURPOSE OF REPORT

The purpose of this report is to advise Members of the progress to date of various strategic and local transportation projects within Aberdeen City and the wider area. These projects flow from the development of the Regional Transport Strategy (RTS) produced by NESTRANS, and the Council's own Local Transport Strategy (LTS).

2. RECOMMENDATION(S)

It is recommended that Members:

1. Draft Fares and Ticketing Strategy:
 - a) agree the draft Aberdeen City and Shire Fares and Ticketing Strategy;
 - b) instruct officers to advise NESTRANS accordingly including any other comments Members may wish to make and;
 - c) instruct officers to continue working with local bus operators and partner organisations to implement the actions outlined within the Strategy.
2. Draft Regional Parking Strategy:
 - a) agree the draft Regional Parking Strategy and;
 - b) instruct officers to advise NESTRANS accordingly including any other comments Members may wish to make.
3. Bridge of Don Park and Ride Site Car Park:
 - a) agree the outcomes of the option assessment for a permanent location for the Bridge of Don Park and Ride Car Park;

- b) agree that existing Park and Ride Car Park provision at the Aberdeen Exhibition and Conference Centre (AECC) is retained on site as the permanent minimum provision and;
 - c) instruct officers to further consider how Options 6 and 10 can achieve the remaining capacity required for the optimum 1000 spaces in the context of the forthcoming AECC Development Framework, including public and stakeholder consultation.
4. Bridge of Dee Capacity Study:
 - a) note the findings and outcomes of the Bridge of Dee Pre-Appraisal;
 - b) instruct officers to publish the Pre-Appraisal report and invite public and stakeholder comment, including offering to meet with the adjacent Community Councils and;
 - c) instruct officers to continue to work with partners to undertake the formal assessment process as quickly as possible thereafter.
 5. NESTRANS
 - a) endorse development through NESTRANS as detailed in the relevant section of this report, including Board decisions and 2011/12 and 2012/13 programmes of work.
 6. Care North
 - a) Approve the attendance of an Elected Member, along with officers working on the project, at the CARE North transport and urban realm meeting in Gothenburg on 6-8th June 2012.
 7. Otherwise note the contents of this report.

3. FINANCIAL IMPLICATIONS

The projects described are being funded through various budgets including NESTRANS, the Regional Transport Partnership. Details are included in the relevant sections. There are no implications for approved PBB options.

4. OTHER IMPLICATIONS

None

5. BACKGROUND/MAIN ISSUES

A) Issues Requiring A Committee Decision

Public Transport

1 Draft Aberdeen City and Shire Fares and Ticketing Strategy

- 1.1 The Local Authorities Bus Operators Forum (LABOF) in its work to improve public transport across the North East region identified that there are a wide range of ticket types and products in circulation on North East bus services. The current multiplicity of fare levels, fare structures and ticket types can, by their nature, be difficult to understand, particularly amongst irregular or non-users, who are not necessarily aware of some of the best value tickets. It is recognised that working in partnership to address such matters, particularly through the development of integrated ticketing, could provide significant benefits.
- 1.2 A LABOF Task Group has been working together since summer 2011 to formulate a Draft Aberdeen City and Shire Fares and Ticketing Strategy, while also discussing the potential implementation of integrated and SmartCard / e-ticketing in Aberdeen City and Shire and ensuring this is embedded in the Strategy document.
- 1.3 A draft Strategy has since been produced and approved by the LABOF Steering Group on the 1st March 2012 and by the Nestrans Board on the 18th April 2012, who have subsequently referred it to both Aberdeen City Council and Aberdeenshire Council for consideration.
- 1.4 An Executive Summary of the draft Strategy is provided in Appendix A, whilst the full Draft Aberdeen City and Shire Fares and Ticketing Strategy can be provided on request.
- 1.5 Given the potential benefits of cross-boundary integrated ticketing and potential economies of scale in relation to e-ticketing it is considered that integrated and smart ticketing is best progressed at a regional level, rather than by any individual authority. On this basis the draft Strategy has been developed for both Aberdeen City and Shire.
- 1.6 The draft Strategy outlines the complex and restrictive legislative background to integrated ticketing; describes the current individual and multi-operator ticketing products that are available; discusses the potential benefits of integrated and smart ticketing; highlights examples of leading practice, and details an Action Plan to deliver the Strategy's aim and objectives.
- 1.7 The overall aim of the Strategy is for the Local Authorities to work in partnership with local bus operators to ensure that the travelling public are aware of, and have on offer, fares which represent value for money and ticket options which reflect their travel patterns, with the objectives being to enhance the image of the public transport product and, in turn, encourage passenger growth and revenue generation.

- 1.8 The Strategy will require the development of fares and ticketing products which will assist 'seamless' ease of travel for passengers, provide perceived value for money, speed passenger boarding and enable efficient service operation.
- 1.9 Given the significant constraints placed upon transport authorities and operators in promoting integrated multi-operator ticketing arrangements and products, the actions identified are considered both desirable and, importantly, deliverable. Short, medium and long term actions are proposed reflecting the priority and complexity of the issue to be addressed.
- 1.10 The draft Strategy has been informed from a number of different sources and strategies all of which have undergone significant public engagement. These include:
- NESTRANS's Regional Transport Strategy
 - The Aberdeen Local Transport Strategy
 - NESTRANS's Bus Action Plan
 - Bus Passenger Satisfaction Surveys
- 1.11 It is therefore recommended that this Committee:
- a) agree the draft Aberdeen City and Shire Fares and Ticketing Strategy;
 - b) instruct officers to advise NESTRANS accordingly including any other comments Members may wish to make and;
 - c) instruct officers to continue working with local bus operators and partner organisations to implement the actions outlined within the Strategy.

Car Parking

2 Draft Regional Parking Strategy

- 2.1 At its Board meeting on 15th February 2012, NESTRANS approved the draft Regional Parking Strategy for consideration by Aberdeen City Council and Aberdeenshire Council.
- 2.2 The draft Regional Parking Strategy, which is appended to this report as Appendix B, was prepared in discussion with officers of Aberdeen City and Aberdeenshire Councils and its objectives and actions have also been subject to discussion at a recent North East Transport Consultative Forum meeting.
- 2.3 The role of the NESTRANS Regional Parking Strategy is to set the high level policy for parking across the region. As the NESTRANS region is

such a large and diverse area covering Aberdeen City, Aberdeenshire towns, villages and rural areas, management and control of parking needs to play a different role in different areas.

2.4 The delivery and management of parking also falls primarily to the two Councils and to private car park operators. This Strategy therefore aims to set out the wider objectives and strategic direction for the region as a whole, rather than set out the specific mechanisms for delivery at a local level.

2.5 The draft Strategy recognises the following:

- Parking can play a significant role in maintaining and improving the economic vitality of town centres
- Management of parking has a role to play in managing traffic in general and encouraging more sustainable modes of travel, thereby contributing to easing congestion
- The volume of traffic and the level of congestion also have a direct impact on local air quality and parking management and control measures form a key element of the Air Quality Action Plan,

and also recognises that parking and its appropriate management is also important for:

- Business and residential amenity
- Park and Ride – both bus and rail
- Disabled access
- Supporting new initiatives such as the recently launched Car Club
- Cycle and motor cycle users.

2.6 The objectives for this strategy are as follows;

- To support the economic vitality of the city and town centres and the wider objectives of the Regional Transport Strategy through a balanced approach to the management of car parking.
- To support and influence increases in the proportion of journeys undertaken by sustainable modes, particularly by bus and rail.

2.7 A range of policies and actions have been developed, which would be the responsibility of the Councils and / or NESTRANS to progress, many of which Aberdeen City Council are already addressing, including but not restricted to:

- Review the current parking zones and restrictions
- Consider allowing local residents to use off-street car parks in the evenings and overnight
- Identify areas where illegal parking and loading causes particular congestion issues with the intention that enforcement can be targeted to address them

- Apply national guidance on maximum parking standards to all new developments and the introduction of car free or low car developments where appropriate
 - Support the set up of car clubs in the North East
 - Consider the benefits and feasibility of applying differential parking charges to reflect the impact of larger and more polluting vehicles.
- 2.8 The draft Regional Parking Strategy has been reviewed in the context of the Local Transport Strategy, the adopted Local Development Plan, the work of the Controlled Parking Working Group and the Priority Based Budgeting elements which refer to future increases in car parking charges and has been found to be fully complimentary with and supportive of these.
- 2.9 A few minor comments will be highlighted to Nestrans, specifically:
- The cost of business permits increases to £500 per year from 1st June 2012
 - City Wardens can tow away vehicles as well as issuing Penalty Charge Notices
 - Should there be mention of the quality of our car parks within the Strategy?
- 2.10 It is therefore recommended that this Committee:
- a) agree the draft Regional Parking Strategy and;
 - b) instruct officers to advise NESTRANS accordingly including any other comments Members may wish to make.

Major Projects

3 Bridge of Don Park and Ride Car Park Site

- 3.1 Reference is made to the minute of the meeting of this Committee on 15th November 2011, wherein Members agreed a short list of options for further investigation associated with the Bridge of Don Car Park. This short list was derived from a longer list of 9 options.
- 3.2 The short list included six options:
- 1) Do nothing
 - 2) Do minimum
 - 3) Developer led site at Blackdog
 - 4) Developer led site at Berryhill / Cloverhill
 - 5) Satellite sites
 - 6) AECC car park

- 3.3 The further investigation involved the aforementioned shortlisted options being assessed against the previously agreed Transport Objectives of the scheme and against the key criteria of the Scottish Transport Appraisal Guidance (STAG). These criteria are Environment, Safety, Economy, Integration, and Accessibility and Social Inclusion. This complies with best practice appraisal methodology undertaken for a range of transportation projects. Also considered were the likely Engineering / Construction Difficulty, Public Acceptability of the options (based on previous feedback to this project and not new consultation feedback), Feasibility and Affordability and the impact of the Aberdeen Western Peripheral Route (AWPR) on the proposals. The optimum option(s) would be that or those which best met the scheme and STAG objectives.
- 3.4 In order to carry out these further investigations it was necessary to firm up on the details of the options previously agreed. Whilst considering Option 2 - Do minimum, it was considered that the car park could remain on the site as existing but could also remain on the site but utilise a smaller footprint by the provision of a multi storey car park. The option was therefore expanded into two options 2a – site to remain as existing and 2b – site to remain at existing location but with a reduced footprint, accommodated by a multi-storey car park. Discussions were undertaken with the Berryhill / Cloverhill developer and it was understood from these early discussions that a reduced provision from the originally anticipated 1000 spaces could be accommodated at the proposed site. It was now only possible to consider 250 surface or 500 multi storey parking spaces at the site. In order to properly consider these options they were expanded into two options 4a – Berryhill / Cloverhill 250 spaces and 4b – Berryhill / Cloverhill 500 spaces.
- 3.5 A further new option arose from the traffic modelling element of the assessment process. As the options were being tested it was apparent that the provision of 1000 spaces provided the best impact on the network in terms of reduced congestion and vehicle journey times. In order to achieve this optimum level of provision, the joint use of the existing Park and Ride car park site and the proposed Blackdog site were considered. This created Option 10 – Option 2a and Blackdog.
- 3.6 Table 1 below summarises the results of the assessment process which are provided in greater detail in Appendix C, along with a plan showing the locations of the various options. All elements are valued on a scale of +3 to -3 (a 7 point scale with 0 = Neutral benefit/impact, +3 as major benefit and -3 as major negative impact / disbenefit). These results show that the provision of 1000 spaces or thereby provides the greatest potential advantage to the network in terms of removing traffic, reducing congestion and promoting modal shift.

	1	2a	2b	3	4a	4b	5	6	10
Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion, improved journey times and benefits public transport users	-	-	-	-	-	-	2	2	2
Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes	-3	1	1	1	-1	1	2	2	2
Deliver overall environmental benefits	-1	1	0/1	1	-1	1	3	3	3
Reduce negative environmental impacts -to a minimal level -when developing infrastructure, including provision of appropriate mitigation measures	0	0	0/-1	-1	-1	-1	-2	2	0
Environment	-	-	-	-	-	-	-	-	-
Safety	-1	1	1	1	0	1	1	2	2
Economy (Support of economic aspirations)	-1	1	1	1	-1	1	2	2	2
Integration (with the transport network)	0	1	1	2	2	2	2	1	2
Accessibility and Social Inclusion	-2	1	1	1	1	1	2	2	2
Engineering / construction difficulty	0	2	-2	-2	-2	-2	-2	2	1
Public acceptability of options	-1	2	1	0/-1	-1	0	2	2	1
Feasibility and affordability	0	2	-2	1	-2	-2	-2	2	1
Impact of Aberdeen Western Peripheral Route on proposal	-1	0	0	1	0	0	1	0	1
Overall score (NB not average)	-2	1	0	0	-1	-1	1	2	2

Table 1 – Summary of assessment

3.7 Of the options that provide 1000 spaces, engineering difficulties and unknown costs are likely to arise from the provision of satellite sites. These sites all require new sites and access roads to be constructed.

3.8 Discussions have been ongoing with the developer of the Berryhill/ Cloverhill site and subsequent to the conclusion of the assessment, they

now consider that land could be made available to provide up to 1000 spaces. A provisional estimate has been provided by the developer of the likely land rental costs to accommodate a potential 900 space surface car park on 8 acres. This land area is not inclusive of external landscaping or SUDS drainage requirements as these will be accommodated elsewhere on the site and this has not been verified by officers due to the timescales of the submission of the offer. Based on the developer's estimates, land rental would cost approximately £180,000 per annum for a 125 year term, increasing by £22,500 per additional acre per annum were the site size to be larger than that identified by the developer. An additional contribution to major infrastructure i.e. the development site's access road, site servicing and SUDS drainage system, of £60,000 per annum is also identified and would be subject to increased costs of £7,500 per acre per annum as previously noted. Infrastructure to support the development is usually funded by the developer. A further offer to rentalise the cost of constructing the car park could also be considered though no terms have been discussed. The usual Council procurement route for a 1000 space car park would incur capital construction costs of an estimated £10M-£12M, based on similar projects designed and/or delivered by the Council. Whilst the capital costs and revenue developer proposals are not like for like, it is clear there is still a significant cost involved in this particular option, either through traditional non-housing capital financing or negotiated lease arrangements. A significant long term budget allocation would therefore be required to provide a P&R at this site under the above terms and at this time this option would not be considered cost effective for Aberdeen City Council and this is reflected in the summary assessment Table 1 above.

- 3.9 The use of the Blackdog site has both positive and potentially negative implications. The delivery of the site is dependent on the delivery of the AWPR as well as Aberdeenshire Council Planning Authority decisions. The timescale for the AWPR is not yet known due to the outstanding legal challenge. The AWPR will provide linkages to proposed and existing Park and Ride sites which fulfils an aspiration of the regional and local transport strategies. However there is potential that a Park and Ride site at Blackdog may attract users of the existing Ellon or Bridge of Don Park and Ride car parks which would not be desirable. The terms of occupancy and use of the Blackdog site have yet to be discussed fully with the developer and therefore remain unquantifiable at this time.
- 3.10 The outcome of this assessment process reveals that the site option which appears to be financially and otherwise the most achievable and offers the most positive results in meeting the objectives is the use of the existing AECC car park. The majority of the infrastructure is in place, such as the access roads, bus priority on the main transport corridor and car parking provision. Work may be required to provide a new accommodation block

and bus turning area should the car park require to be relocated within the AECC site. This option would meet the desire of stakeholders that the site be multifunctional, for example it could be used throughout the day as a Park and Ride and at night for functions at the AECC. It may have implications for the AECC in terms of its proposed tendering process for a development partner to maximise the use of the AECC site, however this would protect the existing Park and Ride site from redevelopment and maintain a valuable public resource that meets with many transportation, health and economic benefits to the locality and wider City.

- 3.11 The Energetica Concept recognises that 'in a global business environment, connections are everything' and the connectivity available from the AECC site supports this. In terms of sustainability 'Energetica is a concept born of innovation' and the AECC site provides the opportunity to tie in with various sustainable projects that would be less feasible on developer sites where long term access to the site may not be guaranteed. Opportunities exist at the AECC site for electric vehicle points, hydrogen bus trials and active travel improvements due to the proximity to various renewable energy projects, residential areas and business and employment centres.
- 3.12 Works have begun on a forthcoming Development Framework for the AECC site which would take into account any potential options regarding the Bridge of Don Park and Ride site and consider these alongside the evolving Development Framework.
- 3.13 Given the potential opportunities which can be investigated by the forthcoming development framework for the AECC site and the outcomes of the appraisal, it seems appropriate that the Bridge of Don Park and Ride site is retained permanently on the AECC site at its present capacity. Further investigation requires to be undertaken via the development framework to explore the delivery in the longer term of the optimum sized 1000 space car park to meet the transportation needs of the growing City and Shire population as well as support the other functions of this location, ensuring that this valuable City resource is maximised for all. The Blackdog option can be revisited at a future date should further expansion on the AECC site not prove possible.
- 3.14 Funding has been secured from the NESTRANS 2012/13 programme to continue with a more detailed study into the recommended options this financial year.
- 3.15 It is therefore recommended that the Committee:
 - a) agree the outcomes of the option assessment for a permanent location for the Bridge of Don Park and Ride Car Park;

- b) agree that existing Park and Ride Car Park provision at the Aberdeen Exhibition and Conference Centre (AECC) is retained on site as the permanent minimum provision and;
- c) instruct officers to further consider how Options 6 and 10 can achieve the remaining capacity required for the optimum 1000 spaces in the context of the forthcoming AECC Development Framework, including public and stakeholder consultation.

4 Bridge of Dee Capacity Study

4.1 Background

4.1.1 A study commenced in 2010/11 to investigate capacity issues and potential opportunities relating to the existing transport network in the Bridge of Dee area of Aberdeen using the methodologies set out in Scottish Transport Appraisal Guidance (STAG) and Design Manual for Roads and Bridges (DMRB). This update on the latest progress of the study gives a summary of the pre-appraisal process and seeks approval to commence the formal STAG and DMRB Stage 1 assessment.

4.1.2 Information and documentation on this key project are available on the Aberdeen City Council website at the following link: http://www.aberdeencity.gov.uk/Roads/transport_projects/roa_access_from_south_home.asp. A report on the Pre-Appraisal process including traffic modelling outcomes and overall conclusion of the study has been prepared and is available on the website.

4.2 Work to Date

4.2.1 A programme was developed to complete the pre-appraisal process defined within STAG and partially complete the DMRB Stage 1 assessment. Full engagement with stakeholders has been and will continue to be undertaken throughout the process. To date the following packages have been completed:

- Inception - identified existing data, reviewed relevant policies and strategies and agreed do-minimum and reference case traffic modelling scenarios and gathered predicted traffic volumes and journey time information within these scenarios.
- Stakeholder workshop 1 (29-11-10): Analysis of existing and future problems and opportunities and setting of project specific SMART (specific, measurable, attainable, relevant and timed) objectives for the study.

- Stakeholder workshop 2 (26-1-11): Option generation – options were suggested that may address the problems and achieve the objectives of the study.
- Stakeholder workshop 3 (3-3-11): Option Sifting and Development – considered previously generated options and sifted them to see if any could be discounted in response to the identified problems and opportunities and the project specific objectives. This included initial consideration of high level engineering data such as alignments, junctions, earthworks, structures and drainage design, together with high level cost estimates and transport and environmental impacts associated with each of the options.

4.2.2 Based on the outcomes of the evaluation noted above the finalised project objectives adopted in respect of this study are as follows:

- To support the implementation of the current Development Plans by 2030 in a manner which does not result in increased journey times compared to 2010 for all classes of road users;
- To incorporate measures which benefit public transport and active travel and encourage modal shift from private car use;
- Improve safety, security, amenity and connectivity for non-motorised road users and communities within the study area;
- Deliver air quality and noise impact benefits in areas adjacent to the local road network by directing traffic towards the strategic road network;
- Recognise the importance of the River Dee SAC (Special Area of Conservation) and the Category A Listed Bridge of Dee and develop proposals to minimise overall environmental impacts, including at these locations, to a level acceptable to the consenting authority; and
- Support the effective operation of the local and national transport networks, including use by traffic of appropriate distributor routes.

4.2.3 The option sifting process has identified which of the proposed concept options are likely to address the problems and achieve the objectives of the study and are therefore worth taking forward for further assessment through the STAG appraisal process and any concepts that should be discounted from further evaluation at this stage. The following is a summary of the outcomes for each option considered:

4.2.4 Concept 1: Public Transport Enhancement / Modification of Existing Infrastructure

This concept was identified to assess the extent to which an option of enhancing existing public transport service levels and carrying out minor modifications to existing transport infrastructure would be capable of meeting the project objectives. It includes construction of a new

pedestrian / cycle bridge adjacent to the Bridge of Dee, facilitating a reduction in the footway provision on the Bridge of Dee and allowing it to accommodate both HGVs and buses. Use of the Bridge of Dee by buses supports the introduction of an orbital bus route on the A90.

Potential variants to this concept are:

- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that as this Concept would be unlikely to be Operationally Effective, and that only one of the Project Objectives was likely to be met, it should be sifted out at this stage, and should not proceed to further appraisal.

4.2.5 Concept 2: Improvement of Existing Junctions

This concept was identified to assess the extent to which improvements to the existing junctions, including major modifications, would be capable of meeting the project objectives. It includes construction of a new pedestrian / cycle bridge adjacent to the Bridge of Dee, for the same purpose as noted for Concept 1.

Potential variants to this concept are:

- At-grade junction improvements;
- Grade separated junction improvements;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location; and
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that:

- As the At-grade Junction Variant would be unlikely to be Operationally Effective, and that only one of the Project Objectives was likely to be met, it should be sifted out at this stage and should not proceed to further appraisal;
- As the At-grade Junction with Link Road to Leggart Terrace Variant would be unlikely to be Technically Feasible or Deliverable and that only one of the Project Objectives was likely to be met, it should be sifted out at this stage and should not proceed to further appraisal; and
- As the Grade Separated Junction Variant would be unlikely to be Deliverable, it should be sifted out at this stage and should not proceed to further appraisal.

4.2.6 Concept 3: Adaptation of Existing Bridges

This concept was identified to assess the extent to which adaptation of the existing Bridge of Dee and King George VI Bridge would be capable of meeting the project objectives. It includes construction of a new pedestrian / cycle bridge adjacent to the Bridge of Dee, for the same purpose as noted for Concept 1, and increasing the capacity of King George VI Bridge by introducing additional road space.

Potential variants to this concept are:

- Operating both Bridge of Dee and King George VI Bridge as two-way bridges;
- Operating Bridge of Dee and King George VI Bridge as one-way bridges functioning as a large gyratory;
- Restricting the use of the Bridge of Dee to public transport / HGV / High Occupancy Vehicles;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location;
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that as this Concept would be unlikely to be Operationally Effective, and that only one of the Project Objectives was likely to be met, it should be sifted out at this stage and should not proceed to further appraisal.

4.2.7 Concept 4: Re-Direction of Traffic

This concept was identified to assess the extent to which re-direction of traffic to other existing crossing points of the River Dee would be capable of being accommodated within the transport network and whether such re-direction would be sufficient to alleviate pressure on the Bridge of Dee and thus be capable of meeting the project objectives. It includes construction of a new pedestrian / cycle bridge adjacent to the Bridge of Dee, for the same purpose as noted for Concept 1, and provision of new infrastructure as required to support the re-direction of traffic.

Potential variants to this concept are:

- Re-directing traffic by means of a new link between the A90 in the Loirston Area and Provost Watt Drive;
- Re-directing traffic via the A956 Wellington Road / West Tullos Road Corridor, including upgrading that corridor as necessary;
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that:

- As the Re-direction via the A956 Corridor Variant was unlikely to be Deliverable and that only one of the Project Objectives was likely to be met, it should be sifted out at this stage, and should not proceed to further appraisal;
- As the Re-direction via A90 / Provost Watt Drive Variant would be unlikely to be Deliverable, and that only two of the Project Objectives were likely to be met, it should be sifted out at this stage and should not proceed to further appraisal; and
- As the Re-direction via A956 / Provost Watt Drive would be unlikely to be Deliverable, and that only two of the Project Objectives were likely to be met, it should be sifted out at this stage and should not proceed to further appraisal.

4.2.8 Concept 5: Additional Downstream Crossing

This concept was identified to assess the extent to which provision of an additional downstream crossing would be capable of meeting the project objectives. It includes construction of a new pedestrian / cycle bridge adjacent to the Bridge of Dee, for the same purpose as noted for Concept 1.

Potential variants to this concept are:

- Cross-section arrangements for both the Bridge of Dee and the new downstream crossing;
- Operational arrangements for the Bridge of Dee and the new downstream crossing;
- Restricting the use of the Bridge of Dee to public transport / HGV / High Occupancy Vehicles;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location;
- The potential to provide a new northern link to the A90 from the connection between the new downstream crossing and Holburn Street; and
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that:

- As the Additional Downstream Crossing Variant was not unlikely to satisfy the Technical Feasibility, Operational Effectiveness, Deliverability and Value for Money / Affordability criteria, it should not be sifted out at this stage, and should proceed to further appraisal; and
- As the Additional Downstream Crossing with Northern Extension to A90 Variant would be unlikely to be Deliverable, and that only two

of the Project Objectives were likely to be met, it should be sifted out at this stage, and should not proceed to further appraisal.

4.2.9 Concept 6: Additional Upstream Crossing

This concept was identified to assess the extent to which provision of an additional upstream crossing would be capable of meeting the project objectives. It includes construction of a new pedestrian / cycle bridge adjacent to the Bridge of Dee, for the same purpose as noted for Concept 1.

Potential variants to this concept are:

- Location of the upstream crossing;
- Cross-section arrangements for both the Bridge of Dee and the new upstream crossing;
- Operational arrangements for the Bridge of Dee and the new upstream crossing;
- Restricting the use of the Bridge of Dee to public transport / HGV / High Occupancy Vehicles;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location;
- The potential to provide a new northern link to the A90 from the connection between the new upstream crossing and Garthdee Road;
- The potential to provide a new northern link between Garthdee Road and North Deeside Road in the Inchgarth area; and
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that:

- As the Additional Upstream Crossing (Inner Line) Variant was unlikely to be Deliverable it should be sifted out at this stage and should not proceed to further appraisal;
- As the Additional Upstream Crossing (Central Line) Variant was unlikely to be Deliverable it should be sifted out at this stage and should not proceed to further appraisal;
- As the Additional Upstream Crossing (Outer Line) Variant was not unlikely to satisfy the Technical Feasibility, Operational Effectiveness, Deliverability and Value for Money / Affordability criteria, it should not be sifted out at this stage and should proceed to further appraisal;
- As the Additional Upstream Crossing (Outer Line) with Link to A90 Variant was not unlikely to satisfy the Technical Feasibility, Operational Effectiveness, Deliverability and Value for

Money / Affordability criteria, it should not be sifted out at this stage and should proceed to further appraisal; and

- As the Additional Upstream Crossing (Outer Line) with Link to A93 Variant was not unlikely to satisfy the Technical Feasibility, Operational Effectiveness, Deliverability and Value for Money / Affordability criteria, it should not be sifted out at this stage and should proceed to further appraisal;

4.2.10 Concept 7: Additional Adjacent Crossing

This concept was identified to assess the extent to which provision of an additional adjacent crossing would be capable of meeting the project objectives.

Potential variants to this concept are:

- Provision for pedestrian / cyclist facilities either within the new adjacent crossing or by amending the use of the existing Bridge of Dee;
- Location of the adjacent crossing;
- Cross-section arrangements for both the Bridge of Dee and the new adjacent crossing;
- Operational arrangements for the Bridge of Dee and the new adjacent crossing;
- Restricting the use of the Bridge of Dee to public transport / HGV / High Occupancy Vehicles;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location; and
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that as this Concept would was not unlikely to satisfy the Technical Feasibility, Operational Effectiveness, Deliverability and Value for Money / Affordability criteria, it should not be sifted out at this stage, and should proceed to further appraisal;

4.2.11 Concept 8: Widening of the Bridge of Dee

This concept was identified to assess the extent to which widening of the existing Bridge of Dee would be capable of meeting the project objectives. It includes pedestrian / cyclists facilities within the widened crossing. It was recognised that due to the nature of the existing Bridge of Dee, any widening proposal would require works within the River Dee SAC.

Potential variants to this concept are:

- Cross-section arrangements for the widened crossing;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location; and
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that while this Concept was unlikely to satisfy the Deliverability criterion should other viable alternatives be available, it was considered that it should not be sifted out at this stage and should proceed to further appraisal to allow for further consideration in the event that other alternatives are not considered viable during more detailed appraisal.

4.2.12 Concept 9: Replacement of the Bridge of Dee

This concept was identified to assess the extent to which, in the event that proposals to widen the existing Bridge of Dee were not capable of being progressed due to their implications for the River Dee SAC, replacement of the existing Bridge of Dee would be capable of meeting the project objectives. It includes pedestrian / cyclists facilities within the replacement crossing.

Potential variants to this concept are:

- Cross-section arrangements for the replacement crossing;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location; and
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that while this Concept was unlikely to satisfy the Deliverability criterion should other viable alternatives be available, it was considered that it should not be sifted out at this stage and should proceed to further appraisal to allow for further consideration in the event that other alternatives are not considered viable during more detailed appraisal.

4.2.13 Concept 10: Free Flow North-South Movements

This concept was identified to assess the extent to which provision of a free flow link between the A90 south of the Bridge of Dee and the A90 north of the Bridge of Dee would be capable of meeting the project objectives. It includes construction of a new pedestrian / cycle bridge adjacent to the Bridge of Dee, for the same purpose as noted for Concept 1.

Potential variants to this concept are:

- The nature of the free-flow link as elevated structure or tunnel;
- The potential need to divert Leggart Terrace southwards to connect to the A90 at a new location; and
- The incorporation of measures to prioritise public transport.

Following review of the option sifting findings it was concluded that:

- As the Flyover Variant was unlikely to satisfy the Deliverability and Value for Money / Affordability criteria, it should be sifted out at this stage and should not proceed to further appraisal; and
- As the Tunnel Variant was unlikely to satisfy the Deliverability and Value for Money / Affordability criteria, it should be sifted out at this stage and should not proceed to further appraisal.

4.3 Conclusions

- 4.3.1 The draft pre-appraisal report available on the Council website gives a detailed summary of the work undertaken to date.
- 4.3.2 The option sifting process takes into account feedback from the third stakeholder workshop held in March 2011 and subsequent consultations with Historic Scotland and Scottish National Heritage. Although strategic traffic modelling provided an indication as to the operational effectiveness of the various concept options, further more detailed microscopic traffic modelling was undertaken on some options to verify their operational effectiveness. The draft pre-appraisal report gives further detail on the detailed modelling process but the outcomes did not impact on the concepts that are now being proposed for taking forward to the next stage.

4.4 Next Stage

- 4.4.1 The detailed modelling has been concluded along with the draft STAG pre-appraisal report. At its meeting in February 2012 the Nestrans Board approved a sum of £175,000 within the 2012/13 Revenue Budget to continue with the Bridge of Dee Study and commence the formal STAG and DMRB Stage 1 assessment.
- 4.4.2 Stakeholder involvement is a key element of the STAG process to ensure that all interests are considered in an open manner, to maximise confidence in the process and to, as far as is possible, reach consensus on outcomes. Whilst the pre-appraisal report is not a primary decision stage in the process, it is recommended that feedback is provided through the publication of the pre-appraisal report and an opportunity given for the public to comment.
- 4.4.3 In order to minimise delay in concluding the pre-appraisal process and starting the formal full assessment process, the pre-appraisal report has

been published on the Aberdeen City Council website alongside all other information relating to the study that is already available. This will give the public an opportunity to comment and an offer will be extended to meet with adjacent Community Councils. Any further feedback will be reported to a future meeting of this Committee.

4.5 It is therefore recommended that Members:

- b) note the findings and outcomes of the Bridge of Dee Pre – Appraisal;
- c) instruct officers to publish the Pre-Appraisal report and invite public and stakeholder comment, including offering to meet with the adjacent Community Councils and;
- d) instruct officers to continue to work with partners to undertake the formal assessment process as quickly as possible thereafter.

NESTRANS

5 NESTRANS Progress and Programmes

5.1 The NESTRANS Board met on the 8th December 2011 and the 15th February 2012 and a copy of the minutes of both these meetings is available within this report in appendix D. The minute of the latest NESTRANS Board meeting on the 18th April are to be approved at their next meeting and will be provided in a report to follow.

5.2 NESTRANS Capital Programme 2011/12

The NESTRANS capital programme for 2011/12 is now complete and a summary of each of the completed projects is provided below.

5.2.1 Active Travel

Core Paths

All works have been completed and comprised:

- Core Path 6 – Seaton Park drainage and resurfacing
- Core Path 27 – Den of Maidenraig path upgrade
- Core Path 78 – Coastal path upgrade
- Core Path 56 – Hazlehead path upgrade
- Core Path 26 – Grandholm path bridge replacement
- Promotional Signage and Leaflets

Aberdeen to Blackburn Cycle Route

Works completed. See section 7.

Cycle Demonstration Project

Works completed. See section 8.

5.2.2 Public Transport

BPIP Buchan Corridor / King Street Bus Lane Construction

A new bus lane southbound on King Street between East North Street and Castle Street became operational in August 2011.

Holburn Street Bus Lane Changes

An extension of the southbound bus lane on Holburn Street (both length of bus lane and operating hours) was completed in January 2012.

Links Road / Beach Boulevard Junction Improvements

The existing stop line has been moved back, traffic signal loops relocated and signal timings amended at this junction.

Jesmond Drive / Scotstown Road Junction

The bell mouth on the south / west side of the junction has been widened to allow buses to manoeuvre into Jesmond Drive without blocking both lanes on the approach to the junction.

Aberdeen City and Shire Bus Stop Information Initiatives

Bus timetable display cases have been replaced and / or provided at bus stops to ensure DDA compliance, that minimum standards and content can be provided, and which allows for standardised region wide improvements to timetabling information.

Night Time Transport Zone

Night time bus stops on Union Street have been implemented.

No 59 Bus Infrastructure Improvements

An infrastructure audit took place in support of a new hybrid bus fleet that is to be introduced and lay-by improvements were implemented on Foresterhill Road.

A96 Park & Ride

Legal costs for land acquisition.

5.2.3 Strategic Road Capacity Improvements

Contribution to 3rd Don Crossing

Site investigation work completed. Scottish Ministers have confirmed the CPO.

Berryden Corridor Improvements

Design work completed.

Segregated left turn Great Southern Road to Stonehaven Road
Traffic modelling carried out and indicated that no further work is required.

Guild Street / Wapping Street Signals
A fibre optic link cable was provided to implement SCOOT at this junction to improve vehicular and pedestrian flow.

Hareness Road / Souterhead Road Junction Improvements
Further investigation into a strategic improvement to be deferred until the impact of construction of the AWPR and outcome of the Bridge of Dee Study are known.

5.2.4 Strategic Road Safety Improvements

Kingswells Roundabout Toucan Crossing
A toucan crossing and footpath links were installed over the C89C north arm of Kingswells roundabout to assist cyclists and pedestrians along the A944 corridor.

Riverside Drive Variable Message Sign
Two variable message signs have been installed.

5.2.5 Strategic Road Prioritised Maintenance

A956 Ellon Road – Southbound from the Parkway to North Donside Road
Resurfacing work completed.

A956 Ellon Road – Southbound from North Donside Road to Balgownie Road
Resurfacing work completed.

A944 Skene Road
Resurfacing work completed.

Great Southern Road
Contribution to resurfacing work.

5.2.6 Rail

Access for All
Accessibility improvements to rail stations in the North East were agreed with Network Rail and implemented at Huntly, Inch, Inverurie, Stonehaven, Laurencekirk and Portlethen stations.

5.2.7 Various

Car Club

The Car Club was successfully launched in April 2012. See Section 6.2.

5.3 NESTRANS Revenue Programme 2011/12

The NESTRANS revenue programme for 2011/12 is now complete and a summary of each of the completed projects is provided below.

5.3.1 Rail Action Plan

Contribution to Dyce Shuttle Bus

Contribution complete.

5.3.2 Freight Action Plan

Care North Year 3 of 3

Contribution complete.

5.3.3 Bus Action Plan

Bus Link Improvements to Anderson Drive

Works complete and further work commissioned.

Bridge of Don Park and Ride Feasibility Study

Environmental and Transport Assessment completed. See Section 3.

Aberdeen Royal Infirmary Interchange

Design work completed.

Bus Lane Enforcement Camera Procurement

Business case prepared on options for bus lane enforcement.

5.3.4 Project Feasibility and Monitoring

Bridge of Dee

Draft summary report has been prepared and requires to be agreed before moving onto next stage of the scheme. See Section 4.

Rob Roy Bridge Feasibility Study

Alignment design investigatory work has been completed.

5.4 The total received from NESTRANS during 2011/12 under capital and revenue was £1,510,313.79 and £188,434.51 respectively.

5.5 NESTRANS Capital Programme 2012/13

The capital programme expenditure for 2012/13 was approved at the NESTRANS board meeting on 18th April 2012 and details of the programme are listed below.

5.5.1 Active Travel

Core Paths (£266,000)

Provision / upgrade of selected Core Paths, promotional signage and leaflets and detailed design of schemes for 2013/14.

Aberdeen to Blackburn Cycle Route (£75,000)

Install Toucan crossings at either end of the new shared pedestrian / cycle route and sign and line the route in support of the revised Traffic Regulation Order.

Cycle Demonstration Project (£115,000)

Develop the feasibility work carried out in 2011/12 to install a series of new paths throughout the Greenbrae School catchment area and to formalise a number of shared use links through signing and lining.

5.5.2 Public Transport

A96 Park and Choose (£135,000)

Specimen design for inclusion in AWPR contract.

Aberdeen City and Shire Joint Bus Stop Information Initiatives (£60,000)

Replace and / or provide bus timetable display cases at bus stops within Aberdeen City and Shire.

Upgrade Bus Lane Enforcement Cameras on Strategic Bus Corridors (£200,000)

Install new camera system to improve enforcement of bus lanes.

ARI Interchange (£60,000)

Construction of improved interchange and associated bus priority measures (jointly funded with NHS Grampian).

Airport Bus Turning Circle (£25,000)

Design, planning and legal costs relating to land.

5.5.3 Strategic Road Safety Improvements

Road Studs and Lining (£40,000)

Renewing road studs and relining works on Wellington Road and the A944.

5.6 NESTRANS Revenue Programme 2012/13

The revenue programme expenditure for 2012/13 was approved at the the same meeting and details of the programme are listed below.

5.6.1 Bus Action Plan

Bridge of Don Park & Ride Feasibility (£20,000)

See Section 3. Continue with detailed study into recommended options.

Bus Link Improvements to Anderson Drive (£15,000)

Continue to investigate possible improvements to public transport movement along and across Anderson Drive post-AWPR.

Upgrade of Backroom Office Equipment for Bus Lane Decriminalisation (£55,000)

To facilitate the enforcement of bus lane violations.

King Street Bus Lane (£5000)

To implement the findings of a recent safety audit.

Night Time Transport Zone (£8000)

For the installation of lit signs at night time bus stops on Union Street.

5.6.2 Rail Action Plan

Contribution to Dyce Shuttle Bus (£46,000)

5.6.3 Project Feasibility and Monitoring

Bridge of Dee - Project Feasibility & Development (£175,000)

Complete and publish Pre Appraisal and commence formal appraisal process. See Section 4 for further details.

5.7 NESTRANS's total capital and revenue expenditure within Aberdeen City for 2012/13 is £976,000 and £324,000, respectively

5.8 It is therefore recommended that Members endorse development through NESTRANS, including Board decisions and 2011/12 and 2012/13 programmes of work.

6 CARE North (Carbon Responsible Transport Strategies)

6.1 Aberdeen City Council is a Partner in a three year European Union (EU) Interreg IVB CARE North (Carbon Responsible Transport Strategies for the North Sea Region) project. The brief is to 'develop innovative carbon reduction strategies for urban transport to maintain and improve

accessibility in a more carbon responsible way'. An update on various projects being progressed as part of CARE North is provided in this section.

6.2 Car Club

6.2.1 On Monday 2nd April the Commonwheels Car Club was formally launched in Aberdeen by Dr Margaret Bochel, Head of Planning and Sustainable Development. The event was publicised in a number of local papers and forums including the Press and Journal, Evening Express and Northsound radio.

6.2.2 The Car Club currently has eleven cars: two Ford Fiesta Econetics, two Hyundai i10s, five Kia Picantos, one Kia Rio and one Kia Sedona. Members book a car via the phone or the internet when they need it and are charged on a pay-as-you-drive basis.

6.2.3 There are currently eight on-street spaces available at Queen Street, Albyn Place, South Silver Street, Hollybank Place, East Craibstone Street, Kittybrewster depot and Rosemount Viaduct. Three spaces are also provided in Marischal College surface access car park but it is hoped that these will be relocated to Littlejohn Street in due course.

6.2.4 In the next twelve months it is hoped to expand the Car Club to other on-street locations as well as to include electric cars. A trial of hydrogen vehicles also took place in May.

6.2.5 Further information is available at www.commonwheels.org.uk or www.aberdeencity.gov.uk/lez

6.3 Transport Masterplan for the City Centre

6.3.1 The City Centre Development Framework (CCDF) was agreed at the Enterprise, Planning and Infrastructure Committee on 24 May 2011. This document promotes a strategy to complement and enhance the features of the City Centre that make Aberdeen unique. The Development Framework identifies 9 character areas and urban quarters focussed on Union Street as the spine of the City Centre. In order to fully meet the aspirations of this Framework, as well as the City's statutory obligations on air quality, there is a requirement to consolidate ongoing and proposed transport projects within the City Centre into one document.

6.3.2 This consolidated document will take the form of the City Centre Transport Masterplan (CCTM). It will enhance and further develop the transport themes contained within the CCDF and will seek to provide a comprehensive and detailed guide to how transport connections will

develop over the next 5 to 10 years and how these connections will help to achieve regeneration of the City Centre as a whole. Although there is an adopted Local Transport Strategy, there is a requirement for a site specific document that deals with City Centre regeneration and specifically the transport element of this in a holistic manner.

6.3.3 The aim of the CCTM is to develop a layered modal strategy for improvement within the City Centre. It will propose policies, strategies or interventions to improve the existing situation and to fill any gaps in transportation infrastructure that have been identified. The policies, strategies and interventions will be consistent with and will assist in the delivery of the objectives and outcomes of all key national, regional and local documents. The layered modal strategy will also provide the basis for prioritising the delivery of improvements and setting timescales for action and will inform the production of an action and delivery programme. This key stage will be developed with stakeholders who broadly represent the users of the City Centre as well as the CCTM Project Team and internal Council colleagues. Once a draft modal strategy has been developed, more broad public consultation will be undertaken.

6.3.4 The Project Team who will be directly involved in the preparation of the CCTM will consist of:

- Aberdeen City Council officers (Transportation Strategy and Programmes and Masterplanning, Design and Conservation)
- NESTRANS, and
- Robert Gordon University (RGU)

The NESTRANS Regional Transport Strategy recognises the importance of a strong City Centre for the economy of the region as a whole. RGU is a partner in the CARE North project.

6.3.5 To date, the Project Team has met and agreed the principle and need for a CCTM document. The work involved in preparing a draft CCTM will take several months and it is envisaged that a draft document will be ready for submission to the appropriate Committees later in 2012. Committees will receive regular updates on the progress of the CCTM.

6.4 Electric Vehicle Procurement Support Scheme 2011/12

6.4.1 Aberdeen City Council was awarded a grant of £59,000 to purchase and install electric charging infrastructure under the Electric Vehicle Procurement Support Scheme 2011/12.

6.4.2 The objective of this Scottish Government scheme is to accelerate the progress of decarbonising road transport by promoting the uptake of electric vehicles in the Scottish fleet and providing a network of supportive

charging infrastructure by using public sector procurement as an exemplar model.

6.4.3 The scheme aims to:

- Provide a platform for electric vehicles to demonstrate their functionality and potential for emissions reduction;
- Develop a network of charging infrastructure to support public sector electric vehicle fleets across Scotland;
- Allow economies of scale to reduce costs via the procurement of a significant quantity of this emerging vehicle technology; and
- Stimulate increased confidence in the electric vehicle market through public sector leadership.

6.4.4 Using these funds, Aberdeen City Council commissioned APT Technologies to supply and install 10 charging posts. These were installed throughout April across Council sites, including Marischal College, Tullos Depot, Kittybrewster Depot, Spring Garden and West North Street car park. These posts will allow faster charging of the Council's current electric vehicles and will provide the opportunity to charge further vehicles should the Council purchase more.

6.4.5 The emphasis of this scheme is on locating outlets so they can support the fleet of Council owned electric vehicles. However, it is envisaged that this is only the first stage in developing a network of electric charging infrastructure across Aberdeen City that can be accessed by the general public in the future.

6.5 Freight Movement and Efficiency

6.5.1 A study is currently underway to consider how freight distribution within Aberdeen's Air Quality Management Area (AQMA) could be made more efficient, reducing associated air quality and carbon emissions.

6.5.2 The first phase is the analysis of distribution on Union Street and on-street loading surveys have been undertaken with frontages surveyed regarding their loading patterns and requirements. Other work is considering the current legislative context for deliveries on Union Street, current best practice as well as the possible future implications of changes to Union Street.

6.6 Public Awareness and Events

6.6.1 Aberdeen City Council has been awarded Air Quality Action Plan Grant money from the Scottish Government for hosting an air quality awareness raising event in 2012. Given that poor air quality is predominantly caused by transport it is proposed to run a transport

themed event promoting sustainable transport. The date for this event would ideally be Sunday 23rd September as part of European Mobility Week. Further information will be provided on the nature of the event as details emerge.

6.7 CARE North Final Conference, Bremen

6.7.1 Councillor Corall and officers in Planning & Sustainable Development and Economic Development attended the CARE North Final Conference in Bremen on 20th - 21st March. The conference focused on the evolving role of low carbon transport solutions such as cycling, walking and collective transportation, and addressed the importance of behavior change and limiting road space as key elements to meet CO2 reduction targets.

6.7.2 Practitioners and researchers alike benefitted from extremely engaging discussions with a wealth of ideas and suggestions. The CARE North message will be submitted to Rio+20. Aberdeen City Council officers would also like to engage one of the presenters to include Aberdeen in the Cities Transport Review which would benchmark transport in Aberdeen against other cities.

6.8 CARE North Transport and Urban Realm Meeting

6.8.1 An opportunity has arisen for a Councillor or Councillors to attend the CARE North transport and urban realm meeting in Gothenburg which will allow attendees to see how the City has changed the focus of its Centre from an area dominated by traffic to one with a focus on business and movement. The Partner meeting will take place on Thursday 7th and Friday 8th June but the opportunity to extend the meeting either on Wednesday, 6th or to the afternoon of Friday, 8th for a demonstration of the City has arisen. This will be funded via CARE North and Nestrans.

6.8.2 To take advantage of this offer, from a City which is suffering from very similar congestion issues to Aberdeen, and to demonstrate the Council's commitment to and support for the project it is recommended that this Committee approve the attendance of (an) Elected Member(s), along with officers working on the project, at the CARE North transport and urban realm meeting in Gothenburg on 6-8th June 2012.

B) Issues for Information

Active Travel and Air Quality

7 Aberdeen – Blackburn (A96 Corridor) cycle path construction.

Phase 1 Haudagain and Auchmill Road sections.

- 7.1 The construction elements of Phase 1 have been completed and footway users are now benefiting from improved footway conditions and crossing points.
- 7.2 The advertisement and consultation of the necessary Traffic Regulation Orders (TROs) for shared pedestrian and cycle routes are being taken forward by Transport Scotland and Aberdeen City Council. It is anticipated that the consultation and advertisement of the local road sections will be reported to this Committee within the Small Scale Traffic Management and Development Associated Proposals Outcomes Report.
- 7.3 Subject to successful completion of the TROs, funding has been secured for the implementation of the signing and lining required for the orders and to install toucan crossings at either end of the route to aid access for cyclists to the facility.
- 7.4 The on road section of the cycle route along the old Inverurie Road will be implemented in spring / summer 2012.
- 7.5 Officers will also be developing the design and programme of the further phases of the route in order to progress the route out towards Blackburn subject to future funding and developments along the corridor.

8 Cycle Demonstration Project

- 8.1 During March the first wave of new infrastructure as outlined within the Greenbrae Cycle Project Action Plan was installed within the project area, comprising a series of dropped kerbs and a new path through the grounds of Greenbrae School. The advertisement and consultation of the necessary TROs for shared pedestrian and cycle routes has recently taken place and the outcomes will be reported to a future meeting of this Committee within the Small Scale Traffic Management and Development Associated Proposals Outcomes Report.
- 8.2 A report on the progress of this project during 2011/12, and looking at priorities for 2012/13, has been prepared and can be viewed on the Council's website at www.aberdeencity.gov.uk/communitycycling or provided on request.

9 Sustrans Grants for School Travel Projects

- 9.1 Between January and March 2012, two grants were received from Sustrans Scotland for school travel projects, with £19,000 received for

cycle parking facilities and £2350 for 'soft measures' projects. Further details are in the next section.

10 External Funding for Active Travel Projects 2011/12

10.1 Throughout 2011/12 the Council's Transportation Strategy and Programmes Team continued to seek to take advantage of external funding opportunities for active travel projects.

10.2 External funding is vital for some projects, such as school travel initiatives, for which no internal funding source currently exists, and increases the available budget for other projects.

10.3 Below is a breakdown of the funding that was successfully attracted during 2011/12 for active travel projects:

- **Sustrans Community Links Fund - £19,050**, used to match fund the upgrade of two walking and cycling paths, one alongside Persley Walled Gardens (£4,050), which forms part of the National Cycle Network Route 1, and the other a Core Path connecting Heathryfold to Auchmill (£15,000).
- **Sustrans School Cycle Parking Fund - £31,600**, used for the installation of new and / or improved cycle and scooter parking facilities at the following schools: Charleston School, Glashieburn School, Greenbrae School, Mile End School, Cults Primary School (match funded with Nestrans) and Fernielea School, Northfield Academy and Riverbank School, the latter three match funded by the Council's Cycling Walking and Safer Streets (CWSS) allocation from the Scottish Government.
- **Sustrans School Soft Measures Fund - £2,350**. Greenbrae School successfully bid for £850 for a bicycle maintenance project, and Stoneywood School received £1,500 for the purchase of road safety education equipment.
- **Cycling Scotland Bikeability Grant - £1,440**, awarded to aid local authorities in the delivery of Bikeability in schools and currently being used for a project benefitting three primary schools – Kirkhill School, Riverbank School and St. Joseph's RC School.

Also:

- **Nestrans - £3,776.12**, additionally awarded to match fund with Sustrans a new cycle shelter at Cults Primary School and to pay for the repair of damaged cycle lockers at St. Machar Academy.
- **CWSS - £29,079.80** was allocated from the Council's CWSS fund to match-fund cycle parking facilities at Northfield Academy, Fernielea School and Riverbank School and to pay for scooter and cycle parking facilities at the following schools: Charleston School, Culter School, Harlaw Academy, Kingsford School, Kirkhill School and Muirfield School.

11 Give Me Cycle Space Campaign

- 11.1 As reported to this Committee in January, Aberdeen City Council is working with Cycling Scotland to deliver the 'Give Me Cycle Space' campaign in Aberdeen. This is a social marketing campaign to increase awareness amongst drivers of children cycling to school and to encourage drivers to give cyclists plenty of room when passing them on the road.
- 11.2 The campaign is running from 8th May to 15th June, using a mix of media. Billboards, bus stop adverts and lamp post banners are being used on-street, while adverts will be running in the local press, on local radio and on both local and national television.
- 11.3 The on-street advertising is concentrated around seven primary schools (Greenbrae, Culter, Cults, Hazlehead, Airyhall, Fernielea and Kingsford) and pupils at each school are taking part in a number of cycling-related activities to coincide with the campaign, culminating in a visit from The Clan, Scotland's premiere bike stunt team, to two of the schools in June.

12 Bikeability

- 12.1 Bikeability Scotland is the new multi-level cycling proficiency training scheme for Scottish schools. Following the withdrawal of Grampian Police from supporting schools in the delivery of cycle training, the Council has been working with Cycling Scotland, the national cycle promotion organisation, to introduce a new model of delivery for Aberdeen.
- 12.2 The Council's City Wardens team is now taking the lead on this with a number of Wardens recently becoming qualified Cycle Trainers. This will allow them to cascade training down to parents, teachers and volunteers at schools, training them as Cycle Training Assistants. It is hoped that having a local resources in the form of the Wardens, able to train future trainers themselves, will prove a sustainable delivery model for Aberdeen, with the Wardens also on hand to help out directly with the training of the children in schools when needed.

13 Scottish Transport Awards 2012

- 13.1 Aberdeen City Council has been nominated for 5 awards at 2012's Scottish Transport Awards.
- Integrated Transport Project of the Year - Strategic Transport Fund (with NESTRANS, Aberdeenshire Council and Aberdeen City and Shire Strategic Development Planning Authority)
 - Achievements in Cycling – The Greenbrae Cycle Project

- Excellence in Walking and the Public Realm – The Green Streetscape
- Innovative Transport Project of the Year - The Greenbrae Cycle Project
- Innovative Transport Project of the Year - Strategic Transport Fund (with NESTRANS, Aberdeenshire Council and Aberdeen City and Shire Strategic Development Planning Authority)

Winners will be announced during a ceremony in Glasgow on 14th June.

14 Cycle Friendly Employer Award

- 14.1 Aberdeen City Council has been named a 'Cycle Friendly Employer' in recognition of the efforts made to encourage, promote and facilitate cycling to work amongst employees. Cycling Scotland, who administers the award, visited three Council buildings in November last year – Marischal College, Kittybrewster and Balgownie One – and certificates were presented to each of these in March.

Public Transport

15 Aberdeen City Bus Information Strategy

- 15.1 The Aberdeen City Bus Information Strategy was adopted in March 2011. As part of the Council's commitment to ongoing monitoring and review of performance in meeting the aims and objectives of the Strategy, the first annual progress report has been prepared and is available on the Council's website at http://www.aberdeencity.gov.uk/transport_streets/public_transport/put_bus_strategy_results.asp. Alternatively, a paper copy can be provided on request.

16 Night Time Buses

- 16.1 This project is nearing completion with the installation of lit signs in the night bus stops within the City Centre the only outstanding element. Due to difficulties in installation, which has led to an increase in costs, there has been a delay in progressing this but funding has now been secured and it is anticipated that the signs will be installed in the summer.

17 Audit Scotland Update Report on Transport for Health and Social Care

- 17.1 A report was submitted to this Committee in January 2012 advising members of the findings of an Audit Scotland Report on Transport for Health and Social Care and providing an update on the performance of the Council and partner organisations.

- 17.2 The Committee requested officers to report back in May with a detailed action plan setting out how the Council and partner organisations would tackle the areas for development following self assessment and how the Council would meet the recommendations set out by the Audit Scotland report.
- 17.3 At this stage there is still one Health and Transport Action Plan (HTAP) partner organisation to undergo self assessment. This is expected imminently following which all self assessments will be collated and an Action Plan prepared collectively with all HTAP partners. As was previously advised to Committee many of the potential actions cannot be delivered in isolation and the development of the required Action Plan must be undertaken in tandem with the partner organisations and in conjunction with the development of HTAP.
- 17.4 Due to the above, it is not possible to provide the requested action plan to Committee at this cycle. However, following identification of areas for improvements, officers have already started to take steps to address these areas in order to improve the Council's position ahead of a collective Action Plan being drafted. An update on progress made since January 2012 is provided in Appendix E.

Major Projects

18 Aberdeen Western Peripheral Route

- 18.1 An appeal by Mr Walton against the judgement of Lord Tyre ([2011] COSH 131) sitting in the Outer House of the Court of Session was heard before Lords Clarke, Bonomy and Philip sitting in the Inner House of the Court of Session between 13th and 16th December 2011. Their Lordships delivered their judgement on 29th February 2012 dismissing the appeal.
- 18.2 Following the issuing of the judgement, the unsuccessful party has a statutory right of appeal within 42 days to the UK Supreme Court in London, which acts as the final court of appeal for all United Kingdom civil cases. Mr Walton on the 12 April 2012 submitted an appeal to the UK Supreme Court. A hearing is set for the 9th and 10th of July.
- 18.3 At this stage it is difficult to be precise about the impact of the appeal will have on the project timetable, however it is likely that the appeal will delay the project by at least a further 9 to 12 months.

- 18.4 Officers from Aberdeen City and Aberdeenshire Councils and Transport Scotland are currently examining the impact of this further delay to the AWPR programme.

19 Access from the North

- 19.1 Further to the Access from the North - An Integrated Transport Solution report to this Committee on 26 November 2009 which detailed “sustainable transport solutions to improve access to and from the north of the City, supporting national, regional and local policy objectives for modal shift and reduced levels of car use”, and the subsequent Delivery Plan report to this Committee on 18 January 2011 detailing the programme for implementation of these proposals, this report updates members on progress of the Delivery Plan to date.
- 19.2 The Delivery Plan takes into account the impacts of all major infrastructure changes to the north of the city including the Third Don Corridor and Berryden Corridor in order to lock in the benefits to the surrounding networks and communities, and provide best value for the Council.
- 19.3 Minor improvements to the network have been achieved in the last financial year however the targets set in the original programme have not been met due to limited resources, both financial and staff time, being prioritised elsewhere. Endeavours will be made to allocate staff time to the development of the options in order that they are ready to put forward for funding streams this and next financial years.
- 19.4 Progress in project groupings

Denmore Road to Scotstown Road pedestrian improvements

Some improved pedestrian provisions, including dropped kerbs and tactile paving at crossing points, have been made along Denmore Road and Greenbrae Drive.

Pedestrian improvements - King Street and Old Aberdeen

The implementation of improved pedestrian crossing points in Old Aberdeen and along King Street has been completed.

Pedestrian and cycle improvements in Grandholm

Upgrade and extension of existing path network.

Pedestrian and cycle improvements on Great Northern Road

Progress is being made towards the advertising of a Traffic Regulation Order for a shared cycle and pedestrian path along the initial section of the route to tie in with the Bucksburn to Blackburn A96 cycle route. This will also provide a toucan crossing adjacent to Haudagain Roundabout on

Great Northern Road. Footpath surfacing improvements have been implemented near to the roundabout.

Pedestrian and cycle improvements on Hilton Drive / Westburn Drive and Craigie Loanings

Improved pedestrian crossing points along Hilton Drive and at Sixways roundabout have been implemented.

Public transport improvements

A Bus Punctuality Improvement Project (BPIP) study has commenced along Great Northern Road/ Auchmill Road corridor.

No action taken to date:

Cycle provisions on Ellon Road

Cycle improvements to Core Paths 13 and 6

Cycle provisions on King Street

West North Street cycle and pedestrian facilities

Sustainable Transport Hubs

Beach Esplanade cycle facilities

Pedestrian and cycle connections in the vicinity of the Parkway

Cycle Facilities linking to the Berryden Corridor

- 19.5 The planning application for the Third Don Crossing was approved subject to conditions at the meeting of full Council on 23rd February 2011.
- 19.6 A Compulsory Purchase Order (CPO) was promoted in 2010 to acquire the land necessary to build the scheme. As there were objections raised over the use of the CPO, Scottish Ministers called for a Public Local Inquiry (PLI) to be held. An independent Reporter was appointed by Scottish Ministers to carry out the PLI which was held in November 2011.
- 19.7 Following the PLI the Reporter recommended that the CPO be confirmed and passed his findings and recommendations to Scottish Ministers in February 2012. Scottish Ministers considered his report and recommendations and confirmed the CPO in March 2012.
- 19.8 On Wednesday 28th March 2012 confirmed copies of the CPO were issued to all owners and occupiers of affected property as well as being advertised in local press and on street notices. Following this notice, objectors have a 6 week period in which to appeal the Scottish Ministers' decision.
- 19.9 A final decision has still to be made as to whether the scheme is delivered as part of the AWPR or as a stand alone project. Preparation of contract documentation is currently ongoing.

20 A96 Park and Ride / Dyce Drive Link Road

- 20.1 The necessary land has been acquired and planning permission granted for these schemes. Work is now underway to prepare specimen designs for inclusion in the AWPR contract.

21 Haudagain Junction Improvements

- 21.1 Scottish Ministers recently announced £3 million funding to take forward the design of the preferred option during 2013, subject to verification of the preferred option in conjunction with the Cumulative Impact Assessment for transport impacts of the Structure Plan and Aberdeen City Council and Aberdeenshire Council's respective Local Development Plans. The Scottish Government has given commitment to delivery of improvements at the Haudagain after the AWPR is open, committing funding from 2013 for the design process. Officers of this Council and NESTRANS met with Transport Scotland in April to discuss taking this forward and further discussions are planned in the coming months.

22 Berryden Corridor Improvements

- 22.1 The preferred option for this scheme was approved by this Committee in November 2009. The next stage of detailed design, costing and programming has been subject to bids to the Non Housing Capital Programme, unfortunately unsuccessfully due to other Council priorities. In 2011/12 NESTRANS funding supported the development of a footprint of the improvements, to ensure there was a minimum availability of knowledge should any developments progress along the corridor that need to take this transportation project into account. No further design work can be undertaken until such times as significant non housing capital funding becomes available and officers will continue to submit this scheme in future NHC programmes for consideration..

5. IMPACT

The contents of this report link to the Community Plan vision of creating a 'sustainable City with an integrated transport system that is accessible to all'.

All of the projects and strategies referred to in this report will contribute to delivery of the transport aims of Vibrant, Dynamic and Forward Looking – **'Improve Aberdeen's transport infrastructure addressing other pinch points Work to improve public transport encourage cycling and walking'**.

The projects identified in this report will also assist in the delivery of actions identified in the Single Outcome Agreement (SOA), in particular the delivery of both Local and Regional Transport Strategies which will contribute directly and indirectly to 14 out of the 15 National Outcomes described in Aberdeen City Council's 2009/10 SOA.

The Local Transport Strategy (LTS) and Regional Transport Strategy (RTS) from which the transportation schemes within this report are an integral part have been subject to Equalities & Human Rights Impact Assessments.

6. BACKGROUND PAPERS

All background papers are referenced within the main body of the report.

7. REPORT AUTHOR DETAILS

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Appendix A

Fares and Ticketing Strategy for Aberdeen City and Shire

Executive Summary

Aim and Objectives

The overall aim of the fares and ticketing strategy is:

- *to work in partnership with operators to ensure that the travelling public are aware of, and have on offer, fares which represent value for money and ticket options which reflect their travel patterns.*

To enhance the image of the public transport product and, in turn, encourage passenger growth and revenue generation, fares and ticketing products will be developed which:

- *assist 'seamless' ease of travel for passengers thus enhancing the travel experience;*
- *provide perceived value for money and enhance the attractiveness of public transport services; and,*
- *speed passenger boarding and enable efficient service operation.*

Context

Across the Nestrans region there is a multiplicity of ticket types available to the travelling public, offered by a number of different operators to encourage customer loyalty, retain market share and maximise revenue flow. In addition to operator specific ticket types there are currently four multi-operator integrated tickets available: *Aberdeen PLUSBUS; Westhill Bus; Railbus; and Aberdeenshire Connect*. Aberdeen City and Aberdeenshire Councils also have a direct involvement by specifying the requirement to issue and accept certain ticket types on their respective supported bus services.

The current multiplicity of fare levels, fare structures and ticket types can undermine confidence in public transport, particularly amongst irregular or non-users, who are not necessarily aware of some of the best value tickets.

The lack of multi-operator transfer tickets and multi-operator travelcards reduces the attractiveness of public transport both to users and non-users as passengers do not have: the flexibility to board the first bus to their destination where a corridor is served by more than one operator, if they have already purchased a better value single operator multi-journey ticket; the opportunity to avail

themselves of a discounted return ticket if their return trip involves the use of a different operator from the inbound trip; and, the ability in some circumstances to purchase value for money transfer tickets where their journey involves interchange between different operators.

The strategy acknowledges:

- the potential benefits of simplified and integrated public transport ticketing systems, including patronage growth;
- that the nature and extent of the benefits depend upon the ticketing system(s) adopted and the local operational environment; and,
- that smart ticketing infrastructure can facilitate better integration but does not in itself guarantee it, and that the potential benefits of such technological investment will not be fully realised without further integration;

There are also significant constraints that are placed upon both local authorities and operators in promoting multi-operator integrated ticketing arrangements as a consequence of competition legislation and the deregulated public transport market. Current legislation makes no provision for local authorities to set the price of tickets under a multi-operator ticketing scheme.

The strategy is based on the premise that the introduction of integrated tickets by voluntary agreement with bus operators is preferable to the imposition of such tickets through the use of statutory legislation.

Strategy Actions

Actions have been identified that are considered both desirable and deliverable, with the timescale for proposed implementation reflecting the priority and complexity of the issue to be addressed.

The Actions identified are to:

Short Term (0-9 months)

- Set supported bus service fares to broadly reflect prevailing commercial fare levels.
- Specify the sale on supported bus services of discounted child fares, including 'half-fare' single fares.
- Set attractive and simple levels of discount for supported bus service return tickets and multi-journey tickets.
- Incorporate the issue and acceptance of appropriate multi-operator tickets as a condition of all supported bus service contracts.
- Specify the sale on supported bus services of a range of discounted 'family/group' tickets.

- Permit the sale of commercial tickets at commercial fare levels on supported bus service journeys that are provided on predominantly commercial routes.
- Encourage operators to provide commercial multi-operator ticketing products.
- Encourage the sale of a range of discounted 'family/group' tickets on commercial bus services.
- Maintain and/or introduce *Aberdeenshire Connect* multi-operator transfer tickets where passenger and other benefits will ensue.
- Provide an electronic ticket machine (ETM) back office for 'smaller operators'.
- Facilitate a revenue redistribution facility for multi-operator ticketing arrangements.
- Encourage bus and rail operators to continue to develop, and participate in, rail-bus 'add-on' through tickets.
- Encourage operators to provide clear and comprehensive information on fares and ticketing.

Medium Term (9-24 months)

- Where possible, simplify supported bus service fare structures.
- Encourage operators to, where possible, simplify commercial bus service fare structures.
- Consider the options for an *Aberdeenshire Connect plus* ticketing product (i.e. an *Aberdeenshire Connect* multi-operator transfer ticket plus add-on for onward/return travel within Aberdeen City).
- Promote, and encourage operators to participate on a voluntary basis, in a joint ticketing arrangement providing for multi-operator travelcards across the Nestrans region on a zonal basis, with a phased implementation.
- Provide fares and ticketing information on the Councils' corporate web sites.
- Lobby Traveline Scotland to hold and disseminate fares information.
- Promote and publicise multi-operator tickets and single operator discounted ticket products.
- Investigate the options for the introduction of an ITSO compliant interoperable e-purse that could be universally accepted by all bus operators in Aberdeen City and Shire.
- Investigate the options for the introduction of an integrated e-ticket product range.

Long Term (24 months +)

- Introduce an ITSO compliant interoperable e-purse that is universally accepted by all bus operators in Aberdeen City and Shire.
- Introduce an integrated e-ticket product range.

Progress on implementing the Actions and, in turn, the effects of the Fares and Ticketing Strategy will be monitored and reported on an annual basis to the Local Authority Bus Operators Forum and Nestrans Board. The Strategy will be reviewed as appropriate in light of the monitoring process.

Appendix B

NESTRANS REGIONAL PARKING STRATEGY

DRAFT

February 2012

1. Introduction

- 1.1. The Nestrans Regional Transport Strategy (RTS) recognises that parking is a key element of managing demand and that parking policy will be an important element in influencing modal choice and achieving the RTS objectives. The RTS also commits Nestrans to producing a Regional Parking Strategy for the North East.
- 1.2. The aim of a Regional Parking Strategy is to agree a policy framework under which actions can be delivered at a local level that ensure provision, management and control of parking in both the city and shire works towards and supports the wider objectives of the RTS and the two Local Transport Strategies (LTSs).
- 1.3. The way in which parking provision is managed and supplied can have a significant impact on a range of issues and links to a wide range of other policy areas. It can be used to stimulate economic activity or encourage the use of more sustainable modes, thereby reducing congestion and improving air quality. Parking policies can also be used to manage demand and ensure access to services.
- 1.4. The availability of parking and the way in which it is managed can also impact significantly on the performance of other services, particularly public transport services and the performance of the local economy in general.
- 1.5. This Regional Parking Strategy will complement the RTS and LTSs through consideration of the ways in which parking control can influence mode choice, environment and the economic vitality of the region.
- 1.6. This strategy has been informed by a review of national, regional and local policy in relation to parking as well as consultation with the two Councils and consultation with stakeholders at the North East Transport Consultative Forum (NETCF). A study was also carried out by Aecom in early 2011 to quantify some of the issues in relation to Private Non Residential Parking and Business Permits in the region.

2. Key issues and opportunities

Key Issues and opportunities

- 2.1. Parking can play a significant role in maintaining and improving the **economic vitality** of town centres to meet the needs of local businesses by allowing access for customers to shops and services, encouraging a turnover of spaces in the town centre and reducing congestion. There are opportunities to manage parking provision in a way that encourages short stay parking in town centres and long stay parking further out. This issue applies equally to Aberdeen City Centre, Aberdeenshire towns and to other retailing centres such as Torry and Rosemount which also depend on a supply of short stay parking for customers.
- 2.2. **Congestion**, particularly during the peak commuting periods, is a key concern in the centre of Aberdeen and on the key routes into the City as well as, to a lesser extent, parts of Aberdeenshire. Increasing congestion is affecting businesses in and around the city and also the operation of the bus fleet which is increasingly being affected by congestion. This is resulting in longer and more uncertainty in journey times which in turn makes it more difficult to encourage increased bus use. Management of parking through charging mechanisms and encouraging use of the most appropriate car park through signing etc can contribute to reductions in car journeys and to reductions in cross city / cross town car trips. Although congestion is less of an issue in Aberdeenshire towns, management of parking has a role to play in managing traffic in general and encouraging more sustainable modes of travel. Congestion is not however simply a city issue and is caused by and affects both City and Shire residents alike.
- 2.3. The volume of traffic and the level of congestion also have a direct impact on **local air quality**, with levels of nitrogen dioxide and particulate matter from transport of particular concern. Air Quality Management Areas (AQMAs) have been designated in Aberdeen City Centre, on Wellington Road and Anderson Drive and an action plan has been developed for these areas. Parking management and control measures form a key element of the Air Quality Action Plan in order to try and better manage the volume of traffic in the city. Air quality is primarily a problem within the city rather than Aberdeenshire however with the growth in population and housing forecast in the Structure Plan, it is an issue that will need to be monitored into the future. Inverurie for example suffers from congestion at some times of day and air quality is a concern, although not at European standard levels.
- 2.4. The role that parking provision and management plays in supporting the delivery of other strands of the RTS is significant, in particular in encouraging increased use of public transport services (including park and ride), air quality, access to services and economic growth. Although one of a number of influencing factors, there is an opportunity for parking policy to better support delivery in these areas.

Other issues and opportunities of relevance

- 2.5. The extent of **private non-residential parking (PNR)** availability in Aberdeen is a key issue and has been identified numerous times during stakeholder consultation. A study undertaken by Aecom in 2011 estimates that there are over 7,000 PNR spaces within the controlled parking zone, almost 34,000 spaces within industrial areas in Aberdeen and major industrial estates in Westhill and Portlethen and a further 5,000 spaces at other locations in the City Region such as Foresterhill and the Universities. In total there is estimated to be 46,000 PNR spaces in the City Region, almost all of which provide free parking predominantly used by commuters. It is estimated that less than 10% of car commuters across the region pay for their parking. This poses a significant challenge to other policies aimed at encouraging mode shift away from private car to bus, rail and park and ride as most drivers will perceive the cost of driving all the way to work as cheaper than using public transport. This challenge is even more acute in respect of peripheral employment areas, where virtually all drivers have free parking readily available. This is the area that is most difficult to influence.
- 2.6. Due to national policy, the Councils have no powers to impose workplace parking charges (this tool is available in England and Wales), however there are opportunities to engage with employers through the travel planning process encouraging them to develop travel plans and adopt car park management policies that support more sustainable modes of travel such as car sharing and cycling. The two Councils and other public sector partners have control over a significant volume of the region's car parks and there are therefore some opportunities for these organisations to lead by example in their control of private workplace parking.
- 2.7. In areas where demand for private non-residential parking is significantly greater than the supply, this can have a significantly detrimental impact on adjacent areas if there are no or few restrictions on parking e.g. residential areas. This has been evident in the past in the areas surrounding the Aberdeen Royal Infirmary and the Universities where overspill parking has meant local residents are unable to park in the area that they live. In such instances, the City Council has introduced **zones with an element of residents only parking** controlled by a permit system with sections of pay and display in some parts. The City Council LTS states that the price and availability of residents' parking permits will be managed in order to minimise the over-subscription of permits, ensure the fairest possible allocation of permits and favour environmentally-friendly vehicles.
- 2.8. There are also opportunities to review the extent of the controlled parking zone and expand into areas where there are currently no restrictions, where problems associated with long stay commuter parking are identified as impacting detrimentally on local residential and/or local business needs. Aberdeenshire currently has zones in which residents permits apply in the towns of Banchory, Banff, Ellon, Fraserburgh, Huntly, Inverurie, Peterhead, Stonehaven and Turriff.
- 2.9. On-street parking raises different issues in different areas in terms of balancing the needs between residents, businesses, customers, employees and others. In terms of the problems they create, where these relate to road safety and traffic

management issues, these are dealt with by the two Councils. This issue is therefore not covered in any detail in this strategy.

- 2.10. The **comparative cost of car parking** in both Aberdeen City and Shire, compared to bus fares is a key contributing factor to the current challenges faced in trying to encourage mode shift. This is an issue for both parking policy and bus policy to try and improve people's perceptions about the true cost of driving, its impacts and to strike the right balance between providing parking at a price that encourages economic growth whilst also supporting wider policies to encourage mode shift. More detailed work may need to be done to fully examine the economic impacts of increasing parking charges as it has to be remembered that less than 10% of people who drive to work pay for their parking.
- 2.11. The rates charged for on-street **business permits** in Aberdeen City (£200 per year / £17 per month) offer a significantly discounted rate compared to a monthly season permit in an off-street car park (£200/month). The objective of this is to ensure that local businesses are still able to access parking adjacent to their premises, an important factor for those that require access to their car for business purposes during the day. This permit system can be used to park all day in short stay parking spaces at any time of the day and is often used for all day parking in spaces zoned for short stay parking. This is evidenced in the results of the study carried out by Aecom in 2011 which surveyed business permit parking and shows that many key city centre streets, adjacent to the main shopping areas, are frequently used by a majority of cars with business permits. The price of the permits, in relation to the cost of season tickets that can be obtained for private off street car parks, appears to be very cheap and enabling long-stay in areas where short-stay is prioritised seems to be counter-productive. Furthermore, the eligibility criteria do not take account of the size of the business or the amount of car parking spaces that are available to that business. A sample survey of business parking on Queens / Rubislaw Terrace also found that 71% of the cars displaying business permits at 0930 in the morning were still parked there at the end of the day, indicating that a high proportion of these vehicles are not in use during the working day, contrary to the LTS objective of encouraging turnover.
- 2.12. Although **parking and loading restrictions** are in place across the region, limited resources mean that not all areas can be enforced at all times and restrictions are sometimes ignored, even on bus lanes and key routes. Abuse of parking and loading restrictions, particularly in the city centre, can seriously disrupt the flow of traffic with even a small disruption to traffic flow having widespread implications across the whole network. This has the knock-on effect of causing congestion which can result in delays to bus journeys. This in turn makes the bus a less attractive mode of transport and increases operational costs for the operators which often then get passed on to the passenger through increased fares.
- 2.13. The Nestrans **Park and Ride** operation study concludes that one of the strongest messages arising as a result of the study is that successful development of Park and Ride is more challenging without the right supporting policies, particularly in respect of a parking strategy in the city centre (and other destination areas where appropriate) and bus priority measures. Park and Ride works well in locations such as Durham and York because access to, and parking in the city centre is necessarily constrained for physical and geographical reasons. In other places

such as Edinburgh there is comparatively little availability of private non-residential (PNR) parking and public parking is relatively expensive, particularly in comparison with bus travel. The North East currently has none of these features and in recent years has suffered from declining bus and park and ride patronage.

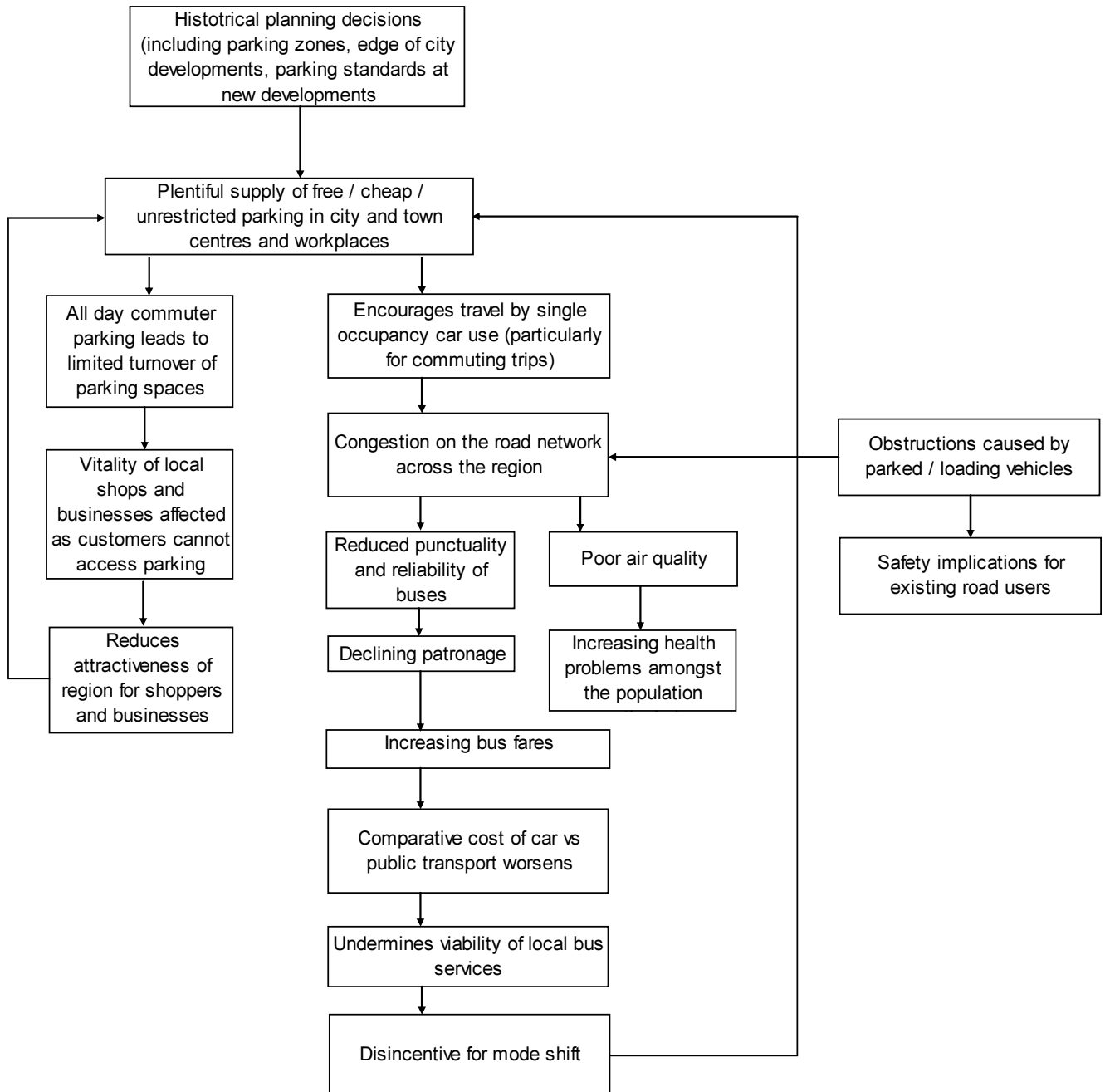
- 2.14. The region's rail stations also provide park and ride opportunities and the opening of Laurencekirk station, with far greater levels of patronage than projected, illustrates the latent demand for rail travel. Although a large proportion of rail passengers should be encouraged to walk and cycle to the rail station, it is important that sufficient parking is also provided in order to make this an attractive option for those that live further away. Parking at some of the eight stations in Aberdeen City and Aberdeenshire is already at capacity. In addition to increasing the supply of parking at rail stations, there are also opportunities to work with ScotRail to influence the way in which the parking is managed. This can mean a number of things and can be achieved through the introduction of time restrictions and / or charges, which could be discounted against rail tickets. This can help to manage the availability of spaces throughout the day but needs to be balanced to ensure people are not put off travelling by train.
- 2.15. **Parking for disabled people** - In March 2010, there were 23,914 Blue Badge holders in the North East¹. This represents some 11% of all cars registered in the region and has risen by approximately 28% since March 2006. The implications of this are that there needs to be appropriate provision and enforcement of parking for disabled people across the region and that this requirement may increase in the future if this trend continues. Aberdeen City Council also operates a Green Badge parking scheme which allows holders to exclusively access certain reserved short stay kerbside spaces in central Aberdeen. These are located as close to key destinations in the city centre as possible in order to minimise walk distances. People eligible for a Green Badge are those who, due to the severity of their disabilities, find it difficult to use other designated city centre parking facilities for people with disabilities. The Green Badge spaces allow for people getting out from a vehicle either by a side ramp or a tail lift, or for the transfer of a driver or a passenger out into a wheelchair. Kerbs have been lowered at every Green Badge location. The Disabled Persons Parking Places (Scotland) Act 2009 now makes it a requirement that all disabled parking bays become legally enforceable. The Councils therefore now have a duty to make all on-street disabled bays and all off-street disabled bays in Council operated car parks enforceable. This is very much a local issue to be addressed by the two councils and is therefore not dealt with further in this strategy.
- 2.16. A **car club** is due to be launched in the City in April 2012. This initiative will see specific car club parking spaces designated in key locations across the city. These spaces have already been agreed and are awaiting implementation once the car club is operational. Car clubs provide the opportunity to have access to a car when you need one, without having to pay for a car when you don't need it. They can help to reduce the number of cars on the road and with every car club car estimated to replace around 10 private cars they can help to cut emissions and help reduce congestion (a study carried out by TRL in 2010/11 showed that 30% of respondents would have bought a car if they had not joined a car club and 61% are

¹ Source: Scottish Transport Statistics 2011

less likely to buy a car in the next few years as a result. It was also shown that, on average, car club members make more frequent use of other modes including bus, walk and cycle).

- 2.17. The most recent transport results from the Aberdeen City Citizens Panel show that 14% of respondents feel that more **cycle parking** at destinations (e.g. shops / work) would encourage them to use an alternative mode of transport. The Aberdeenshire Council Citizens Panel also shows that 52% of respondents travel less than 5 miles for work and 34% travel less than five miles for shopping trips. Although provision of cycle parking now tends to be standard as part of the design of new developments, there are still significant opportunities to enhance cycle parking provision at existing key destinations and workplaces.
- 2.18. Both Councils provide supplementary planning guidance on transport which includes guidance on the **provision of parking at new developments**. Both sets of guidance are being updated as part of supplementary guidance for the Local Development Plans. National guidance on parking at new developments sets maximum standards and local guidance is developed in line with this. The level of parking at new developments, as defined through the planning process will play a significant role in influencing future travel patterns and mode split, particularly on the journey to work.
- 2.19. Discussion with the two Councils has identified that across the region a number of the existing coin based **pay and display machines** will shortly require replacement due to corrosion. Replacement of these machines provides the opportunity to consider new technologies and cashless parking, relying on mobile phone technology. This would minimise collection costs and also provide the opportunity to link to the DVLA database and the potential to consider variation of parking rates dependent on CO2 emissions. This option is currently being explored by Aberdeen City Council to encourage motorists to consider CO2 emissions in future car purchasing, and is reflective of current practice whereby road tax is now charged differentially based on car engine emissions. Such initiatives are intended to influence a cleaner, more efficient national car fleet, which will contribute to improvements in air quality and reducing CO2 emissions.
- 2.20. Car parking charges and the issuing of permits provide a source of **revenue** to the two Councils. The Road Traffic Regulation Act states that parking controls and charges cannot be used as a revenue raising measure and that there must always be a valid traffic regulation reason for applying charges. The income that is generated can be used to ensure the ongoing operation, enforcement, maintenance and future upgrades of the system.

Figure 1: Cause and effect of parking related issues



3. Objectives

- 3.1. Following a review of the relevant policy documents and identification of the key issues and opportunities, two key overarching strategic objectives have been identified. These reflect the role that a regional parking strategy can play in guiding parking policy across the North East, particularly given that the powers to set parking standards and charges lie with the two Councils.
- 3.2. These objectives are in line with the overarching objectives of the Regional Transport Strategy.

- 1. To support the economic vitality of the city and town centres and the wider objectives of the Regional Transport Strategy through a balanced approach to the management of car parking.**

- a. **Indicator:** Length of stay and charging criteria in city and town centre locations compared to locations further away from the centre.

- b. **Indicator:** the number of publicly available parking spaces available in the city centre, smaller retailing centres and Aberdeenshire town centres relative to their size and role as a service centre.

- 2. To support and influence increases in the proportion of journeys undertaken by sustainable modes, particularly by bus and rail.**

- a. **Indicator:** Proportion of journeys undertaken by sustainable modes compared to private car, split by mode.

4. Policies and actions

- 4.1. The role of the Nestrans Parking Strategy is to set the high level policy for parking across the region. As the Nestrans region is such a large and diverse area covering Aberdeen City, Aberdeenshire towns, villages and rural areas, management and control of parking needs to play a different role in different areas.
- 4.2. The delivery and management of parking also falls primarily to the two Councils and to private car park operators. This strategy therefore aims to set out the wider objectives and strategic direction for the region as a whole, rather than set out the specific mechanisms for delivery at a local level.

Turnover of spaces

- a) **Support local parking policies and controls which encourage a high turnover of spaces in city, town and local centres where there is a concentration of local services.**

Different parts of the region have different requirements for parking provision and control. Although the setting of parking controls and charges is very much a local decision and something that varies depending on the location, the following principles should be applied across the region:

- i. Encourage short stay parking in city, town and local centre locations i.e. through maximum stay of 1-2 hours in on-street and off-street spaces in central areas and / or appropriate charging regimes that discourage all day commuter parking;
- ii. Such centres should include Aberdeen City Centre, Aberdeenshire town centres and local centres such as Rosemount and Torry.
- iii. Encourage long stay parking to use areas outwith these central areas and in off-street car parks through the use of appropriate charges and time restrictions.
- iv. Length of stay and charging restrictions should reflect the desire to discourage long-stay parking in areas that rely on a high turnover of spaces to allow customers access to services.
- v. The ongoing revenue requirements for the maintenance and effective enforcement of parking restrictions should be fully considered from the outset to ensure their continued effectiveness. Consistent enforcement of any restrictions in place is essential if they are to be effective.

Nestrans and the two Councils will work together to review current parking zones and restrictions across the region. Although these principles already apply in many areas, changes in recent years may mean that the current zones and restrictions may no longer be appropriate.

- b) The provision and management of parking should support a thriving evening economy in appropriate areas whilst balancing the needs of local residents.**

Visitors and customers of evening and night time services in the city and town centres should be provided with a choice in how they access these services and with a safe and reliable route home. This should include access by car along with public transport provision. Supply and management of parking for evening and night time users should have consideration to the perceived and actual safety of off-street car parks and also the requirements of local residents to park. Allowing local residents to use off-street car parks in the evenings and overnight, allowing a turnover of on-street spaces adjacent to popular trip attractors is supported by this strategy.

Charges

- c) Parking charges should reflect the need to support economic vitality but also be set in the context of the costs associated with using other modes of transport in order that it does not dis-incentivise use of more sustainable modes.**

As well as being a mechanism for managing the length of stay, and therefore turnover, of spaces (as set out in policy a above) parking charges should support both objectives of this strategy and form part of the package of wider RTS measures aimed at encouraging a shift towards more sustainable modes of transport. This means consideration of relevant local bus fares, park and ride charges and rail fares in comparison to parking charges. The focus should be on encouraging commuters and long-stay parkers to use public transport leaving town and city centre spaces available for short stay customers and service users.

In Aberdeen City the scale of PNR and the number of parking spaces under the control of private car park operators means that the influence of the Council on parking charges is limited to Council owned off-street car parks and on-street parking. This does add up to a significant proportion of parking spaces but the comparisons between private off-street and Council parking charges needs to be borne in mind. There are opportunities for the Council to influence the parking charges and restrictions in any new car parks developed through legal agreements as part of the planning process. For example, a requirement could be attached to the operator of a new car park that ensures parking charges are no lower than those charged in Council car parks. This should be considered as appropriate in relation to planning applications for any new private car parks in the central area of Aberdeen.

Aberdeenshire Council currently operate parking charges in Banchory, Banff, Crathie, Ellon, Fraserburgh, Huntly, Inverurie, Peterhead, Stonehaven and Turriff. The regional parking strategy supports a review of parking charges within Aberdeenshire towns, particularly the balance between free and pay and display car park provision and the locations where charges apply to ensure that they remain relevant in the longer term and meet the objectives set by this strategy. This strategy also supports the consideration of parking charges in relation to other key towns in Aberdeenshire where it is felt that turnover of spaces is

restricted and there are opportunities to encourage mode shift for local journeys. A review of parking provision in Aberdeenshire towns will be carried out at a local level and is an action contained within Aberdeenshire Council's Local Transport Strategy.

Permits

- d) Support a review of the current business permit policies currently operating in Aberdeen City.**

As part of the work carried out by Aecom in early 2011 to support the development of this strategy, a sample survey of business permit use was undertaken on Queens / Rubislaw Terrace, just outside the city centre. The survey demonstrated that parking in this location is dominated by business permits and the majority of those cars displaying business permits (71%) were parked there all day. Anecdotal evidence elsewhere supports this evidence that the current system enables all day commuter parking in areas adjacent to the city centre that are zoned for short stay parking. This works against policies that seek to promote accessible short stay parking adjacent to the city centre, and seeks to limit long stay commuter parking in the city centre.

This strategy supports a review of this system to better align it with policy a) above. Any review should have regard for the resource implications of a more complicated scheme as well as the objectives of this strategy.

Enforcement

- e) Support on-going enforcement measures as well as the identification and targeting of specific problem areas which impact on the flow of traffic and / or cause a safety concern.**

In order for policies a) to d) above to be effective in achieving the objectives of this strategy, enforcement of the parking restrictions implemented is essential. Parking is decriminalised in Aberdeen City and is enforced by the City Wardens team for both on and off-street parking and road markings. In Aberdeenshire, parking is enforced through the use of parking operatives using excess charge notices in off-street car parks and by the police for on-street parking. Enforcement is a resource intensive task and the frequency with which areas are enforced and the penalty charges set need to present a sufficient deterrent to non-compliance. Enforcement needs to cover off-street and on-street parking and also other parking restrictions including double yellow lines, bus lane waiting and loading restrictions and disabled parking bays / use of blue badges.

As well as being dangerous, illegal parking can cause significant congestion in Aberdeen City and in Aberdeenshire towns which has knock-on effects for the reliability and punctuality of bus services general traffic. As well as the continuation of existing enforcement measures, this strategy supports the identification and targeting of specific areas where illegal parking and loading causes particular congestion issues. One such area is Market Street in Aberdeen where video surveys in 2010 showed significant parking and loading

occurring during the restricted periods. Problems in this area have been shown to impact significantly on congestion within the city centre.

Infrastructure

- f) **The two Councils and Nestrans should work together to achieve consistency in the exploration and application of new parking technologies and infrastructure e.g. automatic number plate recognition and mobile phone payments.**

There are benefits to ensuring a consistent approach, where possible, across the region in terms of the methods available for payment of parking. Investment in new technologies and easier ways to pay for parking (such as via mobile phone) will contribute to making the city and town centres more attractive as destinations. There may also be benefits and cost savings to be achieved when looking to upgrade and improve parking infrastructure such as ticketing machines and these should be realised where possible. Both Aberdeen City and Aberdeenshire Councils have or are currently trialling cashless parking systems and this strategy supports a co-ordinated approach to this to ensure maximum benefits for the region as a whole.

Park and Ride

- g) **Facilities for bus and rail park and ride across the region will continue to be developed and promoted.**

Park and Ride is one of the key transport tools available to address increasing car use and traffic levels whilst maintaining accessibility, promoting economic vitality and at the same time reducing congestion within the city centre. Both bus and rail park and ride opportunities are currently available in the North East with parking at all rail stations and bus park and ride sites at Kingswells, Bridge of Don and Ellon. Two new bus park and ride sites are also planned on the A96 and A90(S) corridors. Parking controls are one of the most effective measures that can be used to encourage the use of park and ride through discouraging long-stay commuter parking in the city centre. Parking controls and enforcement, which are the responsibility of the two Councils, are therefore key to ensuring the success of regional park and ride facilities.

Currently there are no restrictions on parking at park and ride sites and the charge for use is made on the bus. This method of operation will be reviewed in the light of new developments coming forward adjacent to existing park and ride sites and the development of new park and ride sites in order to ensure that the use of these sites is limited to those using park and ride services and that they are not used as alternative parking for adjacent developments. The operation of the network of existing and new park and ride sites will be reviewed as a whole as well as consideration of the requirements at individual sites.

In relation to capacity of park and ride sites, a recent study commissioned by Transport Scotland considered the effects of parking supply on public transport demand and parking supply, quality and pricing at both rail and bus park and ride sites.

With regard to parking at rail stations, the study concludes that if parking availability is increased, the level of new rail demand is relatively small and the subsequent change in car distances travelled is negligible. As a result, the case for delivering additional car parking spaces at stations must be linked to other objectives, for example addressing specific car parking capacity constraints that may have arisen as a result of rail service improvements. Such rail service improvements have occurred in the North East in recent years resulting in significant patronage growth, particularly at Dyce and Inverurie. Further work to explore in more detail the capacity constraints at station car parks across the region would be beneficial in order to better understand the requirement for additional capacity. Nestrans will explore this issue in partnership with the two Councils.

The same study also looked at bus based park and ride and included surveys at Bridge of Don park and ride site. The study concludes that while there is evidence to suggest bus based park and ride can help to reduce the overall number of car trips, existing schemes in Scotland are relatively under-performing compared with other examples. The linkages with wider transport policy need to be strengthened to help attract a higher number of motorists to use the site. This includes linkages to the cost of city centre parking, the cost of bus fares, tickets available and means of purchase and journey times.

Nestrans will continue to promote both bus and rail park and ride through a variety of channels, including the Getabout brand, and will work with Aberdeen City Council to better integrate parking policy with the success of park and ride. The development of two new park and ride sites on the A96 and A90(S) provides a significant opportunity to consider the operation of the bus park and ride network as a whole including how the car parks are managed. Nestrans will continue to work with both Councils to identify the optimal method of operation of the bus based park and ride network and on actions to increase use.

Planning policy and development management

- h) National guidance on maximum parking standards should be applied to all new developments and the introduction of car free or low car developments supported where appropriate.**

Maximum parking standards are set out in national planning guidance and are incorporated into the Local Transport Strategies and supplementary planning guidance of the two Councils. In addition to the application of these standards, this strategy supports, where appropriate, the introduction of car free developments or developments that propose lower than maximum standards to facilitate mode shift through provision of alternative measures. Such alternative measures to be considered should include contributions by developers, where appropriate, to supporting car clubs as part of the planning agreement and the provision of car share parking bays.

- i) Input will be sought at an early stage in the development of masterplans and supplementary guidance on strategic issues relating to parking and travel planning issues.**

Car parking availability is a key determinant of modal choice and there is a need to ensure that the impacts of parking provision are considered at an early stage in the planning process. Nestrans will provide a strategic, regional perspective to ensure alignment with the objectives and principles of this strategy. In order to fulfil this role, the local authorities should consult with Nestrans on masterplans and on planning applications which propose to exceed the national parking standards.

It is important to also ensure that complementary travel planning is considered at an early stage in the planning process and that implementation of travel plans is monitored. This will be required to be done at a local level by the Councils however the regionally available Travel Plan Builder and I-Trace software provides the tools to aid the standardisation and monitoring of travel plans.

Equal consideration should also be given to the provision of parking for bicycles and motorcycles and other parking policies such as car share bays at new developments and more efficient use of available parking in order to encourage reductions in single occupancy car trips.

Travel Planning

j) Employers and groups of employers should be supported to encourage development, implementation and monitoring of travel plans.

The establishment of travel plan networks where groups of employers in one area can work together, for example by jointly subsidising bus services for staff or through car share databases, should be encouraged. The South Aberdeen Travel Plan Network is an example of this which is currently developing a travel plan for the Altens / Tullos area of the city.

Awareness raising amongst employers of the cost of parking provision compared to subsidising more sustainable forms of travel should also be encouraged. Nestrans will continue to actively engage with employers on this issue through a range of channels including the Getabout brand and will encourage and assist in the set up of other travel plan networks, in partnership with the two councils.

k) Support the set up of car clubs in the North East

Car clubs provide a range of benefits, both for individual members and for the wider community. Car clubs reduce the number of cars on the road and it is estimated that every club car replaces at least ten private cars², therefore helping to reduce demand for parking, cut congestion and reduce air pollution. Aberdeen City Council is already in the process of introducing a car club in the city and have allocated 20 dedicated spaces across the city centre with funding from Nestrans towards lining and signing of the parking bays.

Although this scheme is initially limited to the city centre, this strategy supports the expansion to other parts of the city should it be successful. The benefits in

² Atkins & The University of Aberdeen (2009) "Mitigating Transport's Climate Impact in Scotland" suggests that one car club car replaces 23 cars as 14 cars are sold and 9 not purchased.

terms of the potential to cut emissions, reduce congestion, alleviate parking problems and increase accessibility means that it will meet a range of local and regional objectives. Although historically successful in densely populated urban areas, there may be benefits to introducing car club facilities in the more populated parts of Aberdeenshire such as Inverurie and Stonehaven (where there are also large Council offices) and also in areas with lower car ownership. If such a scheme was considered viable in Aberdeenshire, this strategy would support a regional approach to this and extension of the scheme operating in the city. This would ensure that residents signed up to a scheme in each local authority area could benefit from cars available across the region.

l) Encourage partners and public sector organisations to lead by example in implementing car parking policies for their own staff which contribute to the aims and objectives of this strategy.

Aberdeen City Council, Aberdeenshire Council and other public sector partner organisations need to recognise the role that they have to play as significant employers in the region and manage their own staff car parks in ways that contribute to the objectives of this strategy. The public sector needs to lead by example in terms of travel plans, carbon reduction targets and corporate responsibility.

Reducing carbon emissions and improving air quality

m) Support work to consider the benefits and feasibility of applying differential parking charges to reflect the impact of larger and more polluting vehicles.

The City Council is investigating the potential for introducing emissions based parking charges, and this policy aims to support the objectives of this and wider objectives in relation to local air quality and carbon emissions across the region as a whole. Aberdeenshire Council will also be reviewing their car parking charging regime which will include consideration of this issue.

There are significant opportunities to encourage the take up and use of low emission vehicles through parking management and incentives such as discounted parking charges. In the City, this policy is supported through the City Council's Air Quality Action Plan 2011 which identifies that car parking policies have the potential to have a significant air quality impact.

Summary of actions and delivery timescales

The parking strategy will be implemented over a 3 year timescale following its approval and will be reviewed after this time to ensure the policies and actions remain valid and appropriate. The indicators will be monitored on an annual basis as part of the Nestrans RTS monitoring report in order to monitor progress in meeting the objectives.

Policy	Action	Responsibility	Timeframe
a)	Review the current parking zones and restrictions in the region to ensure they remain appropriate.	Nestrans, Aberdeen City & Aberdeenshire	Years 1-2
b)	Consider allowing local residents to use off-street car parks in the evenings and overnight, allowing a turnover of on-street spaces adjacent to popular trip attractors.	Aberdeen City Council	Year 1
c)	Consider the use of planning conditions to influence parking charges in any new private car parks in the centre of Aberdeen.	Aberdeen City	Year 1
c)	Review parking provision in Aberdeenshire towns on a town by town basis.	Aberdeenshire	Years 1-3
d)	Review the business permit policy in the city.	Aberdeen City	Year 1
e)	Identify areas where illegal parking and loading causes particular congestion issues with the intention that enforcement can be targeted to address them.	Nestrans, Aberdeen City & Aberdeenshire	Ongoing
f)	Work in partnership to achieve consistency in the exploration and application of new parking technologies and infrastructure.	Nestrans, Aberdeen City & Aberdeenshire	Ongoing
g)	Explore in more detail the capacity constraints at station car parks across the region in order to better understand the requirement for additional capacity.	Nestrans	Year 2
g)	Strengthen the policy linkages between park and ride and the cost of city centre parking, the cost of bus fares, tickets available and means of purchase and journey times in order to increase useage.	Nestrans, Aberdeen City & Aberdeenshire	Years 2-3
g)	Review the operation of park and ride car parks.	Nestrans, Aberdeen City and Aberdeenshire	Years 1-2
g)	Continue to promote bus and rail park and ride through a variety of channels, including Getabout.	Nestrans, Aberdeen City & Aberdeenshire	Ongoing
h)	Apply national guidance on maximum parking	Aberdeen City &	Ongoing

Policy	Action	Responsibility	Timeframe
	standards to all new developments and the introduction of car free or low car developments where appropriate.	Aberdeenshire	
i)	Local authorities to consult with Nestrans on masterplans and on planning applications which propose to exceed the national parking standards.	Aberdeen City & Aberdeenshire	Ongoing
j)	Actively engage with employers on the issue of travel planning through a range of channels including the Getabout brand and encourage and assist in the set up of travel plan networks.	Nestrans, Aberdeen City & Aberdeenshire	Ongoing
k)	Support the set up of car clubs in the North East.	Nestrans	Year 1
l)	Lead by example in terms of travel plans, carbon reduction targets and corporate responsibility.	Nestrans, Aberdeen City & Aberdeenshire	Ongoing
m)	Consider the benefits and feasibility of applying differential parking charges to reflect the impact of larger and more polluting vehicles.	Aberdeen City & Aberdeenshire	Years 1-2

Appendix C

Bridge of Don Park and Ride - Full Option Assessment

All elements to be valued on a scale of 3 to -3 (i.e. 7 point scale).

		Option 1 - Do Nothing
Transport planning Objectives	Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.	The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.
	Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes	The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes. The loss of the P&R provision along the A90 (N) trunk road corridor does not support either of these strategies, in fact it works in direct opposition to them.
	Deliver overall environmental benefits	The loss of this provision reduces the sustainable transport options for the existing users and any future, potential users. This adds to congestion on the existing network.
		-3
		-1

	Reduce negative environmental impacts to a minimal level when developing infrastructure, including provision of appropriate mitigation measures	0	As the existing car park infrastructure will remain in place until the site is required to be redeveloped, the impacts of this option are neutral.
	Environment		
	Safety	-1	The potential for increased volumes of traffic on an already congested network will have safety implications for road users. Also driver mileage increases leading to a greater likelihood of accidents.
	Security	0	The site is currently serviced by lighting and CCTV. Height restrictive barriers have been installed to prevent access to large vehicles. These provisions will no longer be required if the service is abandoned.
	Accessibility to sustainable modes	-2	Work is required to improve access for sustainable modes directly from Ellon Road. At present all those entering the site must use Parkway East. A foot/cycle path from Ellon Road would be achievable and would increase access however the cost/benefit of providing infrastructure for a limited period would not present best value to the Council and funding is unlikely to be awarded. Loss of the No. 40 bus service would result in reduced sustainable options for all existing park and ride users and workers in the adjacent Business Park.
	Integration with the transport network	0	No impact
	Engineering / construction difficulty	0	No impact

	Public acceptability of options	-1	The loss of an existing service to customers and the negative impact on the surrounding network from the subsequent increase in vehicle numbers will likely receive a negative public response
	Feasibility and affordability	0	There are low cost implications from this option. Possible costs would include decommissioning site and customer cabin. Temporarily increasing access by active transport modes. Increased wear and tear on the remaining road network due to increased volumes of traffic using network.
	Support of economic aspirations	-1	The continued use of the existing site may be restrictive to the future development opportunities for the AECC site as a whole. A tendering process will be progressed over the next couple of years to procure a development partner to work with the Council on the maximisation of the site's value. In the event of the site being redeveloped the facility will be lost. This will result in increased vehicle journeys on the network in creating congestion on a busy network, increasing costs to business and not meeting the aspirational needs of the network.
	Impact of AWPR on proposals	-1	The AWPR reduces the volume of traffic on the network in the vicinity of the existing Park and Ride site. The loss of the car park will result in vehicles returning to the network and reducing the positive impact of the AWPR.

Transport planning Objectives	Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.	1	<p>Option 2a - Do Minimum</p> <p>The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.</p> <p>The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes. The maintenance of the existing volume of park and ride provision promotes the use of sustainable modes however it does not met the aspiration to expand these.</p> <p>This option maintains the existing level of Park and Ride provision and therefore has a neutral impact. There is potential to improve access for active modes to the site further reducing congestion on the existing network.</p> <p>As the existing car park infrastructure will remain in place until the site is required to be redeveloped, the impacts of this option are neutral.</p>
Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.	Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes	0	Deliver overall environmental benefits
Reduce negative environmental impacts to a minimal level when developing		0	

	infrastructure, including provision of appropriate mitigation measures		
	Environment		
	Safety	0	No impact. Though improved access for active modes may provide some increased safety for users.
	Security	0	The site is currently serviced by lighting and CCTV. Height restrictive barriers have been installed to prevent access to large vehicles.
	Accessibility to sustainable modes	1	Work is required to improve access for sustainable modes directly from Eillon Road. At present all those entering the site must use Parkway East. A foot/cycle path from Eillon Road would be achievable and would increase access.
	Integration with the transport network	0	No impact
	Engineering / construction difficulty	0	No impact
	Public acceptability of options	1	The maintenance of the existing level of service would be viewed favourably by existing users and would have no detrimental effect on the existing network.
	Feasibility and affordability	1	There are low cost implications from this option. Increasing access by active transport modes will incur minimal infrastructure costs.
	Support of economic aspirations	0	The continued use of the existing site may be restrictive to the future development opportunities for the AECG site as a whole. A tendering process will be progressed over the next couple of years to procure a development partner to work with the Council on the maximisation of the site's value. The maintenance of the facility will continue to provide benefits to the existing network and reduce delays on the congested network.

	<p>This option itself does not provide for an increase in provision and therefore has a neutral impact.</p> <p>The AWPR reduces the volume of traffic on the network in the vicinity of the existing Park and Ride site. The AWPR provides greater linkage around Aberdeen however the distance from the existing site to the new AWPR junction may not lead to a great attraction from surrounding areas to the car park.</p>
Impact of AWPR on proposals	0

Transport planning Objectives	<p>Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.</p> <p>Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes</p> <p>Deliver overall environmental benefits</p> <p>Reduce negative environmental</p>	<p>1</p>	<p>Option 2b - Do Minimum with multi storey</p> <p>The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.</p> <p>The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes. The maintenance of the existing volume of park and ride provision promotes the use of sustainable modes however it does not met the aspiration to expand these.</p> <p>This option maintains the existing level of Park and Ride provision and therefore has a neutral impact. There is potential to improve access for active modes to the site further reducing congestion on the existing network. The construction of new multi storey infrastructure will have a negative impact on some environmental aspects however construction mitigation will minimise these as the site has previously been developed.</p> <p>The construction of a multi storey car park will have some negative environmental impacts however these will be minimised through</p>	
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	impacts to a minimal level when developing infrastructure, including provision of appropriate mitigation measures		construction mitigation and the site has previously been developed and is set within an urban/ industrial environment.
	Environment		
	Safety	0	No impact. Though improved access for active modes may provide some increased safety for users.
	Security	0	The site is currently serviced by lighting and CCTV. Height restrictive barriers have been installed to prevent access to large vehicles.
	Accessibility to sustainable modes	1	Work is required to improve access for sustainable modes directly from Eillon Road. At present all those entering the site must use Parkway East. A foot/cycle path from Eillon Road would be achievable and would increase access.
	Integration with the transport network	0	No impact
	Engineering / construction difficulty	0	No impact
	Public acceptability of options	1	The maintenance of the existing level of service would be viewed favourably by existing users and would have no detrimental effect on congestion on the existing road network.
	Feasibility and affordability	-2	The cost of constructing a multi storey car park within the existing site boundary has significant cost implications and would maintain not increase on the existing provision therefore may be difficult to justify as a cost/ benefit ratio. Increasing access by active transport modes will incur minimal infrastructure costs.
	Support of economic	0	The continued use of the existing site may be restrictive to the future

	aspirations		<p>development opportunities for the AECC site as a whole. A tendering process will be progressed over the next couple of years to procure a development partner to work with the Council on the maximisation of the site's value.</p> <p>The maintenance of the facility will continue to provide benefits to the existing network and reduce delays on the congested network. This option itself does not provide for an increase in provision and therefore has a neutral impact.</p>
	Impact of AWPR on proposals	0	<p>The AWPR reduces the volume of traffic on the network in the vicinity of the existing Park and Ride site. The AWPR provides greater linkage around Aberdeen however the distance from the existing site to the new AWPR junction may not lead to a great attraction from surrounding areas to the car park.</p>

Transport planning Objectives	<p>Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.</p> <p>Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes</p> <p>Deliver overall environmental benefits</p> <p>Reduce negative environmental impacts to a minimal level when developing</p>	<p>1</p> <p>1</p> <p>1</p> <p>-1</p>	<p>Option 3 - Blackdog The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.</p> <p>The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes. The maintenance of the existing volume of park and ride provision promotes the use of sustainable modes however it does not met the aspiration to expand these.</p> <p>This option maintains the existing level of Park and Ride provision however it reduces the total distance travelled by road traffic creating environmental benefits in terms of reduced fuel consumption and congestion.</p> <p>This site will be constructed on land currently used for agriculture but zoned for development. Mitigation will be put in place to prevent environmental damage e.g. proposed use of suds and landscaping.</p>
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infrastructure, including provision of appropriate mitigation measures			
Environment			
Safety	1		Reduced vehicle miles may increase driver safety. Facilities within the car park would be expected to include lighting. The multiple use of the site i.e. the adjacent hotel and foodhall, will provide greater footfall in the area and greater perceived levels of safety for car park users.
Security	1		The site would be expected to be serviced by lighting and CCTV. Extra security would be provided by the use of the site 24 hours a day i.e. adjacent to hotel.
Accessibility to sustainable modes	1		Blackdog is remote from other settlements though the core path network does come in close proximity to the site and there are residential properties in walking/ cycling distance. Bus services are available from the nearby A90 (N).
Integration with the transport network	2		The site is proposed close to a new junction onto the proposed Aberdeen Western Peripheral Route and will therefore have radial links round Aberdeen, to other park and ride sites as well as access to the A90 (N) trunk road network. The site is in close proximity to the core path network.
Engineering / construction difficulty	-2		The site is proposed to be constructed by a developer in tandem with a foodhall and hotel. The site will only be constructed if the AWPR goes ahead and the junction is constructed as currently proposed. At this time legal challenges are causing delays to the delivery of the AWPR and therefore the construction timescale for this site is undetermined.
Public acceptability of options	0/-1		This site could receive positive feedback from the public as it is a multi use site and the only option to provide users with a dual purpose for using the site.

			<p>It is unlikely that Bridge of Don users would travel to Blackdog to use the service as they currently do at the existing site (approx. 30% of users) therefore some existing customers would be lost. It may also cause conflict with the existing customer base at Ellon Park and Ride if there was felt to be a perceived benefit to users to remain in their cars to Blackdog rather than park in Ellon.</p>
	Feasibility and affordability	1	<p>Whilst there have only been very informal discussions carried out to date with the developer it is anticipated that the construction works would be carried out at their expense and terms would be required to ensure the use of the site for park and ride is preserved and affordable.</p>
	Support of economic aspirations	0	<p>This option alone maintains the existing level of facility and will therefore continue to provide benefits to the existing network and reduce delays on the congested network. This option itself does not provide for an increase in provision and therefore has a neutral impact.</p>
	Impact of AWPR on proposals	-2	<p>This option is dependent on the delivery of the AWPR and will not be able to proceed until the new junction is in place.</p>

Transport planning Objectives	Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.		Option 4a - Berryhill/ Cloverhill with 250 spaces The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.
Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes	-1	The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes. This option does maintain some level of park and ride provision but at less than half of the existing provision. This does not build on the success of the existing site or provide for future expansion.	
Deliver overall environmental benefits	-1	This option does not reduce the volume of traffic on the existing road network and reduces the potential for increasing further P&R use.	
Reduce negative environmental impacts to a minimal level when developing infrastructure,	-1	This site will be constructed on land currently used for agriculture but zoned for development. Mitigation will be put in place to prevent environmental damage e.g. proposed use of suds and landscaping.	

	including provision of appropriate mitigation measures		
	Environment		
	Safety	0	Facilities within the car park would be expected to include lighting. The low volume of parking would suggest limited footfall throughout the day and lower perceived safety.
	Security	0	The site would be expected to be serviced by lighting and CCTV.
	Accessibility to sustainable modes	1	Murcar is relatively close to Bridge of Don settlements with the core path network in close proximity to the site and residential properties in walking/ cycling distance. Bus services are available from the nearby A90 (N).
	Integration with the transport network	2	The site would have access to the A90 (N) trunk road network from the Murcar Roundabout and potentially through the Business and Energy Park to Exhibition Road. The site is in close proximity to the core path network.
	Engineering / construction difficulty	-2	The site is proposed as part of the development to expand the existing Business and Energy Park. A long access road is required to the first phases of the Park and significant changes would be required to adjust the existing round about to accommodate the change of flows at the junction.
	Public acceptability of options	1	Bridge of Don users may be willing to travel to Murcar to use the service as they currently do at the existing site (approx. 30% of users) though some existing customers may be lost. The opportunity to reach the site before reaching the queues at the Parkway / Eilon Road Roundabout may also appeal to users.
	Feasibility and affordability	-2	Whilst there have only been very informal discussions carried out to date with the developer it has been suggested a significant contribution to the construction works of the site or the access road would be required in

			order that the site provide some value to the developer. Further discussions would be required to ascertain the extent of these potential costs either initial capital or long term revenue.
	Support of economic aspirations	-1	This option itself leads to a loss in provision and therefore has a negative impact on the potential network benefits.
	Impact of AWPR on proposals	0	This option is independent on the delivery of the AWPR however it would be reasonably close to the new junction and may provide links radially to other park and ride sites.

Transport planning Objectives	Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.		<p>Option 4b - Berryhill/ Cloverhill with 500 spaces</p> <p>The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.</p>
Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes	0	0	<p>The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes.</p> <p>This option maintains the existing level of park and ride provision but does not build on the success of the existing site or provide for future expansion.</p>
Deliver overall environmental benefits	0	0	<p>This option maintains the benefits to the existing road network of the current site and the vehicle distances travelled at present.</p>
Reduce negative environmental impacts to a minimal level when developing infrastructure,	-1	-1	<p>This site will be constructed on land currently used for agriculture but zoned for development. Mitigation will be put in place to prevent environmental damage e.g. proposed use of suds and landscaping.</p>

	including provision of appropriate mitigation measures		
	Environment		
	Safety	0	Facilities within the car park would be expected to include lighting.
	Security	0	The site would be expected to be serviced by lighting and CCTV.
	Accessibility to sustainable modes	1	Murcar is relatively close to Bridge of Don settlements with the core path network in close proximity to the site and residential properties in walking/ cycling distance. Bus services are available from the nearby A90 (N).
	Integration with the transport network	2	The site would have access to the A90 (N) trunk road network from the Murcar Roundabout and potentially through the Business and Energy Park to Exhibition Road. The site is in close proximity to the core path network.
	Engineering / construction difficulty	-2	The site is proposed as part of the development to expand the existing Business and Energy Park. A long access road is required to the first phases of the Park and significant changes would be required to adjust the existing round about to accommodate the change of flows at the junction.
	Public acceptability of options	1	Bridge of Don users may be willing to travel to Berryhill to use the service as they currently do at the existing site (approx. 30% of users) though some existing customers may be lost. The opportunity to reach the site before reaching the queues at the Parkway / Eilon Road Roundabout may also appeal to users.
	Feasibility and affordability	-2	Whilst there have only been very informal discussions carried out to date with the developer it has been suggested a significant contribution to the construction works of the site or the access road would be required in order that the site provide some value to the developer. Further discussions would be required to ascertain the extent of these potential

			costs either initial capital or long term revenue.
	Support of economic aspirations	1	This option itself maintains the existing level of park and ride provision and therefore maintains the existing network benefits.
	Impact of AWPR on proposals	0	This option is independent of the delivery of the AWPR however it would be reasonably close to the new junction and may provide links radially to other park and ride sites.

Transport planning Objectives	<p>Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.</p> <p>Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes</p> <p>Deliver overall environmental benefits</p> <p>Reduce negative environmental impacts to a minimal level when developing infrastructure,</p>	2	<p>Option 5 - Satellite Sites</p> <p>The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.</p> <p>The use of satellite sites in local communities should lead to greater patronage of existing bus services and therefore increased potential for expansion of these services.</p> <p>The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes.</p> <p>This option potentially creates double the existing level of park and ride provision enabling increased benefits in terms of user choices and reduced network congestion.</p> <p>This option has the potential to substantially reduce congestion by removing vehicles from the network before they reach the city network and encouraging modal shift. Improving journey times for all vehicles.</p> <p>The sites proposed are currently used for agriculture or recreation but zoned for development. No site has been identified as yet for the Potterton corridor and therefore these environmental impacts cannot be determined. Mitigation will be put in place to prevent environmental damage e.g. proposed use of suds and landscaping.</p>
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	including provision of appropriate mitigation measures		
	Environment		
	Safety	0	Facilities within the car park would be expected to include lighting. As 2 of the sites would be proposed to be smaller sites there would be reduced footfall which may impact on the perceived safety of the site however the proximity to residential areas would have a positive impact. The site would be expected to be serviced by lighting and CCTV.
	Security	0	Using local bus services and improved links to sustainable modes would have a positive impact. This option is anticipated to have a positive impact on modal change.
	Accessibility to sustainable modes	2	The sites being located on the 3 main corridors into the city from the north means that the park and ride sites are local and integrated within easy access for commuters.
	Integration with the transport network	2	The sites all require new build which may have engineering difficulties. Access to the Scotstown Road site may be difficult to achieve from the existing road due to visibility, network speeds and existing site boundaries/ vegetation etc. A site has yet to be identified for the Potterton corridor and therefore cannot be determined at this stage. The Blackdog site requires the construction of the proposed AWPR junction prior to the development of the site. This is currently experiencing delays due to legal challenges.
	Engineering / construction difficulty	-2	These sites will enable users to access Park and Ride car parks along their commuting corridors and have the potential to lead to improved local bus services for communities. Existing Bridge of Don P&R users may also still be able to access these services. There may be local opposition to the construction of a car park at Scotstown Road depending access provisions to the site.
	Public acceptability of options	2	

		-2	As previously detailed the Blackdog site is developer led but dependent the delivery of the AWPR. The Scotstown site has previously been identified for sheltered housing and access to the site may be problematic. No site has been identified for the Potterton corridor at this time.
Feasibility and affordability		2	This option expands the existing level of park and ride provision and therefore supports the economic aspirations of the north of the city particularly in relation to the sustainability of the Energetica Corridor.
Support of economic aspirations		1	Elements of this option are dependent of the delivery of the AWPR and several of the routes to the proposed sites would be impacted by the changes to the road network as a result of the AWPR. The proximity of the sites to the AWPR would enable services and users to benefit from radial journeys.
Impact of AWPR on proposals			

Transport planning Objectives	Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.	2	<p>Option 6 - AECC Car Park</p> <p>The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.</p> <p>The use of the existing car park at AECC would reduce congestion on the network and promote modal change.</p>
	Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes	2	<p>The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes.</p> <p>This option potentially creates double the existing level of park and ride provision enabling increased benefits in terms of user choices and reduced network congestion.</p>
	Deliver overall environmental benefits	3	<p>This option has the potential to substantially reduce congestion by removing vehicles from the network before they travel into the city network and by encouraging modal shift. Improving journey times for all vehicles.</p>
	Reduce negative environmental impacts to a minimal level when developing	2	<p>The use of the existing car park reduces the need for new construction and existing infrastructure can be used to access the site.</p>

	infrastructure, including provision of appropriate mitigation measures		
	Environment		
	Safety	2	Facilities within the car park would be expected to include lighting. The increased volume of the site would provide for a greater footfall which could lead to an increased perceived feeling of safety.
	Security	1	The site would be expected to be serviced by lighting and CCTV.
	Accessibility to sustainable modes	2	Access can be more directly achieved than that of the current site and improvements could be achieved for sustainable modes.
	Integration with the transport network	1	The site is well located on the existing network however adjacent junctions are subject to congestion at peak times.
	Engineering / construction difficulty	2	Minimal new construction is required to provide an accommodation block and turning area for buses.
	Public acceptability of options	2	Use of this site would enable the public to see better usage of the AECC car park as the majority of events are held at night leaving the car parks empty during the day. Approx. 30% of current users are from the Bridge of Don and they would likely continue to use this site as no further diversion is required.
	Feasibility and affordability	2	As the majority of the infrastructure is in place this is an affordable option, particularly when compared to sites which require to be started from scratch. There may be a financial implication for the AECC in its plan to promote the site for a developer partner however the provision of sustainable transport options at the site will be of benefit overall.
	Support of economic aspirations	2	This option expands the existing level of park and ride provision and therefore supports the economic aspirations of the north of the city particularly in relation to the sustainability of the Energetica Corridor.
	Impact of AWPR on	0	The proximity of the site to AWPR may lead to some linkages with other

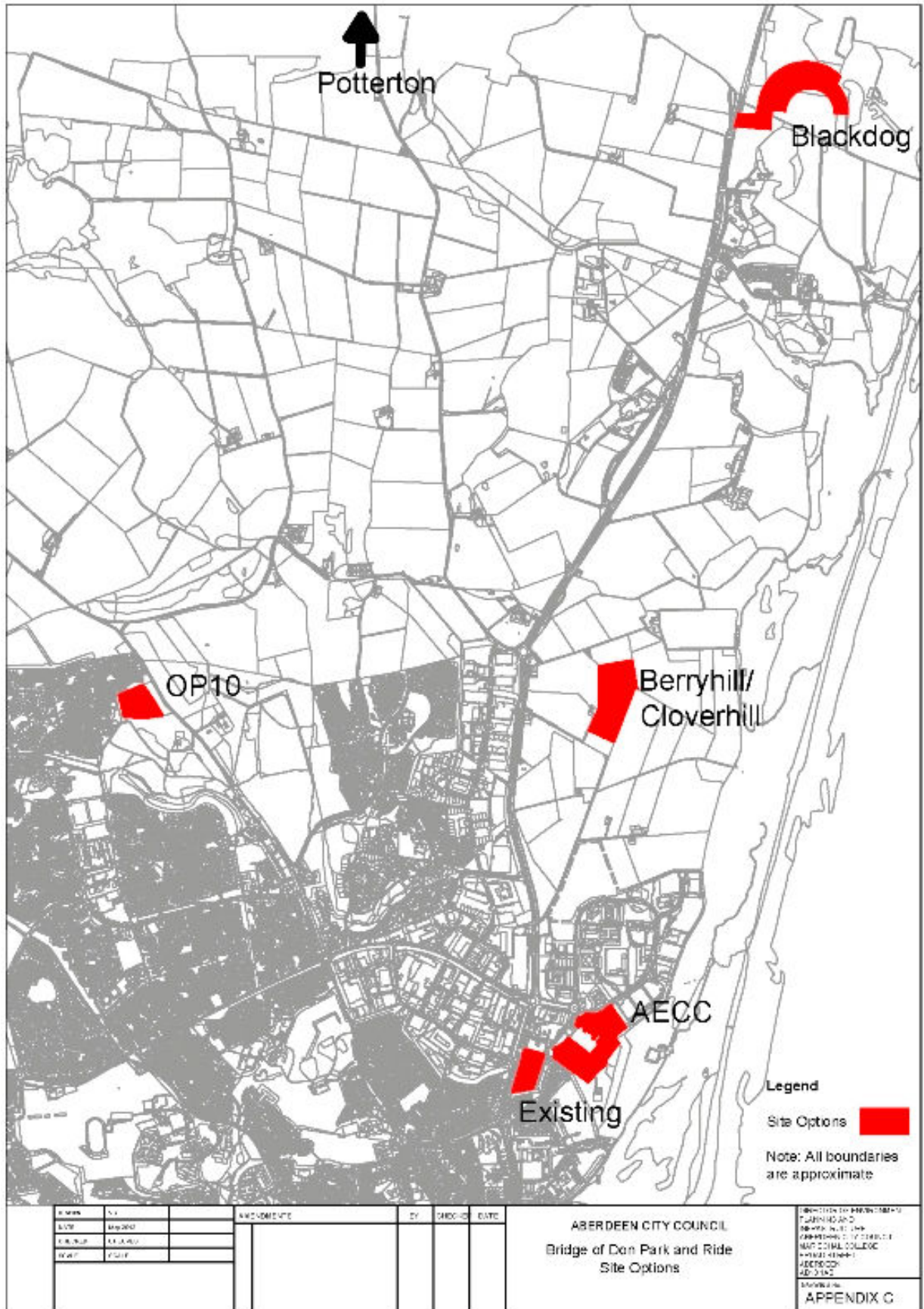
	proposals		radial routes but this option is not dependent on the provision of the AWPR nor impacted by the proposed changes to the surrounding network.
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Transport planning Objectives	Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion and improved journey times, and benefits public transport users.	2	<p>Option 10 - Existing Park and Ride Car Park plus Blackdog</p> <p>The Aberdeen City and Shire Structure Plan aims to make most efficient use of the transport network.</p> <p>The use of the existing car park and Blackdog would provide increased opportunities to reduce congestion on the network and promote modal change.</p>
	Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes	2	<p>The RTS looks to expand P&R provision and the LTS to promote the use of sustainable transport modes.</p> <p>This option potentially creates double the existing level of park and ride provision enabling increased benefits in terms of user choices and reduced network congestion.</p>
	Deliver overall environmental benefits	3	<p>This option has the potential to substantially reduce congestion by removing vehicles from the network before they travel into the city network and by encouraging modal shift. Improving journey times for all vehicles.</p>
	Reduce negative environmental impacts to a minimal level when developing	0	<p>The use of the existing car park reduces the need for new construction and existing infrastructure can be used to access the site. The Blackdog site requires new build and major infrastructure to access the site.</p>

	infrastructure, including provision of appropriate mitigation measures		
	Environment		
	Safety	2	Facilities within the car park would include lighting.
	Security	2	The site would be expected to be serviced by lighting and CCTV. The existing site has been fitted with height restrictive barriers to prevent access by unsuitable vehicles. Blackdog site will benefit from its dual use as it is adjacent a foodhall and there will be increased footfall throughout the day.
	Accessibility to sustainable modes	2	Access can be improved for sustainable modes to both sites.
	Integration with the transport network	2	The sites are well located on the existing network however adjacent junctions are subject to congestion at peak times.
	Engineering / construction difficulty	1	No construction is required at the existing site. The Blackdog site is being developed as part of a major development and is reliant on the AWPR which is currently subject to delay due to legal matters.
	Public acceptability of options	1	Approx. 30% of current users are from the Bridge of Don and they would likely continue to use this site as no further diversion is required. Blackdog would be seen as a dual use site due to the presence of the foodhall/ hotel which may help with public acceptance. The presence of 2 P&R car parks along the same corridor as Ellon P&R may lead to concerns from the public. Care would be required to ensure no deterioration in the success of Ellon P&R.
	Feasibility and affordability	1	Use of the existing site is both feasible and affordable. The Blackdog site is developer led and terms would have to be agreed prior to taking these works forward. At present it is understood that minimal input would be required to secure use of this site. Blackdog is also closely linked to the

			provision of the AWPR junction and therefore timescales for delivery cannot currently be determined.
	Support of economic aspirations	2	This option expands the existing level of park and ride provision and therefore supports the economic aspirations of the north of the city particularly in relation to the sustainability of the Energetica Corridor.
	Impact of AWPR on proposals	2	The Blackdog option is dependent on the provision of the AWPR however it stands to benefit most from its proximity to the new route as it will link radially with other proposed and existing P&R sites.

Bridge of Don Park and Ride – Site Options



Appendix D

NORTH EAST SCOTLAND TRANSPORT PARTNERSHIP

Minute of Meeting of the North East Scotland Transport Partnership Board

Aberdeen, 8 December, 2011

Present:- Councillor Yuill (Chairperson), and Councillors Boulton and Dean (Aberdeen City Council); Councillors Argyle, Clark, Robertson and Webster (Aberdeenshire Council); and Mr Eddie Anderson; and Dr Margaret Bochel and Mr Stephen Archer (Advisers to the Board).

In Attendance:- Derick Murray, Rab Dickson and Kirsty Morrison, Nestrans Office; Julie Anderson (Aberdeenshire Council) and Martin Allan (Aberdeen City Council).

Apologies:- Jennifer Craw and Derek Provan.

The agenda and reports associated with this minute can be found at:-
<http://www.Nestrans.org.uk/47/board-meetings.html>

TREASURER TO THE BOARD

1. The Chair advised the Board that Derek Yule, Treasurer to the Board had left Aberdeenshire Council and that Alan Wood would be Acting Treasurer to the Board. The Chair welcomed Julie Anderson to her first meeting who would present the budget matters on behalf of the Acting Treasurer.

The Board resolved:-
to note the information.

MINUTE OF PREVIOUS MEETING OF 5 OCTOBER, 2011

2. The Board had before it the minute of its previous meeting of 5 October, 2011.

The Board resolved:-
to approve the minute.

LIAISON BETWEEN REGIONAL TRANSPORT PARTNERSHIPS (RTPs) AND THE SCOTTISH GOVERNMENT AND OTHERS

3. With reference to article 5 of the minute of the meeting of the Board of 5 October, 2011, the Board had before it a report by the Director which provided an update on liaison with other Regional Transport Partnerships (RTPs) and the Scottish Government and others.

The meetings included:-

- Regional Transport Partnership Lead Officers' meeting in Edinburgh on 19 October, 2011;
- Health and Transport Steering Group meeting with Transport Scotland and the Scottish Government's Health Directorate in Aberdeen on 1 November, 2011;
- National Roads Maintenance Review meeting in Edinburgh on 2 November, 2011;
- Regional Transport Partnership Lead Officers' meeting in Edinburgh on 16 November, 2011;
- Local Authority and Bus Operator Forum Steering Group special meeting on access to Union Square in Aberdeen on 17 November, 2011;
- Rail Franchise Consultation meeting in Aberdeen on 28 November, 2011;
- Local Authority and Bus Operator Forum Steering Group meeting in Aberdeen on 5 December, 2011;
- Health and Transport Action Plan Steering Group meeting in Aberdeen on 6 December, 2011; and
- Regional Transport Partnership Chairs' meeting including meeting with the Minister for Transport and a meeting with CPT (the trade association representing the UK's bus, coach and light rail industries) and an MSP Briefing in Edinburgh on 7 December, 2011.

The Director provided further information on the meeting with the Minister for Transport on 7 December and explained that topics discussed included the RTPs vision of how Regional Transport Partnerships fit into the Christie Commission's recommendations, the common voice of RTPs across Scotland and also the possibility of RTPs running train services in Scotland. The Director explained that although there was a low turnout of MSPs at the briefing, four were from the Nestrans area and that the discussion centred around similar themes to the meeting with the Minister.

The Board then discussed the importance of co-ordinating a response in relation to the Rail Utilisation Strategy report and Rail Franchise consultation and the importance of presenting a co-ordinated response to the Transport Scotland Rail Directorate at their meeting in Glasgow on 15 December, 2011, which the Chair and the Director would attend.

The Board resolved:-

- (i) to note that the Director would liaise with the Aberdeen Chamber of Commerce and other bodies to get their views in relation to the Rail Utilisation Strategy document and Rail Franchise consultation prior to meeting with the Transport Scotland Rail Directorate in Glasgow on 15 December, 2011 to present a co-ordinated response to Transport Scotland; and
- (ii) to otherwise note the report.

NORTH EAST TRANSPORT CONSULTATIVE FORUM MEETING

4. With reference to article 5 of the minute of the meeting of the Board of 24 August, 2011, the Board had before it a report which provided information on the outcome from the latest meeting of the North East Transport Consultative Forum (NETCF) held at Woodhill House on Monday, 31 October, 2011.

The Board resolved:-

to note the details contained in the report.

STRATEGIC TRANSPORT FUND CONSULTATION

5. The Board had before it a report by the Director which provided information on the results of consultation on the Strategic Transport Fund Supplementary Planning Guidance and informed members of the next steps.

The Board resolved:-

to note the report.

HEALTH AND TRANSPORT ACTION PLAN – ANNUAL REPORT

6. With reference to article 8 of the minute of the meeting of the Board of 5 October, 2011, the Board had before it the annual report on the Health and Transport Action Plan.

The Board resolved:-

- (i) to note that officers would investigate whether they could provide a response or advise in relation to concerns from residents in Deeside (particularly in summer) of the noise from motorbikes in the area; and
- (ii) to otherwise note the annual report.

BUS QUALITY PARTNERSHIP

7. With reference to article 13 of the minute of the meeting of the Board of 24 August, 2011, the Board had before it a report which identified correlations between levels of bus patronage and other factors such as fares, journey times and congestion.

The Board discussed issues relating to the bus operators in the City and the Shire. Issues such as the cost of bus fares; the increased journey times (due to the Traffic Commissioner's instruction on punctuality); the safety aspects for passengers on buses; and the marketing of the bus services were all discussed.

The Board resolved:-

- (i) to request that the Director submits the following issues to a future meeting of the Local Authority and Bus Operator Forum:- safety on buses; bus fares; and the marketing of bus services;
- (ii) to request that the Director write to the Traffic Commission to outline the concerns of bus passengers that although the punctuality of buses has become more reliable, this has had a detrimental effect on the increased running times of buses and the amount of time that a bus will sit at a terminus (or other stops) with passengers on board; and
- (iii) to otherwise note the report.

PROGRESS REPORT

8. With reference to article 10 of the minute of the meeting of the Board of 5 October, 2011, the Board had before it a progress chart summarising the work in the three sub-strategies of the Regional Transport Strategy as at 30 November, 2011.

The Board resolved:-

- (i) to agree that updates relating to the Health and Transport Action Plan should be reported through the progress report; and
- (ii) to otherwise note the report.

PUBLICATIONS AND CONSULTATIONS

9. With reference to article 11 of the minute of the meeting of the Board of 5 October, 2011, the Board had before it a report by the Director which summarised and advised on a number of different publications and consultations and sought approval of draft responses as appropriate.

The report outlined details relating to:-

- Competition Commission – Local Bus Services Market Investigation – provisional decision on remedies;
- Cairngorms National Park – Main Issues Report on National Park Plan;
- Draft Civil Aviation Bill, an effective regulatory framework for UK aviation;
- Rail Franchise consultation;
- Inveramsay Bridge;
- Strategic Development Planning Authority Main Issues Report; and
- Trans European Network (Transport) (TEN-T).

In relation to the rail franchise consultation document, the report explained that as the consultation closed on 20 February, 2012, there would be time to draft a response to the consultation for the Board's consideration at its meeting on 15 February, 2012.

The Board resolved:-

- (i) to request that any figures/evidence used in the preparation of the rail franchise consultation in relation to requirements in the North East of Scotland be shared; and
- (ii) to otherwise note the report.

MATTER OF URGENCY

The Chair intimated that he had directed in terms of Section 50(B)(4)(b) of the Local Government (Scotland) Act 1973, that (a) the supplementary report requesting approval for a contribution from the Nestrans budget to improvements to bus interchange provision at Aberdeen Royal Infirmary, and (b) a virement from the Peterhead Cycling Programme to progress cycling in the Fraserburgh area and the remainder to be spent on strategic

maintenance of the A98 at Portsoy be considered as matters of urgency so that the Board could consider these financial requests at the earliest opportunity.

DECLARATION OF INTEREST

Prior to considering the following item of business, Councillor Dean declared an interest by virtue of her appointment by Aberdeen City Council to the NHS Grampian Board. Councillor Dean chose to remain in the meeting during consideration of the item.

BUDGET MATTERS

10. With reference to article 12 of the minute of the meeting of the Board of 5 October, 2011, the Board had before it a report from its financial adviser which provided an update on spend and programming of the Partnership's 2011/12 budget and which also provided a forecast outturn.

The Board also had before it a request for approval for a contribution from the Board's budget to improvements to the bus interchange provision at Aberdeen Royal Infirmary. The supplementary report explained that a preferred option had been developed, in partnership with NHS Grampian and the two Councils and bus operators for improvements to increase the capacity of the bus interchange at Aberdeen Royal Infirmary and improve the flow of buses through the site. The report explained that a detailed design and costing of the preferred option was currently being carried out by Aberdeen City Council and the initial cost estimate for this work was £120,000. The report put forward the recommendation that the Board consider approving funding from the Nestrans budget of 50% for this project (£60,000) in partnership with NHS Grampian.

The Board were then addressed by the representative of the Acting Treasurer to the Board who requested that the Board consider viring money from the Peterhead Cycling Programme budget to enhance cycling programmes in the Fraserburgh area and that the remainder of the funding be used for strategic maintenance of the A98 at Portsoy.

The substantive report explained that an underspend was now expected to occur on the Ellon Park and Ride Project due to delays forced by land acquisition issues and it was proposed that £48,000 of the agreed project cost be allocated instead to the cost of strategic maintenance works on the A947 north of Turriff.

The Board resolved:-

- (i) to agree to funding 50% of the proposed improvements to bus interchange at Aberdeen Royal Infirmary at a cost of £60,000 (to be funded from an underspend in another area of the budget or from an allowance made from the 2012/13 budget);
- (ii) to agree to the virement from the Peterhead Cycling Programme as detailed above; and

(iii) to agree to the proposed budget virement from the Ellon Park and Ride project as detailed in the report.

2012/13 FUNDING

11. The Board had before it a report by the Acting Treasurer which asked the Board to consider outline revenue and capital funding requests for 2012/13 and agree the proposals for preparing draft revenue and capital budgets for future consideration.

The report explained that the challenging economic climate and budget constraints within the public sector must be considered when requesting funding from the constituent local authorities and it was suggested that the Board seek funding commensurate with the budget allocations that relevant services are receiving within Aberdeen City and Aberdeenshire Councils.

The report advised that the Councils will be considering their own 2012/13 Capital programmes over the next few months and it was suggested that until Council commitments and Government grants become clearer that Nestrans prepares capital and revenue budgets in line with the priorities in the Delivery Plan and based on anticipated available funding from the various funding sources, noting Government and Council's budget considerations.

The report recommended:-

that the Board –

- (a) instruct the Director to write to the two local authorities outlining the Board's considerations and requesting that contributions be allowed for in the Council's budgets; and
- (b) instruct a further report detailing the proposed 2012/13 Capital and Revenue budget expenditure for consideration at the next Board meeting in February, 2012.

The Board resolved:-

to approve the recommendations contained in the report.

STATEMENT OF ACCOUNTS 2010/11 AND THE EXTERNAL AUDITOR'S REPORT TO THE PARTNERSHIP 2010/11

12. The Board had before it a report by the Acting Treasurer to the Board which submitted for the information of the Board members the audited Statement of Accounts for 2010/11 and the external Auditor's report to members of the Board for 2010/11.

The Board heard the representative of the Acting Treasurer explain that the Board had received an unqualified audit from Audit Scotland and that this would be the last year that Audit Scotland would be the external auditor for the Board. She explained that for next year Deloitte would be the external auditor. The report explained that during the course of the audit, the auditors raised a number of matters with officers and the Action Plan in the appendix to the external auditor's report summarised the main issue to be addressed by the Partnership (the adjustment for accumulated absences was not correctly

accounted for in the financial statement submitted for audit). The report explained that the Acting Treasurer and appropriate officers have discussed the Action Plan and have produced a response which includes the planned management action, the responsible officer(s) and target dates for the action.

The report recommended:-

that the Board –

- (a) consider the Statement of Accounts for 2010/11 and the external Auditor's report;
- (b) agree the responses to the external Auditor's Action Plan and that the responsible officers address the issues raised in the Action Plan as set out in the appendix to the external Auditor's report; and
- (c) agree that a £10 charge for a printed copy of the Statement of Accounts be made and that the accounts can be accessed and downloaded free of charge from the Nestrans website.

The Board resolved:-

to approve the recommendations contained in the report.

MATTER OF URGENCY

The Chair intimated that he had directed in terms of Section 50(B)(4)(b) of the Local Government (Scotland) Act 1973, that a supplementary information bulletin report on the Chancellor of the Exchequer's recent response to the air passenger duty consultation be considered as a matter of urgency so that the Board could consider the details at the earliest opportunity.

INFORMATION BULLETIN

13. With reference to article 13 of the minute of the meeting of the Board of 5 October, 2011, the Board had before it a report by the Director which provided information and updates for the Board on a number of matters not requiring a decision.

The report provided updates on the current position of the following projects/issues:-

- Letter from Transport Scotland's Director of Rail;
- High Speed 2 – High Speed Rail;
- Rail Overcrowding Surveys;
- Rail Changes in new December timetable;
- Aviation matters;
- Access to Union Square;
- Aberdeen Royal Infirmary Bus Tickets;
- Nestrans Press Releases; and
- Getabout Events.

The Board also had before it a supplementary information bulletin report. In relation to the Sustainable Framework for UK Aviation consultation, the Board noted that the proposed purchase of BMI by BA is to be assessed for consideration by the European Union (DG Competition) and should BA be the

purchaser this would remove competition between the Scottish Airports and Heathrow as BMI have already pulled out of the Glasgow to Heathrow route. The report explained that Nestrans officers had contacted the Office of Fair Trade in the UK to highlight the possibility of the loss of competition between Scottish airports and Heathrow and the Office of Fair Trade were aware of the proposals and are monitoring the proposed sales progress. The report explained there is a process where the DG Commission can refer the sale back to the UK for consideration and Nestrans has asked the Office of Fair Trade to consider whether this may be appropriate given that the concern is for flights internal to an EU member country.

In relation to Access to Union Square, the Board were informed that Dr Bochel had spoken to Grampian Police regarding the issues and they will be in attendance at Union Square every weekend in the lead up to Christmas to try and alleviate the access problems to the shopping centre. The Board also noted that work was ongoing in relation to the traffic light timings at the junctions leading into Union Square. The report had appended to it a minute of a meeting held to discuss the access at Union Square which outlined short term and longer term proposals to alleviate the traffic concerns in this area.

The supplementary report outlined the highlights of the Chancellor of the Exchequer's response to the air passenger duty consultation which included details such as:-

- helicopters are to be exempt from the duty;
- air passenger duty rates are to increase as identified in the budget report with no changes to the geographical bandings;
- investigations are still underway into the feasibility and impacts of the devolution of air passenger duty to Scotland but devolution to Northern Ireland was confirmed;
- whilst reaffirming the UK Government's intention to rebalance the UK economy, the Government noted that the regional impact of air passenger duty was tied to the UK Government's Aviation Policy deliberations due to be published for consultation in March, 2012;
- in relation to the Scottish Government's published Infrastructure Investment Plan for the period 2012 to 2030 the Plan outlined details relevant to the North East of Scotland which included timing of the Aberdeen Western Peripheral Route development and Balmedie to Tippetty improvement;
- timings for the Aberdeen to Inverness rail improvements; and
- timings in relation to the Aberdeen to Central Belt rail improvements.

The document also explained that the upgrading to dual carriageway of the A96 between Inverness and Aberdeen was important, however, the expenditure plan only showed the dualling between Inverness and Nairn by 2030.

The Board resolved:-

- (i) to request that the Director arrange a joint meeting with Hitrans and the European Union to discuss the regional airport issues relating to the possible loss of competition between Scottish airports and Heathrow;
- (ii) to request officers to consider other options to alleviate the traffic concerns at Union Square, including a stacking lane on Market Street and the use of additional car parking facilities in the area;

(iii) to request that the Director add the timing of the improvements to Inveramsay Bridge to the agenda for the meeting with Francis Duthie, Director of Rail, Transport Scotland;

(iv) to agree that the Chair and the two leaders of the Councils write to Keith Brown, MSP, Minister for Transport outlining their concern as to the timing of the improvements to the Aberdeen to Inverness railway and the dualling of the A96; and

v) to otherwise note the detail of the reports.

CONFERENCES AND PRESENTATIONS

14. With reference to article 14 of the minute of meeting of the Board of 5 October, 2011, the Board had before it a report by the Director summarising recent and forthcoming conferences of interest to the Partnership along with presentations by Nestrans and its partners.

The Board resolved:-

to note the details.

PENDING BUSINESS AND REPORTS FOR FUTURE MEETINGS

15. With reference to article 15 of the minute of meeting of the Board of 5 October, 2011, the Board had before it a report by the Director detailing pending business and information on reports to be submitted to future Board meetings.

The Board resolved:-

to note the report.

IAN YUILL, Chairperson.

NORTH EAST SCOTLAND TRANSPORT PARTNERSHIP

Minute of Meeting of the North East Scotland Transport Partnership Board

Aberdeen, 15 February, 2012

Present: Councillor Yuill (Chairperson), and Councillors Boulton, Dean and John West (as substitute for Councillor McCaig) (Aberdeen City Council); Councillors Argyle, Clark, Robertson and Webster (Aberdeenshire Council); Jennifer Craw and David Sullivan; and Dr Margaret Bochel and Stephen Archer (Advisers to the Board).

In Attendance: Derick Murray, Rab Dickson and Kirsty Morrison (Nestrans Office); Julie Anderson (Aberdeenshire Council); and Martin Allan (Aberdeen City Council).

Apologies: Councillor McCaig (Aberdeen City Council), Eddie Anderson and Derek Provan.

The agenda and reports associated with this minute can be located at the following link:

<http://www.nestrans.org.uk/48/board-meetings.html>

MINUTE OF PREVIOUS MEETING

1. The Board had before it the minute of its previous meeting of 8 December, 2011.

The Board resolved:

to approve the minute as a correct record.

LIAISON BETWEEN REGIONAL TRANSPORT PARTNERSHIPS (RTPs) AND THE SCOTTISH GOVERNMENT AND OTHERS

2. With reference to article 3 of the minute of its meeting of 8 December, 2011, the Board had before it a report by the Director which provided an update on liaison with other Regional Transport Partnerships (RTPs), the Scottish Government and others.

The meetings included –

- Local Authority and Bus Operator Forum Steering Group meeting in Aberdeen on 5 December, 2011
- Transport Scotland Rail Directorate meeting in Glasgow on 15 December, 2011
- Scottish Ambulance Service meeting in Edinburgh on 12 January, 2012
- Meeting with the Minister for Transport in Edinburgh on 24 January, 2012
- First ScotRail and Tactran meeting in Aberdeen on 26 January, 2012
- Regional Transport Partnership Lead Officers' meeting in Edinburgh on 15 February, 2012.

In relation to the meeting with the Minister for Transport held in Edinburgh on 24 January, 2012, the Director advised that a series of workshops involving stakeholders would be held in February/March 2012 to discuss how to progress the dualling of the road at the Inveramsay Bridge, and that these meetings would advise decisions on progress of this and other schemes. The Director then informed the Board that he was to have a separate meeting with Transport Scotland on 29 February, 2012 to discuss the Inveramsay Bridge proposal.

The Director then advised of potential changes to the train timetables which would affect Aberdeen and the North East, including extra stops at Portlethen and Laurencekirk. He further explained that it was proposed that the 05:56 Aberdeen to Edinburgh train leave six minutes earlier, and that these proposals would be put out for consultation prior to being considered by Transport Scotland, and if agreed, would be incorporated into the timetable by 12 December, 2012.

The report also provided details of future meetings planned –

- Health and Transport Action Plan Steering Group to be held in Aberdeen on 28 February, 2012
- Local Authority and Bus Operators Forum Steering Group meeting to be held in Aberdeen on 1 March, 2012 and
- The Regional Transport Partnership Chairs' meeting to be held in Dumfries on 7 March, 2012.

The Board resolved:

to note progress on liaison arrangements with other RTPs, the Scottish Government and others and arrangements for future meetings.

REGIONAL PARKING STRATEGY

3. With reference to article 4(C) of the minute of its meeting of 24 August, 2011, the Board had before it a report by the Director which presented the draft Regional Parking Strategy for consideration.

The Board resolved:

- (i) to instruct that the draft Regional Parking Strategy be submitted to the two Councils for their consideration;
- (ii) following consideration of the draft Strategy by the two Councils, and incorporation of their comments, to instruct that the draft Strategy be published for formal consultation with stakeholders and the public; and
- (iii) following consultation, to instruct that a final Strategy be brought back to the Board for approval.

REGIONAL TRANSPORT STRATEGY REFRESH

4. With reference to article 8 of the minute of its meeting of 8 December, 2011, the Board had before it a report by the Director which updated members on the proposed timescales for reviewing and refreshing the Regional Transport Strategy.

The Board resolved:

to approve the proposed timeframe for the refresh of the Regional Transport Strategy, as appended to the report.

ALTERNATIVE FUELS – HYDROGEN BUSES

5. Reference was made to article 3 of the minute of its meeting of 24 August, 2011, at which time the Board were advised that Aberdeen Renewable Energy Group (AREG) had submitted a bid for European funding for a hydrogen bus trial. Members had before them on this day a report by the Director which advised members of progress made to date with this project, and requested future consideration of any possible Nestrans' involvement.

The Board resolved:

(i) to support the project in principle, and to agree to further discussions with the project developers and stakeholders;

(ii) to request that these discussions should take cognisance of the potential funding discussions underway and the operational aspects of the project;

(iii) to request that the discussions should consider the priority for funding from the transport budget for this type of project, given the priorities and commitments already made in the Nestrans delivery programme; and

(iv) to instruct that a further report be prepared for the Board's consideration in due course, incorporating the aspects discussed above, once further detail and clarification is available.

DYCE SHUTTLE BUS/ DYCE STATION UPGRADE

6. With reference to article 4(B) of the minute of its meeting of 8 June, 2011, the Board had before it a report by the Director which advised members of progress in relation to the proposal to upgrade Dyce railway station, and sought approval for funding to continue the Nestrans funded shuttle bus, linking the train station to the airport and industrial estates.

The Board resolved:

(i) to note the content of the report;

(ii) to instruct that further work be undertaken to develop proposals to enhance the station, including the potential for bus access to the west side;

(iii) to authorise the issuing of a tender for the continuation of the service 80 shuttle bus; and

(iv) to delegate authority to the Director, in consultation with the Chair and Vice Chair of the Board, to appoint an operator for the next 12 months.

SCOTRAIL 2014 CONSULTATION RESPONSE

7. The Board had before it a report by the Director which sought approval of a response to Transport Scotland's document titled "Rail 2014 – Public Consultation", which was appended to the report.

The appendix contained a suggested response to the main questions contained in the consultation document, however, there were a number of key points which formed the basis of the proposed submission, as follows –

- That maintaining through trains between the North East and London was critical both for economic/business and for leisure trips. The possibility of requiring all passengers to change trains at Edinburgh was unacceptable, would inconvenience many travellers, harm economic and tourism potential, create severe capacity issues and limit travel choice to/from the North East. Transport Scotland and the Scottish Government should insist on the Department for Transport's commitment to dual purpose rolling stock being commissioned as soon as practical.
- That maintaining an overnight sleeper service between Aberdeen and London was a priority. The sleeper provided an important link and the Scottish Government should commit to its long term future whether as part of, or as separate from, the new franchise.
- The possibility of splitting the franchise into economic and social parts of the railway causes concern. There were many uncertainties around this possibility which were unresolved, such as what parts of the network would be considered "economic", particularly if that may include some parts of routes.
- That there was a need for further development of rail potential in and around the North East, including new stations and incremental enhancements towards a local cross rail service. Stopping patterns, stations and pathing in this area all suffer from under investment and lack of priority within the planning of railway infrastructure and services. Commitment was required to improve track, signalling, rolling stock and stations to fulfil the potential of the railways in the North East.
- Fare structure needs to be reconsidered to ensure that services were operated efficiently and fairly.

The Board resolved:

- (i) at question 27 to the response (how can local communities be encouraged to support their local station?) that proper fare structures be added at this point;
- (ii) to remove the word "perceived" in the response at question 29(7) (comfort of trains);
- (iii) to add the word "absolutely" in front of unacceptable at the start of the response to question 30;
- (iv) to emphasise as strongly as possible the importance of improving the quantity and quality of rolling stock and the requirement for further availability of seats on the network;
- (v) to emphasise the importance of passengers being able to park at local stations prior to embarking on journeys (i.e. cars, motorbikes and bicycles);
- (vi) to emphasise the importance of using the available seats on trains in the most efficient manner (including reserved seats);
- (vii) to emphasise that tourists using the rail network can use it for both social and economic reasons and to add at question 28 the importance of addressing disabled users' needs; and
- (viii) to otherwise agree the contents of the report and endorse the response as Nestrans' comments on the Rail 2014 consultation document.

PROGRESS REPORT

8. With reference to article 8 of the minute of its previous meeting of 8 December, 2011, the Board had before it a progress chart summarising the work in the three sub strategies of the Regional Transport Strategy as at 7 February, 2011.

The Board resolved:

- (i) to note that the Director will circulate details on the deadlines for submissions to the Green Bus Fund; and
- (ii) to note the content of the progress chart.

PUBLICATIONS AND CONSULTATIONS

9. With reference to article 9 of the minute of its previous meeting of 8 December, 2011, the Board had before it a report by the Director which summarised and advised on a number of different publications and consultations as follows and sought approval of draft responses as appropriate –

- Scottish Ferries draft plan
- Possibility of road user charging for HGVs.

The Board resolved:

to note the report and the documents referred to above, and to endorse the comments as Nestrans' response to the consultations.

BUDGET MATTERS

10. With reference to article 10 of the minute its previous meeting of 8 December, 2011, the Board had before it a report by the Acting Treasurer which provided an update on spend and programming of the Partnership's 2011/12 budget and forecast outturn in this regard.

The report explained that there was an underspend on the Ellon Park and Ride project and it was proposed that £47,000 of this project's money be allocated to the cost of strategic maintenance works on the A98 at Smiddybone to Kindrough, Portsoy.

The Board resolved:

- (i) to note the monitoring position and forecast as detailed within the report; and
- (ii) to agree to the budget virement as detailed above.

2012/13 BUDGETS

11. With reference to article 11 of the minute of its meeting of 8 December, 2011, the Board had before it a joint report by the Director and the Acting Treasurer to the Board which asked the Board to agree capital and revenue budgets for 2012/13 subject to available funding being confirmed.

The Director explained that the Scottish Council for Development and Industry (SCDI) were to hold a meeting with the Minister for Transport (Keith Brown) and he proposed that Nestrans meet the cost of the dinner at a cost of approximately £2,000 to £3,000.

The report recommended:

that the Board approve the 2012/13 revenue and capital budgets as shown in the appendices to the report, subject to anticipated funding levels being confirmed and making any appropriate adjustments following the Board's consideration.

The Board resolved:

- (i) to agree to meet the cost of the SCDI dinner with the Minister for Transport;
- (ii) to request that the Director liaise with Ian Armstrong of the SCDI with regard to subject matters to be discussed at the dinner which will be of benefit to Nestrans and its partners and following which to request that the Director email all members of the Board with a draft set of topics to be considered; and
- (iii) to approve the recommendation contained in the report.

TREASURY MANAGEMENT 2012/13

12. The Board had before it a report by the Acting Treasurer which sought approval for the revised Treasury Management Strategy Statement 2012/13.

The Board resolved:

to approve the revised Treasury Management Strategy as appended to the report.

MATTER OF URGENCY

The Chair intimated that he had directed in terms of Section 50(B)(4)(b) of the Local Government (Scotland) Act 1973, that details of a meeting held on 13 February, 2012 regarding parking/traffic flow at Union Square, Aberdeen be considered as a matter of urgency so that the Board could consider the details at the earliest opportunity.

INFORMATION BULLETIN

13. With reference to article 13 of the minute of its previous meeting of 8 December, 2011, the Board had before it a report by the Director which provided information and updates for the Board on a number of matters not requiring a decision as follows –

- Railway Stations Win Awards
- Draft Civil Aviation Bill: An Effective Regulatory Framework for UK Aviation
- H2S – High Speed Railway
- Aviation Consultation Update
- Laurencekirk Junctions
- Strategic Transport Fund
- Scottish Transport Awards

- Green Corridors in the North Sea Region (GreCor)
- Rail Changes in December, 2011 Timetable
- Nestrans Press Releases
- Getabout Events
- Performance Indicators
- Union Square/Market Street – Parking/Traffic Flow - Update

The Board resolved:

to note the content of the bulletin.

CONFERENCES AND PRESENTATIONS

14. With reference to article 14 of the minute of its previous meeting of 8 December, 2011, the Board had before it a report by the Director summarising recent and forthcoming conferences of interest to the Partnership along with presentations by Nestrans and its partners.

The Board resolved:

to note the information as presented.

PENDING BUSINESS AND REPORTS FOR FUTURE MEETINGS

15. With reference to article 15 of the minute of its previous meeting of 8 December, 2011, the Board had before it a report by the Director detailing pending business and information on reports to be submitted to future Board meetings.

The Board resolved:

to note the information.

- IAN YUILL, Chairperson.

Appendix E

Updated Self assessment checklist for Regional Transport Partnerships, the ambulance service, NHS Boards, councils and other partners to improve transport for health and social care.

Action	Assessment of Current Position (Reported January 2012)	Comments on Current Position (Reported January 2012)	Progress Made since January 2012
<p>We routinely collect data on the activity, costs (including unit costs) and quality of services we provide</p>	<p>Yes – in place but needs improving</p>	<p>Financial monitoring and reporting is undertaken on all aspects of transport. Spends are reviewed and associated with each activity. Quality of service is routinely monitored for School Transport, whereby a Contract Monitoring Officer undertakes daily spot checks, undertakes site visits following complaints and produces bi-annual quality monitoring reports. There is less activity undertaken to monitor and collect information on the quality of Social Work Transport and in particular the councils in-house passenger transport.</p>	<p>We have identified the need to have performance indicators in place to monitor more closely how we are performing with all our transport services, which will in turn provide data on the quality of service and this will compliment our spend analysis. Primarily this will be focused around increased levels of routine monitoring of services and annual customer satisfaction surveys for those transported on councils in-house passenger transport. It is anticipated these will be introduced over the course of this financial year.</p>
<p>We routinely benchmark performance and cost to ensure resources</p>	<p>Yes – in place but needs improving</p>	<p>All School and Social Work transport is procured through the undertaking of a competitive tender process and where practical transport is conducted by utilising the Councils in-house</p>	<p>It is anticipated that the introduction of performance indicators, as above noted, will allow us to better draw a direct comparison between performance and</p>

<p>are used efficiently</p>		<p>passenger fleet. This process allows the Council to ensure that transport procured is best value. As above noted performance is monitored by a Contract Monitoring Officer. More work could be undertaken to draw a direct link in terms of benchmarking performance and cost.</p>	<p>costs as we will have substantially more data to indicate how we are performing than at present.</p>
<p>We regularly review funding arrangements for transport for health and social care to ensure that they maximise value for money and reflect local need</p>	<p>Yes – in place but needs improving</p>	<p>All Social Care transport is subject to an assessment being completed by officers in Social Care and Wellbeing. Only if a client passes the assessment criteria, is transport put in place. Transport is arranged with operators who have been approved following a tendering process or they are transported using the council's in-house passenger transport ensuring low costs. Improvements can be made by increasing the levels of joint working.</p>	<p>Work is ongoing through HTAP for increased levels of working with partner organisations. The proposed introduction of a health transport information hub will allow the partner organisations to collate more data as to what the public needs are for access to health and social care. Since January it has been established that the Scottish Ambulance Service will manage the transport hub. Work is ongoing to establish a base for this and a staff resource, Scottish Ambulance Service officials have already identified a number of sources which are undergoing further investigation.</p>
<p>We have improved how we arrange transport services within our own organisation and considered the need for a central team or</p>	<p>Yes – in place and working well</p>	<p>As aforementioned all of the Council's passenger transport is arranged through the Public Transport Unit. This central point has allowed for greater integration between school, social work and in-house transport services. In addition we have utilised our in-house passenger fleet more effectively since early</p>	<p>No Action Required.</p>

<p>coordinated approach</p>		<p>2010 by using these vehicles to provide School Transport, more ad-hoc social work transport and to provide a DRT service (Community Transport).</p>	
<p>We have reviewed the timing of appointments and care services to make sure that transport provision is considered.</p>	<p>Yes – in place but needs improving</p>	<p>The Public Transport Unit's primary Social Care service function is transport to day centre's and day activities. The timings of these cannot realistically be altered due to the nature of the service they provide. However a number of the centres do have staggered start times which allow for one vehicle to undertake transport for multiple establishments. Most of the on demand ad-hoc Social Care transport requests are flexible in their timings which allow the PTU to ensure the most appropriate and cost effective transport can be used, particularly allowing utilisation of the in-house passenger fleet. More consideration could be undertaken from a Social Care aspect as to the placing of clients, particularly respite clients, as transport does not appear to be given due consideration in this aspect.</p>	<p>The main problem identified in this area is the locations of respite placements as they often do not compliment existing transport arrangements for clients. This is not an easy matter to resolve as there is generally a good reason for a respite placement being where it is. However the PTU have been receiving respite information in advance from Social Work colleagues and the most recent round of information has certainly allowed for more efficient transport being arranged as we have had notifications further in advance than previously.</p>
<p>We have reviewed the use of taxis and considered scope for efficiencies within our own organisation and in partnership with others</p>	<p>Yes – in place but needs improving</p>	<p>All School and Social Work transport is subject to competitive tender, which ensures best value. We have examined the costs of using external operators against that of using our own in-house fleet. In the last year we have ceased using external operators to provide day centre transport and this is now fully delivered in-</p>	<p>Ongoing work through the HTAP no progress to report.</p>

		<p>house. We have also utilised our in-house transport to deliver two more school transport services, along with using the in-house fleet to deliver as many of the ad-hoc, on demand Social Care transport requests as possible. Further work is required to examine the potential of working in partnership with others. Work is ongoing under the HTAP to identify opportunities for partnership working and to establish if efficiencies can be made. This level of work is likely to take a relatively considerable period of time.</p>	
<p>We have worked with partners to clarify responsibilities for planning and delivering transport for health and social care.</p> <p>We have put systems in place to routinely engage with service users to ensure that their views inform the development of transport for health and social care services.</p>	<p>No – but action in hand</p>	<p>The Council has not undertaken any work on this aspect. However the matter has been raised and will be progressed through further work of the HTAP.</p>	<p>Ongoing work through the HTAP no progress to report.</p>
<p>We have put systems in place to routinely engage with service users to ensure that their views inform the development of transport for health and social care services.</p>	<p>Yes – in place but needs improving</p>	<p>This area is relatively poor. The Council has undertaken satisfaction surveys, but not to any great extent with regards to Education or Social Care Transport services and certainly measures are required to improve engagement. The Community Transport DRT service was designed following surveys and consultation with service users and we annually undertake satisfaction surveys with members of the service. Any suggestions are taken into consideration and issues are addressed. Action</p>	<p>As above noted we will soon be undertaking annual customer satisfaction surveys for all clients who are transported on the Councils in-house passenger fleet. We also hope to survey social work colleagues and establishments in the near future to establish their thoughts on how transport services are delivered. We would always discuss and consult on developments or changes to transport</p>

<p>We assess the impact of service changes on users and other services, taking account of transport needs.</p>	<p>Yes – in place but needs improving</p>	<p>is required in this area. All service changes require an Equality and Human Rights Impact Assessment to be completed. All PBB options which may have affected these services would also have been subject to Equality and Human Rights Impact Assessments. Improvements can be made by further engaging with service users and working with partner organisations to ensure viable transport options are available.</p>	<p>with Social Work colleagues. Ongoing work through HTAP. No progress to report.</p>
<p>We ensure that transport for health and social care services are based on an assessment of need and regularly monitor and evaluate them to ensure value for money.</p>	<p>Yes – in place and working well</p>	<p>Social Care transport is only put in place following an assessment for eligibility. This eligibility is reviewed by Care Managers on an annual basis. Transport is only put in place with operators who have been through a competitive tendering process, so this also ensures best value.</p>	<p>No action required.</p>
<p>We have ensured that staff members are well informed about all transport options in our area so that they provide good information to the public about available transport options, eligibility criteria and</p>	<p>Yes – in place but needs improving</p>	<p>This is an area which has weakened over the past couple of years. When the council operated Concessionary Travel schemes, the team administering these schemes had good knowledge about transport options in the region and was able to disseminate this information. With a reduction in the team and less involvement in this area knowledge certainly has been reduced and needs to be reviewed. Also, as aforementioned, consideration is being given</p>	<p>As above noted the health transport information hub is being further progressed through HTAP and is moving closer to implementation. We will also require a number of PTU staff, as part of their personal development plan, to refresh their knowledge of concessionary travel schemes and transport options over the next year.</p>

<p>charges.</p>		<p>to the creation of a 'health transport information hub'. This hub would have three main functions: to provide information on available transport services to those requiring transport for health purposes; where appropriate to broker trips e.g. on community transport services; and, to create an information database on trip requests for planning purposes as there is currently only limited information on unmet travel needs.</p>	
<p>We have put in place a plan to integrate or share services where this represents more efficient resources and better service for users, including considering an integrated scheduling system.</p>	<p>Yes – in place but needs improving</p>	<p>As above noted all of the Council's passenger transport is arranged through the Public Transport Unit. This central point has allowed for greater integration between school, social work and in-house transport services. Also we have utilised our in-house passenger fleet more effectively since early 2010 by using these vehicles to provide School Transport, more ad-hoc social work transport and to provide a DRT service (Community Transport). In addition investigations are being conducted looking at the possibility of a shared Public Transport Unit with Aberdeenshire Council. Aberdeen City Council currently does not use an integrated transport scheduling system. All transport is scheduled manually. Further investigations will also be considered under the HTAP to examine whether further integration or scheduling can be undertaken with partner organisations.</p>	<p>Ongoing work through HTAP. No progress to report.</p>
<p>We collect information</p>	<p>Yes – in</p>	<p>Applications for School Transport and Social</p>	<p>Ongoing work through HTAP. No</p>

<p>on the personal characteristics of people who need transport for health and social care to allow monitoring of equality and diversity and to develop services that meet their needs.</p>	<p>place and working well</p>	<p>Care Transport Eligibility Forms/ Transport request forms require full information to be detailed with regards to each service user's requirements and needs. This includes mobility aids/wheelchair information, medical information, behavioural information and emergency procedure information, all being detailed. This, along with transport assessments, means we can ensure transport is tailored to meet service users needs, including whether they require an escort on transport. In addition, with the Community Transport services, users must notify the PTU at the time of booking of any mobility aids used / wheelchair type and any medical conditions.</p>	<p>progress to report.</p>
<p>We involve the voluntary sector in the planning and delivering transport for health and social care to meet the needs of the local population.</p>	<p>No – Action needed</p>	<p>Currently we do not undertake any large involvement with the voluntary sector. Discussions had been held in previous years around whether the Council and voluntary organisations could assist each other. It is anticipated that further work through the HTAP will allow this Council to be more proactive on this front.</p>	<p>Ongoing work through HTAP. We have however introduced a transport run whereby our in-house passenger transport is providing after school transport on behalf of VSA. This is sharing a transport resource already in operation for the Council and has been implemented as a trial. It has proved to be positive in opening lines of communication between the PTU and the voluntary sector and sharing a resource which is more cost efficient.</p>

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ABERDEEN CITY COUNCIL

COMMITTEE: **Enterprise, Planning and Infrastructure**

DATE: **31st May 2012**

DIRECTOR: **Gordon McIntosh**

TITLE OF REPORT: **Review of Charges for Street Occupations**

REPORT NUMBER: **EPI/12/094**

1. PURPOSE OF REPORT

The purpose of this report is to consider a review of existing Charges for Street Occupations covered by the Roads (Scotland) Act 1984 and the New Roads and Street works Act 1991.

2. RECOMMENDATION(S)

That the Committee:

1. Approve the charges detailed in the report and that these be applied from the 1st July 2012

3. FINANCIAL IMPLICATIONS

The revised charges will generate revenue income that will meet increased staff costs and sustain service provision.

4. OTHER IMPLICATIONS

None

5. BACKGROUND/MAIN ISSUES

In accordance with statutory obligations Aberdeen City Council as the Local Roads Authority has a responsibility to regulate and manage street occupations to ensure that

essential works have a minimal impact on the road network and that the infrastructure is protected in line with specification and guidelines. Considerable officer time is dedicated to the management of the road network and the consideration of applications for various road work operations and street occupations.

A scale of charges for this service was previously set by the Committee in May 2011 and is due for review in 2012.

Officers have reviewed the charging regime and have prepared a revised scale of charges which are detailed in Appendix A of this report. The revised charges have been generally increased in line with inflation. However where charges are applied as a result of illegal operations or to reduce occupation periods, these charges have been significantly increased to encourage contractors to comply with regulatory requirements and to minimise disruption to all road users.

Road users and the wider public face increasing safety problems and delays due to mobile platforms (also known as cherry pickers). The danger to the public is greater than that of mobile scaffolds due to the need for road traffic management and therefore a set of charges is proposed to reflect this.

Officers are regularly made aware of illegal activities using such equipment and at present find the level of the surcharge is insufficient to act as a deterrent and that the new charge suggested should reflect the severity of the unlawful activity. It is clear from experience of charging where the introduction of high surcharges exist, for example skip permits, very few unauthorised occupations have occurred. It is felt that the current level of surcharge should be significantly increased to reflect the severity of the activity in relation to the risk to the public and the road user in the hope that it acts as a deterrent to illegal activities.

No changes are proposed to the current charges under the Development / Construction rates for occupation based on a square metre charge as the current scales have encouraged contractors to pursue alternative means for site set up facilities off road keeping the road network clear of obstructions wherever possible.

Appendix A also includes a proposal to increase the charge to developers for the promotion of permanent Traffic Regulation Orders. The current fee of £1500 has remained in place since pre 2004, yet the cost of advertising the orders and staff costs have continued to increase. These small scale traffic management measures are taken forward at the request of developers as part of the development management process or as part of a planning condition.

Consideration was also given to increased charges for Pavement Cafés, Promotional Events, Licensed hot/cold food units and Day Rates for use of road space for commercial purposes. Unlike the charges for occupations that have been in place for many years these new charges were only introduced last year and it was considered that a bedding in period was appropriate before any further increases are made.

It is recommended that the charges detailed within Appendix A be adopted and come into operation on the 1st July 2012 if approved by the Finance and Resources Committee.

6. IMPACT

This subject of this report provides support to the economic and environmental well being of the city. It promotes safety and management of the local road infrastructure, with the intension of minimising disruption to the network, whilst maintaining delivery of essential transport services for the travelling public. The report supports service delivery and maintains positive operational outcomes to meet statutory obligations.

7. BACKGROUND PAPERS

None

8. REPORT AUTHOR DETAILS

Raymond Moffat
Team Leader
Roadworks Co-ordination Unit
Tel. 538066

Appendix A - Revision of charges from 1st July 2012

Temporary Traffic Restrictions

	Charge from 01/05/2011	Charge from 01/07/2012
For continuous period of up to 21 days:		
Application received 28 days or more prior to Order coming in to effect	£750	£800
Application received less than 28 days prior to Order coming in to effect	£1000	£1100
For continuous period over 21 days but not more than 6 months:		
Application received 28 days or more prior to Order coming in to effect	£1500	£1575
Application received less than 28 days prior to Order coming in to effect	£1750	£1850
For repeat posting of weekly or monthly notices per event	£150	£160

Allowing for work load, 28 days is the minimum normally required to process an application for a temporary traffic order. While an application can be processed in less time, more expensive media will be required for the advertising. This additional cost is reflected in the new charges made to applicants.

Permanent Traffic Orders

	Charge from 01/05/2004	Charge from 01/07/2012
Charges for the promotion of permanent traffic orders	£1500	£2000

Temporary/Permanent Industrial Access

	Charge from 01/05/2011	Charge from 01/07/2012
Charge for visiting site, processing application and granting permission	£400	£425
Surcharge for illegal occupation	£400	£500

Consent for Excavating in the Road under Section 56 or 61 of the Roads (Scotland) Act 1984.

	Charge from 01/05/2011	Charge from 01/07/2012
Excavation not involving installation of private apparatus	£400	£425
Surcharge for illegal occupation	£400	£500

Consent for Excavating in a Road under Section 109 of the New Roads & Street Works Act 1991 (NRSWA)

	Charge from 01/05/2011	Charge from 01/07/2012
Charge for initial application and permission for works which are only one inspection unit	£400	£425
Surcharge for illegal occupation	£400	£500
Additional charge for works over one inspection unit	£65 per additional unit	£75 per additional unit
Charge for recording plant information in VAULT overlays.	£150	£150

These charges contribute towards the Council's expenditure on the Symology system, which is employed to notify Statutory Undertakers of work in accordance with NRSWA. Applicants will also be required to meet Inspection charges in accordance with regulations issued under NRSWA.

Street Occupations

Scaffold/Hoarding permits (per street):	Charge from 01/05/2011	Charge from 01/07/2012
For a continuous period of up to 1 month (Under 25 metres in length):	£100	£105
(25 metres or over)	£150	£160
For each subsequent month of original application (Under 25 metres in length):	£65	£70
(25 metres or over)	£100	£105
For subsequent extension for up to 1 month (Under 25 metres in length):	£90	£95
(25 metres or over)	£125	£135
Surcharge for Illegal occupation of the road.	£200	£250
Mobile tower scaffolds (to be considered as a mobile scaffold it must be erected and dismantled daily otherwise see scaffold charges above)		
Charge per day per location:	£20	£25
Charge per week per location:	£35	£50
Surcharge for illegal occupation of the road.	-	£150

Crane, Hydraulic Platform Permits:	Charge from 01/05/2011	Charge from 01/07/2012
Charge for granting permission (day permit, per site).	£65	£70
Charge for granting permission (week permit, per site)	£85	£90
Roaming permit (day permit up to 5 sites, max 90 minutes per site).	£85	£100
Surcharge for illegal occupation of the road.	£65	£250

Temporary Traffic Lights Permits:	Charge from 01/05/2011	Charge from 01/07/2012
For a continuous period of up to one week, charge for granting permission	£75	£80
Surcharge for illegal occupation of the road.	£400	£400

Skip Permits:	Charge from 01/05/2011	Charge from 01/07/2012
For continuous period of 7 days	£30	£32
Surcharge for illegal occupation of the road	£100	£120
Additional daily charge for a skip sited within a Pay and Display area.	£30 for zones ABCEF & G £15 for all other zones	£30 for zones ABCEF & G £15 for all other zones

The surcharge is payable where a skip is placed without a permit being first obtained.

The daily charge for Pay and Display contribute to the loss of revenue from parking charges and the amount equates to standard charges, for which there has been no increase, made by the Council for daily occupation of a Pay and Display space for the purposes of loading etc.

Development / Construction – Occupation of Road Space:	Charge from 01/05/2011	Charge from 01/07/2012
<p>Occupation of Road Space The area enclosed by the outer edges of the following roads; Holburn Street, Willowbank Road, Springbank Terrace, Wellington Place, South College Street, Guild Street, Regent Quay Virginia Street, Commerce Street, East North Street, West North Street, Mounthooly Roundabout, Gallowgate, Spring Garden, Maberly Street, Rosemount Place, Esslemont Avenue, Rose Street, Union Street and all traffic sensitive roads listed on the Scottish Street Works Register (data retained on the Street Works Gazetteer contact the Roadworks Co-ordination Unit for more information).</p> <p>This is based on the area of a parking bay of 15sq.metres for parallel parking where an average charge would be £30 for all day parking.</p> <p>All other roads not included above</p> <p>This is based on the area of a parking bay of 15sq.metres for parallel parking where an average charge would be £15 for all day parking.</p>	<p>Footprint area of occupation charged at £2 per square metre per day.</p> <p>Footprint area of occupation charged at £1 per square metre per day</p>	<p>Footprint area of occupation charged at £2 per square metre per day.</p> <p>Footprint area of occupation charged at £1 per square metre per day</p>

Promotional events for commercial purposes:	Charge from 01/05/2011	Charge from 01/07/2012
<p>St. Nicholas Street, Castlegate, the Green, and other pedestrian priority areas are charged as follows;</p> <p>Minimum invoice charge will be £50 per day to cover Administration costs.</p>	<p>Footprint area of occupation charged at £10 per square metre per day or part day</p>	<p>Footprint area of occupation charged at £10 per square metre per day or part day</p>

Pavement Café Licence	Charge from 01/05/2011	Charge from 01/07/2012
<p>Application for permission to site temporarily, tables & chairs on the footway. Validity period from 1 April – 31 March</p>	<p>£100 per year or part.</p>	<p>£100 year or part.</p>

Licensed hot / cold food units:	Charge from 01/05/2011	Charge from 01/07/2012
<p>Licensed hot / cold food unit across the City would be charged at a rate (Note, this exceeds the fees for pavement cafes which are now £100 per annum).</p>	<p>£250 per annum for lease of the road space.</p>	<p>£250 per annum for lease of the road space</p>

Day rates for the use of the road space for commercial purposes, for profit.	Charge from 01/05/2011	Charge from 01/07/2012
<p>These charges apply to semi-permanent units within the city Centre.</p>	<p>Footprint area of occupation charged at £0.67 per square metre per day. (minimum charge of £10 per day)</p>	<p>Footprint area of occupation charged at £0.67 per square metre per day. (minimum charge of £10per day)</p>

Driveway application out with planning application process	Charge from 01/05/2011	Charge from 01/07/2012
Administration costs for the processing of applications	£60	£60

Switching Off / Bagging of Traffic Signals	Charge from 01/05/2011	Charge from 01/07/2012
Administration costs for the processing of Bagging Signals	£50	£60

Note; these charges do not apply to Utilities Companies or their associated Contractors.

ABERDEEN CITY COUNCIL

COMMITTEE **Enterprise, Planning and Infrastructure**

DATE **31 May 2012**

DIRECTOR **Gordon McIntosh**

TITLE OF REPORT **Justice Mill Lane –
Revised Traffic Management Proposals**

REPORT NUMBER: **EPI/12/104**

1. PURPOSE OF REPORT

To advise the Committee of the results of the revised traffic management scheme proposals on Justice Mill Lane, as directed by the decision taken at the EP&I Committee of 13 September 2011.

2. RECOMMENDATION(S)

That the Committee

1. instructs officers to prepare the detailed design and tender the improvement works in Justice Mill Lane
2. instructs officers to proceed with the Traffic Regulation Order to ban right-turn traffic from Justice Mill Lane
3. instructs officers, under delegated powers, to accept the Tender provided that it is within appropriate budget
4. instructs officers, in principle, to prepare the detailed design and tender the works for the pedestrian crossing detailed in Option 1, upon conclusion of further discussions with bus operators about amalgamation of bus services

3. FINANCIAL IMPLICATIONS

Developer contributions are available for the implementation of the proposed traffic management scheme and subject to the preparation of an appropriate detailed design would fund the works.

The preliminary estimate for the revised traffic management proposals on Justice Mill Lane is £230,000. This cost for the signalised crossing on Holburn Street north (Option 1) is estimated at £37,000 and the signalised crossing on Holburn Street south (Option 2) is estimated at £35,000. Therefore the total cost of the works is; Option 1 - £267,000 and Option 2 - £265,000.

The proposed improvements whilst enhancing the environment will potentially reduce future road revenue maintenance costs. There are

existing maintenance issues including drainage and surface degradation and these would be addressed during construction of the traffic management scheme, therefore reducing the immediate ongoing road revenue maintenance costs. However, maintenance costs for a pedestrian crossing north of Justice Mill Lane would increase due to the use of double height signal heads, requiring the use of a cherry picker for maintenance access, also necessitating lane closures. These additional maintenance costs cannot be accurately estimated at this time.

4. OTHER IMPLICATIONS

None

5. BACKGROUND / MAIN ISSUES

5.1 Report

At its meeting on 23 April 2010 the Enterprise, Planning and Infrastructure Committee considered a report by officers relating to the Traffic Management Proposals for Justice Mill Lane.

The Committee resolved to approve the preliminary scheme on Justice Mill Lane and to commence the required legal process for the necessary Traffic Regulation Order.

At its subsequent meeting on 13 September 2011 the Enterprise, Planning and Infrastructure Committee considered a report by officers in respect of a traffic management design for Justice Mill Lane which included the introduction of a one-way system (westbound), a revision of parking restrictions, traffic management, realignment of kerb lines and the upgrading of the footway to provide a 2m minimum width. A controlled pedestrian crossing was also proposed on Holburn Street north of the Justice Mill Lane junction. .

The Committee resolved not to proceed with the proposed one-way system, and instructed officers to review the area and to develop an option which proposed an alternative traffic management scheme that would allow there to remain two-way traffic and would benefit pedestrians, cyclists and motorists.

5.2 Proposals Review

5.2.1 The revised design to allow two-way vehicular traffic includes, as previous, traffic calming measures, a right-turn ban when exiting Justice Mill Lane westbound into Holburn Street, realignment and upgrade of the north footway to provide a continuous 2m minimum width. Due to the road space available there is no option which can accommodate two-way vehicular traffic and a dedicated cycling facility. In order to maintain two-way traffic and a standard 2m wide footway it has been necessary to include a give and take priority section where there is limited road space for two vehicles to pass and this effectively

enhances the traffic calming element of the design. As indicated in Cycling by Design the option chosen seeks to use the existing carriageway environment and improve this for cycling and other non motorised users by controlling the speed and volume of traffic.

- 5.2.2 A controlled pedestrian crossing is also proposed as part of the scheme on Holburn Street to improve pedestrian access, with two options being considered.

Crossing Option 1 – a crossing directly north of the junction of Justice Mill Lane. This crossing is on the pedestrian desire line, as it forms a direct link between the existing, heavily used, pedestrianised Alford Lane and Justice Mill Lane/Holburn Street/Union Street and is the current crossing point for the majority of pedestrians. To locate the crossing here would require the removal or amalgamation of existing bus stops in order to provide suitable line of sight when approaching the crossing from Union Street. As visibility of the proposed crossing location is compromised by buses at the bus stops in the southbound lane of Holburn Street and vehicles queuing at the Holburn Street/Union Street junction, it is recommended that this crossing should include double height signal heads in order to provide some visibility of the crossing to drivers, as potentially both the primary and secondary signal heads would be obscured by large vehicles in both directions.

The inclusion of double height signal heads and associated works increases the costs of this option by £2000.

The Public Transport Unit (PTU) and bus operators have been consulted but are not in favour of additional pedestrian crossings on Holburn Street, particularly north of Justice Mill Lane. In their view, bus services are already delayed in accessing and moving off from bus stops at this location. The PTU and bus operators are of the opinion that any additional crossing here has the potential to cause significant delays to these bus services.

The PTU would also prefer not to remove or amalgamate any of the existing bus stops north of Justice Mill Lane. Their view is that these are heavily utilised services and that amalgamation of stops is not possible. With regard to the removal of any bus stops and relocation south of Justice Mill Lane, PTU feel that due to existing footway widths the stops would have to be located a considerable distance south of the junction and would not be as convenient for passengers.

It is the Road Safety officers' view that Option 1 is the preferred Option. The vast majority of pedestrians cross here at present, at an uncontrolled crossing point. There are many pupils from Harlaw Academy using Alford Lane and then crossing Holburn Street to head towards Union Street and the main shopping areas. The provision of a controlled crossing on what is clearly the pedestrian desire line is felt to provide Best Value in terms of expenditure, as this crossing is more likely to be used than a controlled crossing provided south of Justice Mill Lane.

Crossing Option 2 – a crossing directly south of the junction of Union Grove where there is currently a pedestrian refuge island. However this option is away from the principal pedestrian desire line as outlined above and is therefore unlikely to alter the current pedestrian movement of crossing directly at the junction of Justice Mill Lane. It would though, provide an enhancement over the existing uncontrolled pedestrian crossing at this location. It is the Road Safety officers' view that a crossing at this location would not be used by the majority of pedestrians and would not offer Best Value in terms of expenditure.

5.2.3 Plans of the proposed traffic management proposals and crossing locations are attached in Appendices A and B.

5.2.4 A ban on right-turning traffic from Justice Mill Lane is proposed. A right-turn ban supports road safety at this location by removing the existing dangerous vehicular conflicts. The right-turn ban is required, should Option 1 be adopted, to meet design standards relating to the location of a pedestrian crossing in close proximity to a junction.

5.3 Consultation Comments

Consultation documents have been sent to elected members as the first stage of this process but no comments have been received.

5.4 Detailed Design

It is recommended that officers take steps to conclude the detailed design for the proposed scheme in Justice Mill Lane and prepare the contract documents for Tender purposes. It is also recommended that officers hold further discussions with bus operators regarding amalgamation of bus stops north of Justice Mill Lane so that detailed design of Crossing Option 1 can be concluded, followed by the preparation of contract documents for Tender purposes.

6. IMPACT

Within the Community Plan, City Centre redevelopment is identified as a strategic priority, and within the Single Outcome Agreement, two national outcome objectives identified are that “We live in well designed, sustainable places” and that “We value our natural and build environment and protect it and enhance it for future generations.”

In its document, “Vibrant, Dynamic and Forward Looking,” the City’s Partnership states in the Economic Development Section, that it will “Implement master plans, working with partners, including those in the private sector,” and that it will “Continue to drive regeneration and take advantage of regeneration opportunities city wide, as they present themselves”

7. BACKGROUND PAPERS

Minutes of Enterprise Planning and Infrastructure Committee meetings
20 April 2010.

<http://councilcommittees/mgConvert2PDF.aspx?ID=1559&T=1>

and 13 September 2011

<http://councilcommittees/mgConvert2PDF.aspx?ID=1908&T=1>

8. REPORT AUTHOR DETAILS

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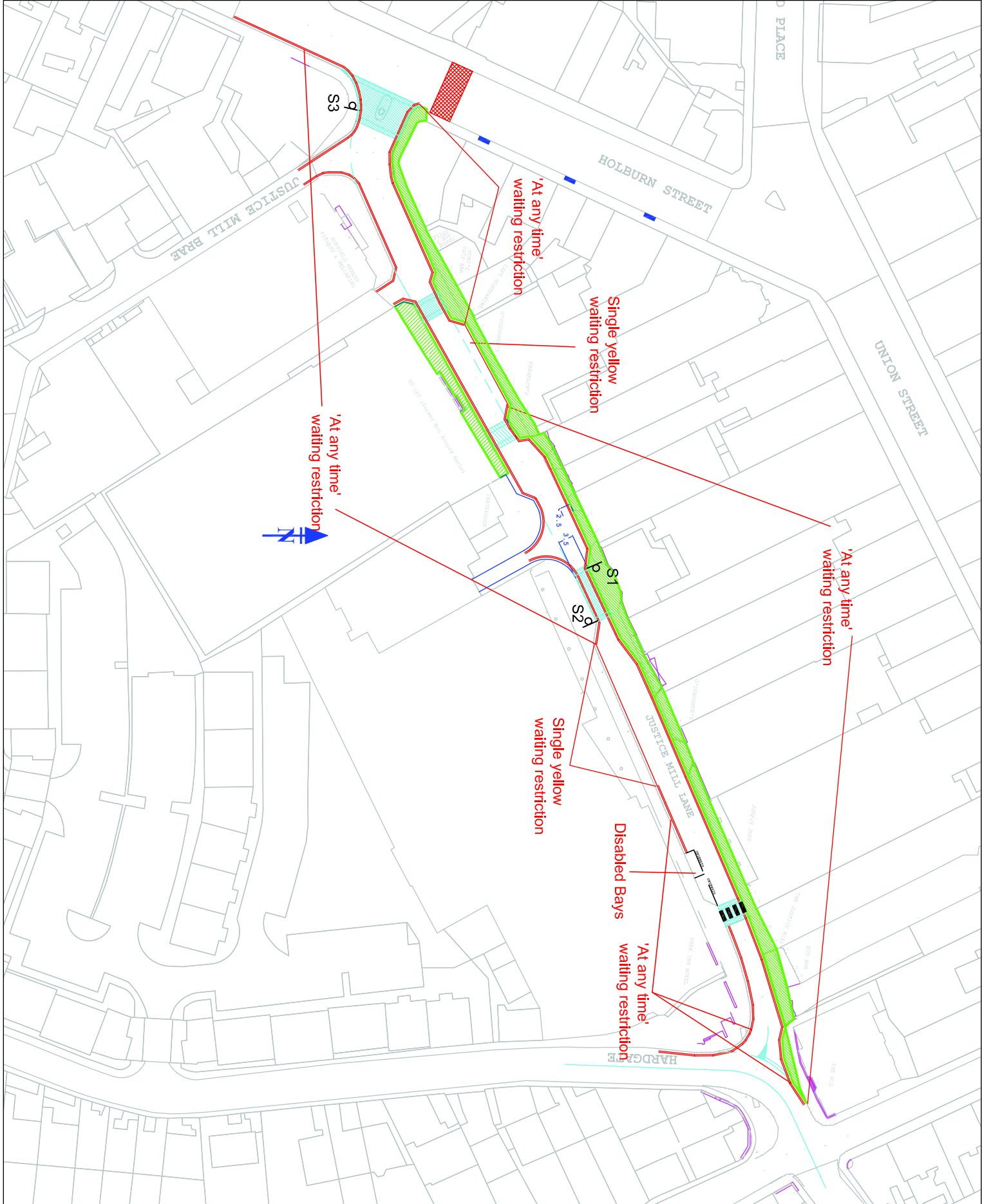
Appendix A

Crossing Option 1 - north of Justice Mill Lane






Appendix B

Crossing Option 2 - south of Justice Mill Lane

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KEY

-  RAISED TABLE
-  FOOTWAY RECONSTRUCTION
-  PEDESTRIAN CROSSING
-  ZEBRA CROSSING
-  BUS STOPS



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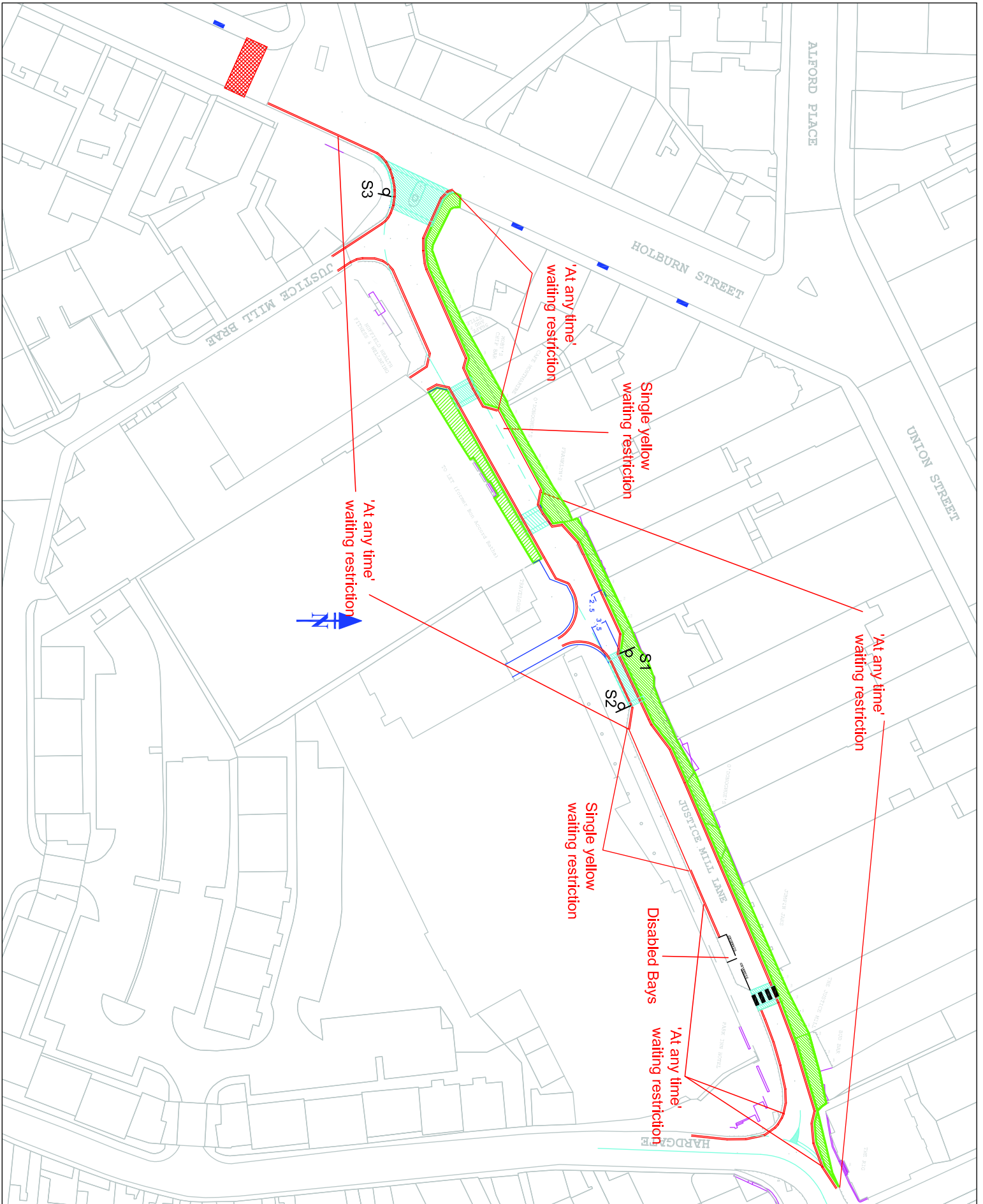
Client: ABERDEEN CITY COUNCIL

Project: JUSTICE MILL LANE









APPENDIX A - CROSSING OPTION 1
 TRAFFIC MANAGEMENT PROPOSAL

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KEY

-  RAISED TABLE
-  FOOTWAY RECONSTRUCTION
-  PEDESTRIAN CROSSING
-  ZEBRA CROSSING
-  BUS STOPS
-  **S1**
Diag. 615
-  **S2**
Diag. 611
-  **S3**
Diag. 612

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ABERDEEN CITY COUNCIL

Project: JUSTICE MILL LANE

Title: APPENDIX B - CROSSING OPTION 2
 TRAFFIC MANAGEMENT PROPOSAL

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure	DATE	31 May 2012
DIRECTOR	Gordon McIntosh		
TITLE OF REPORT	Roads and Transport Related Capital Budget Programme 2012-2013		
REPORT NUMBER:	EPI/13/121		

1 PURPOSE OF THE REPORT

This report brings together the proposed roads and transportation programme from the approved Capital budgets for 12/13. This is presented as a provisional programme. Members are asked to approve the specific schemes where detailed and the budget headings for the remainder. In addition provisional programmes for 2013/14 and 2014/15 are also included where possible.

2 RECOMMENDATIONS

1. The Committee approves the schemes listed in the Appendices as the detailed proposals for expenditure within budget headings.
2. Instruct appropriate officials to implement the detailed programme.
3. Agree for officers to amend the programme in consultation with local members should priorities change during the year.
4. Where traffic legislation is necessary, to approve the proposals in principle and instruct the appropriate officials to progress the necessary legal procedures.

Where no significant objections have been received at the statutory consultation or public advertisement stages to instruct the appropriate officials to implement the scheme, otherwise these would be reported back to future committee.

- 5 Grant approval to appropriate officers to award contracts on receipt of a valid tender submission subject to necessary funding in the approved revenue and capital budget

3. FINANCIAL IMPLICATIONS

Expenditure will be in accordance with the Council's approved Capital budgets for 2012 - 2013.

4. SERVICE & COMMUNITY IMPACT

This report has no direct implications in relation to Equalities & Human Rights Impact Assessment.

The implementation of the programme will assist roads and footways within the City being maintained to an acceptable standard thus reducing the risk of injury to members of the public.

The proposals are in line with our Transportation Strategy to provide safe crossing, cycling and walking facilities and reduce traffic speeds thereby contributing to accident reduction across the City and improve safety for all road users.

Aberdeen is an even more attractive place in which to do business (VD&FL).

Continue to invest in proper maintenance of roads, pavements and street lights (VDFL);

Single Outcome Agreement, National Outcomes 5, 6, 10, 12, & 14

5. OTHER IMPLICATIONS

There are risks in promoting Traffic Regulation Orders due to possible public objection and this may delay some of the proposed schemes.

6. REPORT

This report brings together, for members' information, the proposed programme for Capital Funding spend for both Roads and Transportation for 2012/2013 together with provisional reserve list programme for 2012/2013.

The provisional reserve programme for 2013/2014 will allow substitution of schemes should it not be possible to implement any of the proposed 2012/2013 schemes.

The appendices set out the proposed programme of works which will be funded through the approved Capital budgets of the Council together with linkages to the community action plans

Estimated Costs for the individual proposed works are included in a Confidential Report to the Committee.

Traffic Lights and Pedestrian Crossing:

A Capital budget of **£500,000** has been allocated to allow the continued upgrade of the systems across Aberdeen, corridor delays are reduced by the upgrading of these outdated systems ensuring that there are no delays in obtaining outdated parts. This proposed programme of works and reserve programme are set out in **Appendix A**

Lighting Improvements:

Planned lighting improvements have been allocated a capital budget of **£300,000**. This will be used, in the majority, for the replacement of lighting columns that have been identified as potentially dangerous or beyond their design life. All new street lights are being designed with energy reduction being a major consideration this reduces the energy bill and also lowers Aberdeen City Council's carbon footprint. Proposed programme and reserve programme for Street Lighting are in **Appendix B**.

Cycling Walking Safer Streets:

A grant of **£252,000** has been awarded by the Scottish Government for Cycling Walking Safer Streets (CWSS) projects in Aberdeen. The programme for these works are detailed in **Appendix C** and will provide significant road safety benefits in an effort to achieve accident reduction as well as reduce the number and severity of injuries sustained in road traffic accidents across the city. All schemes will be implemented as soon as possible subject to the successful promotion of any required legislation. A requirement for this budget is that the spend on cycling schemes should be a minimum of 36% of the grant.

Road Safety Schemes:

A Capital Budget of **£50,000** has been allocated for Road Safety Schemes. This budget is primarily used for the implementation of small scale traffic schemes, lining and signing which assists in the provision of safer streets for all the traveling public. The proposed programme of spend for this budget is in **Appendix D**

Footway Resurfacing:

A budget of **£512,000** has been allocated for footway resurfacing. The programme has been formulated on the basis of detailed surveys, in order to maintain a standard level of comparison of all footways they have been assessed by the same person, and targeted at footways categorised as being in a bad or poor condition.

At present the footways in Aberdeen as assessed as follows:

- Firstly they are assessed as good, adequate, poor or bad.

- If the condition is considered as bad the footway is given a further condition index of 1 - 10. 10 is the very worst condition and if finance were available then these footways would be resurfaced as soon as possible.

As the 1 -10 condition index can be very subjective we have recently started measuring the amount of reconstruction necessary to more accurately refine condition index.

From next year we will be changing the condition index to 1 - 4 to have consistency across Scotland as various evaluation and scoring systems currently exist.

The condition of sections of footway included in the programme are shown in the report under **Assessed Condition**, The detailed programme is set out in **Appendix E**. A Reserve programme is given in **Appendix F**

Carriageway Resurfacing:

The Capital carriageway resurfacing programme has been allocated a budget of **£2,013,000**. The programme is generally prepared on the basis of the results of the road condition surveys of the existing infrastructure.

The survey identifies sections of road as falling into one of three categories, Green – acceptable condition, Amber – causing concern and should be considered for treatment and Red – of concern and requiring treatment. This information is built up from a survey programme of all Scotland's roads and allows annual comparisons with all Roads Authorities in the country.

The detailed programme is set out in **Appendix G**. A Reserve Programme is given in **Appendix H**.

(A copy of the Road Condition Index is attached as **Appendices L & M**)

The condition of sections of carriageway included in the programme are shown in the report under **Assessed Condition**, in order to maintain a standard level of comparison all roads surfaces have been assessed to the same criteria. Due to the severity of the weather from the floods in September and October through to the snow and extremely low temperatures from November to January many road surfaces have suffered significant deterioration since the Road Condition Survey was carried out and staff have and are currently reassessing these roads and changes to the proposed programme may be necessary during this financial year.

Drainage: A Capital Budget of **£100,000** has been allocated for the Drainage works, this programme is shown in **Appendix J**

Road Sign Replacement: A Capital Budget of **£50,000** has been allocated for the Road Sign Replacement Programme; this programme is shown in **Appendix K**

Weak & Major Bridge Repairs: A Capital Budget of **£50,000** has been allocated for both Weak Bridge Repairs and Major Bridge Works.

Flood Prevention Schemes: A Capital Budget of **£50,000** has been allocated for Flood Prevention Schemes

Appendices L & M shows the changes in the Road Condition Index (RCI) for Aberdeen City under the new approved measurement criteria with Audit Scotland. It has been decided that the measurement for the RCI will now consider a four year survey period rather than the two year period measured to date. This change was required due to increased concerns that the U Class roads were not reflecting the true index when measured over a two year period. It should be noted that only 10% of the U Class roads are measured on an annual basis.

7. REPORT AUTHOR DETAILS

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Appendix A

ITS UNIT 2012/2013 TRAFFIC SIGNAL REFURBISHMENT PROGRAMME

No	Site	Installation	Type	Estimated Cost
1	HOLBURN STREET / GREAT WESTERN ROAD	Refurb'	Junc	
2	HUTCHEON STREET / BERRYDEN ROAD	Refurb'	Junc	
3	WESTBURN ROAD / WATSON STREET	Refurb'	Junc	
4	SKENE SQUARE NORTH of ROSEMOUNT PLACE	Refurb'	Pelican	
5	COMMERCE STREET @ BEACH BOULEVARD	Refurb'	Pelican	
6	SKENE SQUARE SOUTH of ROSEMOUNT PLACE	Refurb'	Pelican	
7	BERRYDEN ROAD @ CHESTNUT ROW	Refurb'	Pelican	
8	FIBRE OPTIC CONNECTIONS			
			Total	£500,000

Appendix B

Proposed Capital Programme 2012/13

Scheme	Estimate
Inchgarth Road Phase 2 – Column Replacements	
Skene St./Carden Pl. – Column Replacements	
Desswood Place – Column Replacements	
Thomson Street Lane – Column Replacements	
Holburn Street – Column Replacements	
Huntly/Rose Chapel Streets – Column Replacements	
Kidd St./Gilcomston Land – Column Replacements	
Albyn Place – Column Replacements	
Mounthooly Roundabout – Column Replacements	
Albury Road – Column Replacements	
Craigie Loanings – Column Replacements	
Total	£300,000

Appendix C
Cycling Walking Safer Streets

LOCATION / PROPOSALS	DESCRIPTION OF WORK	IMPLICATIONS	JUSTIFICATIONS TO CWSS	ESTIMATED COSTS
Schemes C/F from 11/12				
At the time of writing this report there are a number of outstanding contracts from financial year 11/12 that have yet to be completed and which may extend into financial year 12/13. This may have an effect on the schemes identified below.				
Deeview Road South Item No.	Implementation of High containment kerbs (Kerbs purchased previously)	No detrimental implications	Pedestrian / Child Safety. Improvements to driver safety.	
Schemes for 2012/13				
Various locations across the City. Item No.	Small scale improvements to pedestrian crossing / Disabled Crossing points / Core Paths - Dropped kerbs and pedestrian guard-rails.	Budget for implementation at various locations City wide. Individual location assessment - Local Councillors to be kept appraised.	Pedestrian/ Child Safety	
Various locations across the City. Item No.	Publicity in relation to Promotion of Bike Week / Cycle Map / Zenith / Other Cycling Initiatives across the City / Green Transport Week / European Mobility week.	Budget to be spent City wide.	Pedestrian/ Child Safety + Safety to Cyclists	

Various locations across the City. Item No.	Cycling Facilities /Links / Parking / Lining & Signing throughout the City to provide missing Links on the road network (Requirement of Grant Offer to spend minimum 36% but preferably 50 % on cycling schemes)	Budget for implementation at various locations City Wide. Discussions have been ongoing with the Cycling Forum to identify a series of improvements throughout the City. Some schemes may require the promotion of legislative procedures for a Traffic Regulation Order which may affect implementation.	Pedestrian/ Child Safety + Safety to Cyclists	
Various locations throughout the City Item No.	Small scale improvements to signing & lining, bollards, barriers and all new works associated with traffic management / traffic Orders and road safety.	Budget for implementation at various locations City wide. Individual location assessment - Local members to be kept appraised.	Some of this work will require the promotion of legislative procedures which may effect implementation.	
Various locations throughout the City Item No.	Route action work on various rural routes and City wide locations that have been identified for improvements from the annual accident scan.	Pedestrian / Child Safety. Improvements to driver safety and speed reduction.	No detrimental implications.	
Various Safety Campaigns throughout the City (not site specific) Item No.	Publicity in relation to Road Safety Campaigns & Community Safety Safe Drive - Stay Alive Campaign.	Budget for Aberdeen City Contribution Local Transport Strategy Policy / Proposal SP1.	No detrimental implications.	
The Bush - Peterculter Item No.	Stop Accesss to Hillview Road as agreed with residents	No detrimental implications	Improved pedestrian access and safety	
North Deeside Road at Old Ferry Road Item No.	Implementation of new traffic island	Waiting restrictions required and promotion of legislative procedures for a Traffic Regulation Order which may affect implementation. Cycle lane required.	Improved pedestrian access and safety Pedestrian/ Child Safety.	

Back Hilton Road between Ashgrove Road and Belmont Road Item No.	Implementation of new traffic island	Waiting restrictions required and promotion of legislative procedures for a Traffic Regulation Order which may affect implementation. Cycle lane required.	Improved pedestrian access and safety Pedestrian/ Child Safety.	
Footpath - Abbotshall Crescent to Abbotshall Walk Item No.	Provision of street lighting for children going to and from school (Path has no lights)	No detrimental implications	Improved pedestrian access and safety Pedestrian/ Child Safety.	
Dyce Drive North of Dyce Avenue Item No.	Provision of new bus stop / dropped kerbs and hardstanding	No detrimental implications	Improved pedestrian access and safety	
Aberdeen City Council Road Safety Plan Item No.	Bi-annual review and publication of the Road Safety Plan. (Statistical update only - Aberdeen City)	Budget for City wide Local Transport Strategy Policy / Proposal SP1.	No detrimental implications.	
Broomhill Road Item No.	Replace existing Zebra crossing to a Puffin Crossing at the Primary School (Equipment purchased last financial year)	No detrimental implications	Improved pedestrian access and safety Pedestrian/ Child Safety.	
Holburn Road Item No.	Replace existing Zebra crossing to a Puffin Crossing. (Equipment purchased last financial year)	No detrimental implications	Improved pedestrian access and safety Pedestrian/ Child Safety.	
			Overall Allocated Non-Housing Capital Budget	£252,000

**Appendix D
Traffic Calming and Road Safety**

LOCATION / PROPOSALS	DESCRIPTION OF WORK	JUSTIFICATION	IMPLICATIONS	ESTIMATED COSTS
Schemes C/F from 11/12				
At the time of writing this report there are a number of outstanding contracts from financial year 11 / 12 that have yet to be completed and which may extend into financial year 12 / 13. This may have an effect on the schemes identified below.				
Route Action - Unclassified Road between Dyce Drive and Bendauch Farm / Caskieben (Phase 3)	Hazard marker posts, and improvements to signing / lining	Improvements to driver safety	No detrimental implications.	
Schemes for 2012/13				
Various locations throughout the City Item No.	Re-texturing of existing road surface at various locations / corners that have been identified as a concern from the annual accident scan.	Improvements to driver safety and speed reduction.	No detrimental implications.	
North / South Grampian Circle Item No.	Proposed "One Way"	The proposals will require the promotion of the legislative procedures for a Traffic Regulation Order which may affect implementation.	Improvements to driver safety Pedestrian / Child safety	
Foresterhill Road / Westburn Road junction Item No.	Improvements to central reserve	Central reservation and bollard are always being run over.	Improvements to driver safety Pedestrian /Child safety	
Foresterhill Road / Ashgrove Road West junction	Set back stop line and traffic signal loops	To allow buses to turn right from Ashgrove Road West into Foresterhill Road	No detrimental implications.	
Various locations throughout the City Item No.	Route action work on various rural routes and City wide locations that have been identified for improvements from the annual accident scan.	Pedestrian / Child Safety. Improvements to driver safety and speed reduction.	No detrimental implications.	
Allocated Non Housing Capital Budget				£50,000

Appendix E
Capital Footway Programme 2012-2013

Name of Road	Location and Description of Works	Assessed Condition	Estimated Cost
VARIOUS SITES	Tree Removals and Footway Reinstatements	10	
ALBYN PLACE	South Side from Albyn Grove to Queen's Cross. Resurface sections of slabs and replace grano pedestrian crossings with pavoirs .Install City Centre bollards.	10	
PROVOST GRAHAM AVENUE	North Side from Queens Road to Hazlehead Crescent. Renew kerbs and replace slabbed footway with bitmac.	10	
BROOMHILL RD	South Side.Worst sections between opposite 78 to Abergeldie Road .Renew concrete slabs, replace insitu concrete with pavoirs at crossings, install morpeth bollards and reinstate distressed areas of carriageway in HRA.	10	
CAIRNCRY ROAD	From 255 to 243. Resurface footway with bitmac.	10	
BIRKHALL PARADE	From 90 to 106 and from 106 to 140 section crossing end of grassed island in cul de sac. Resurface footway with bitmac.	10	
GREAT NORTHERN ROAD	St Machar Drive - 152(North Side) Renew kerbs and resurface footway in slabs/bitmac.	10	
ABOYNE ROAD	East Side from Ramsay Crescent to 23/25.Renew kerbs and resurface footway with bitmac.	10	
GREAT NORTHERN ROAD	ATM at Clydesdale Bank(433) to Central Bar(403 - 415),Lloyds Pharmacy(433) to Roscos Shop(447 - 451) and 493 to (481-487) Resurface 3 sections of footway in bitmac.	10	
HOLBURN STREET	East Side.Howburn Place to entrance to Talisman Oil. Relay stone kerbs and resurface footway with precast concrete slabs. Install bollards.	10	
INVERCAULD ROAD	South Side from Gairnsheil Avenue to no 37.Renew kerbs and resurface footway with bitmac.	10	
MANOR WALK	West and North Side from Manor Avenue to opposite 17.Relay stone kerbs and replace slabs with bitmac.	10	
Capital Footway Programme 2012-2013		Total	£512,000

**Appendix F
Footway Reserve List 2012-2013**

HOLBURN STREET	West Side.Nellfield Place to 190.(South end of Gillies Furniture Store).Relay stone kerbs and resurface footway with precast concrete slabs. Install bollards.	10
HOLBURN STREET	East Side.Entance to Talisman Oil to Holburn Bar excluding section recently completed outside Tesco Express.Relay stone kerbs and resurface footway with precast concrete slabs. Install bollards.	10
WALKER ROAD	West Side from Grampian Place to Polwarth Road.Relay stone kerbs and resurface footway in bitmac.	9
MANOR WALK	West and North Side from opposite 17 to Manor Drive. Relay stone kerbs and replace slabs with bitmac.	9
GREAT NORTHERN ROAD	South Side.Sections outside Lloyds Pharmacy(451) and Iceland. Resurface footway with bitmac.	9
MOIR DRIVE	1 - 17/19 Moir Drive.Renew kerbs and resurface footway with bitmac.	9
CAIRNWELL DRIVE	West Side from 137 - 167.Renew kerbs and resurface footway with bitmac.	9
ORCHARD ROAD	West Side from opposite 34 to Orchard Street. Relay stone kerbs and replace slabs with bitmac.	9
ORCHARD ROAD	East Side from University Rd to lane. Relay stone kerbs and replace slabs with bitmac.	8
ORCHARD ROAD	East Side from lane to No 4. Relay stone kerbs and replace slabs with bitmac.	8
MARQUIS ROAD	East Side Outside flats from 58 -60.Relay stone kerbs and resurface footway in bitmac.	8
GRAY STREET	Phase 4	9
MURRAY TERRACE	Phase 4	9
Current Footway Reserve List Total		£340,000

Appendix G

Capital Works Resurfacing 2012-2013

Name of Road	Location & Description of Works	Assessed Condition	Area (Sq m)	Estimated Cost
A956 Ellon Road	Southbound Balgownie Road to Don Bridge Resurface Carriageway	Red	1500	
A956 King Street	From Don Bridge to access to Lidl Resurface Carriageway	Red	3050	
A956 Wellington Road	Greenbank Road to Craigshaw Drive Southbound Carriageway Resurface Carriageway	Red	2350	
A956 Wellington Road	Roundabout at Queen Elizabeth Bridge Resurface Carriageway	Red*	1500	
A956 Wellington Road	Roundabout at West Tullos Road Resurface Carriageway	Red*	1600	
Holburn Street	Riverside Place Junction Resurface Carriageway	Red	800	
A956 King Street	Bus Lane Southbound from Errol Street to Seaforth Road Carriageway Resurfacing EME2	Red	350	
A956 King Street	Nearside Lane Northbound from Union Street to West North Street Carriageway Resurfacing EME2	Red	700	
B999 Tarves Road	At Hill of Tramaud Structural Carriageway Repairs	Red	1000	
Riverview Drive	East of Overton Circle to Stoneywood Road Resurface Carriageway	Red*	5100	
Greenwell Road	Rail Goods Entrance to Entrance to Nordan Resurface Carriageway	Red*	2600	
Dubford Road	Lamp Post 3 to Lamp Post 12 Resurface Carriageway	Red*	2190	
Sclattie Park	A96 to Kepplehills Drive Resurface Carriageway	Red*	1900	
Gordon Terrace	At War Memorial Resurface Carriageway	Red*	500	
Coronation Road	North Deeside Road to School Road Resurface Carriageway	Red	3500	
Morningside Road	At bend approaching Broomfield Road Resurface Carriageway	Red*	1600	
Contlaw Brae	North Deeside Road to Colthill Circle Resurface Carriageway	Red	1485	
School Road	Hillside Road to Tower View Walk Resurface Carriageway	Red	2250	
Culter				
Old Wellington Road	Junction of Cove Road Resurface Carriageway	Red	830	
Whitestripes Road	Various locations Structural Carriageway Repairs	Red	Sum	
Great Northern Road	North of Belmont Road to Northern Hotel Resurface Carriageway	Red	3150	
Fairview Brae	Fairview Street to Laurel Drive Resurface Carriageway	Red*	2450	

Name of Road	Location & Description of Works	Assessed Condition	Area (Sq m)
George Street	St Andrew Street Junction Resurface Carriageway	Red	240
Cove Road	East of Track (South Loirston Road) Structural Repair to Carriageway	Red	Sum
Various Locations	Structural Carriageway Repairs at Junctions		Sum
Kepplehills Road	At School Structural Repair to Carriageway	Red	Sum
Greenbank Road	From Wellington Road to Lampost No.9 Resurface Carriageway	Red*	2600
Victoria Street	Pitmedden Road Junction Resurface Carriageway	Red	700
Great Western Road	No. 530 to No. 540 Eastbound Structural Repair to Carriageway	Red	Sum
Earlswells Road	At Cults Primary School Resurface Carriageway	Red	700
Great Northern Road	Southbound at Don Street Junction Resurface Carriageway	Red	500
Greenwell Road	Entrance to Nordan to Pedestrian Railway Underpass Resurface Carriageway	Red*	2400
Foresterhill Road	From Cornhill Drive Roundabout to Entrance to Murdo's Bar Resurface Carriageway	Red	1150
Powis Place	George Street Junction Resurface Carriageway	Red	1400
Caskieben Road	At Caiesmill Resurface Carriageway	Red*	2200
Bressay Brae	Eday Gardens to Cava Close Resurface Carriageway	Red	1750
Beach Boulevard	Beach Ballroom Design of Road Re-alignment	Red	Sum
Arnage Drive	Fernhill Road to East Mains Avenue Resurface Carriageway	Red*	2800
Arnage Place	Arnage Drive to Mastrick Road Resurface Carriageway	Red*	400
Greenbrae Drive	Denmore Road to Lamp Post 13 Resurface Carriageway	Red*	2275
Kepplehills Drive	Sclattie Park to Cloverfield Place Resurface Carriageway	Red*	1300
Countesswells Road	Various locations Structural Carriageway Repairs	Red	600
Capital Works Resurfacing 2012-2013		Total	£2,013,000

Appendix H

Capital Works Resurfacing Reserve List 2012-2013

Name of Road	Location & Description of Works	Assessed Condition	Area (Sq m)	Estimated Cost
Hilton Avenue	Hilton Drive to No. 173 Resurface Carriageway	Red	3000	
Riverside Drive	From Great Southern Road past Cemetery to Lamp Post No. 54 Resurface Carriageway	Red	2400	
C55C Pitmedden Road	Forest Walks to Bendauch Resurface Carriageway	Red	3000	
Great Northern Road	Southbound from Lamp Post No. 86 to Lamp Post No. 72 Resurface Carriageway	Red	2400	
Kirkton Avenue	Lamp Post 6 to Pitmedden Road Resurface Carriageway	Red*	1460	
Riverview Drive	East of Overton Circle to Asda Junction at Netherview Avenue Resurface Carriageway	Red*	6900	
Commerce Street	Virginia Street Junction Resurface Carriageway	Red	1000	
Market Street	Junction Virginia Street Resurface Carriageway	Red	1000	
Balloch Way	Resurface Carriageway Riverview Drive to Netherview Avenue	Red*	3000	
Netherview Avenue	Balloch Way/Princess Drive to Berrywell Walk Resurface Carriageway	Red*	1460	
Beach Boulevard	Beach Ballroom Road Re-alignment and Carriageway Resurfacing	Red	5500	
Regent Walk	King Street to Golf Road Resurface Carriageway	Red	4200	
Capital Works Resurfacing Reserve List 2012-2013		Total		£937,000

Appendix J

Capital Works Drainage Main Programme 2012-2013

Name of Road	Location & Description of Works	Estimated Cost
Ladyhill Road	Install 2 No. Road Crossings	
Kirk Brae Cults	At Craigbank Install Overflow Pipe in Burn at Access	
B997 Scotstown Road	North of Shielhill Junction at Perwinnes Drainage Improvement	
C89C Chapel of Stoneywood Road	South of Forrit Brae Drainage Improvement	
Kirk Brae, Cults	South of Countesswells Road Drainage Improvement	
C55C Pitmedden Road	Bendauch Drainage Improvement	
Caskieben Road	The Rock Drainage Improvement	
Various	Unallocated to Resolve Serious Flooding Issues Arising	
Various	Replacement of Gullies	
	Capital Works Drainage Programme 2012-2013 Total	£100,000

Appendix K Signage Improvements

LOCATION / PROPOSALS Schemes C/F from 11 / 12		JUSTIFICATION	IMPLICATIONS	ESTIMATED COSTS
At the time of writing this report there are a number of outstanding contracts from financial year 11 / 12 that have yet to be completed and which may extend into financial year 12 / 13. This may have an effect on the schemes identified below.				
Schemes for 2012/13				
Controlled parking (Zone M.) Item No.	Rationalisation of existing pay & display / residents parking signage and lining throughout the zone	This scheme requires to be completed ASAP otherwise Aberdeen City Council may be open to further appeals and having to repay penalty charge notices.	If not completed, these areas could become unenforceable and therefore result in considerable loss of income to the Council	
City Wide Item No.	Review of speed limits on principal routes throughout the City	In August 2006 the Scottish Government published new guidance on setting local speed limits (Circular 01/2006). In addition, the Scottish Government formally requested that roads authorities review speed limits on all Class A and Class B roads by 2011 in accordance with the revised guidance. There is currently no requirement to review C class and unclassified roads.	Some of this work will require the promotion of legislative procedures which may effect implementation.	
City Wide Item No.	Removal of existing "At any Time" plates (Phase 2)	This scheme requires to be completed ASAP as the existing signage must be removed to comply with current legislation.		
Great Southern Road / Bridge of Dee roundabout Item No.	Improvements to road lane signage / line markings on approach to the A90 / Leggart Terrace / Bridge of Dee roundabout	Additional signage and rationalisation of existing line markings	Road Safety Improvements.	
Beach Esplanade Item No.	Width / Weight restriction signage	This scheme requires to be completed ASAP as the existing signage does not comply and is not enforceable by Grampian Police	Road Safety Improvements.	

Harbour Signage Item No.	Improvements to road signage associated with Aberdeen Harbour / Ferry terminal, particularly from the South	Additional signage required following complaints from Road Haulage Operators and Freight Transport Association.	Road Safety Improvements. Improved vehicular access	
Union Square / Bus Station / Railway Station / Harbour / Ferry terminal Item No.	Improvements to existing pedestrian finger post signs around Bus / Railway station / ferry terminal / Union Square to the city centre	Additional signage and rationalisation of existing signage	Improved pedestrian access and safety	
Various locations throughout the City Item No.	Improvements to signage associated with traffic management / parking restrictions and road safety.	Budget for implementation at various locations City wide to comply with current legislation and our signing strategies. Individual location assessment - Local members to be kept appraised.	Pedestrian / child safety, Improvements to driver / cycle safety and speed reduction	
			Overall Allocated Non-Housing Capital Budget (Number 550)	£50,000

APPENDIX L: SRMCS RCI REPORT 2008-2011- ABERDEEN CITY

System: WDM PMS
 System Version: 4.4.0
 Run Identifier: SRMCS PI

Report Run Date : 14/02/2012
 Weighting Set ID: WSAllClasses v 0202
 Rule Set ID: RP10.01

Calculation Date: 14/02/2012
 From Date: 01/01/2008
 To Date: 31/12/2011

Results from Network Lengths Surveyed

Environment	Class	RED		AMBER 1		GREEN 1		COVERAGE		NETWORK km
		km	%	km	%	km	%	km	%	
Urban	A	5.777	5.807%	20.429	20.54%	73.270	73.66%	99.476	195.05%	51.000
	B	0.677	2.889%	4.316	18.42%	18.444	78.70%	23.437	81.10%	28.900
	C	3.063	6.454%	8.158	17.19%	36.241	76.36%	47.462	107.62%	44.100
	U	19.398	7.520%	67.911	26.33%	170.634	66.15%	257.943	39.92%	646.100
Rural	A	0.160	1.037%	2.478	16.07%	12.786	82.90%	15.424	214.22%	7.200
	B	0.551	3.862%	2.713	19.01%	11.005	77.13%	14.269	93.26%	15.300
	C	2.917	7.171%	10.529	25.88%	27.234	66.95%	40.680	69.66%	58.400
	U	2.152	10.474%	5.658	27.54%	12.736	61.99%	20.546	38.55%	53.300
Overall by Class	A	5.937	5.167%	22.907	19.94%	86.056	74.90%	114.900	197.42%	58.200
	B	1.228	3.257%	7.029	18.64%	29.449	78.10%	37.706	85.31%	44.200
	C	5.980	6.785%	18.687	21.20%	63.475	72.01%	88.142	85.99%	102.500
	U	21.550	7.738%	73.569	26.42%	183.370	65.84%	278.489	39.82%	699.400
Urban	All	28.915	6.751%	100.814	23.54%	298.589	69.71%	428.318	55.62%	770.100
Rural	All	5.780	6.357%	21.378	23.51%	63.761	70.13%	90.919	67.75%	134.200
All	All	34.695	6.682%	122.192	23.53%	362.350	69.79%	519.237	57.42%	904.300

Results of Surveys Weighted Across Total Network Lengths

Environment	Class	RED		AMBER 1		GREEN 1		COVERAGE		NETWORK km
		km	%	km	%	km	%	km	%	
Urban	A	2.962	5.807%	10.47	20.54%	37.56	73.66%			51
	B	0.835	2.889%	5.32	18.42%	22.74	78.70%			28.9
	C	2.846	6.454%	7.58	17.19%	33.67	76.36%			44.1
	U	48.588	7.520%	170.10	26.33%	427.41	66.15%			646.1
Rural	A	0.075	1.037%	1.16	16.07%	5.97	82.90%			7.2
	B	0.591	3.862%	2.91	19.01%	11.80	77.13%			15.3
	C	4.188	7.171%	15.12	25.88%	39.10	66.95%			58.4
	U	5.583	10.474%	14.68	27.54%	33.04	61.99%			53.3
Overall by Class	A	3.036	5.167%	11.630	19.94%	43.533	74.90%			58.200
	B	1.426	3.257%	8.231	18.64%	34.543	78.10%			44.200
	C	7.034	6.785%	22.696	21.20%	72.771	72.01%			102.500
	U	54.171	7.738%	184.782	26.42%	460.446	65.84%			699.400
Urban	All	55.231	7.17%	193.480	25.12%	521.388	67.70%			770.100
Rural	All	10.436	7.78%	33.859	25.23%	89.905	66.99%			134.200
All	All	65.667	7.26%	227.339	25.14%	611.294	67.60%			904.300

SPI CARRIAGEWAY CONDITION

Urban	32.30%	Rural	33.01%	All	32.40%
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